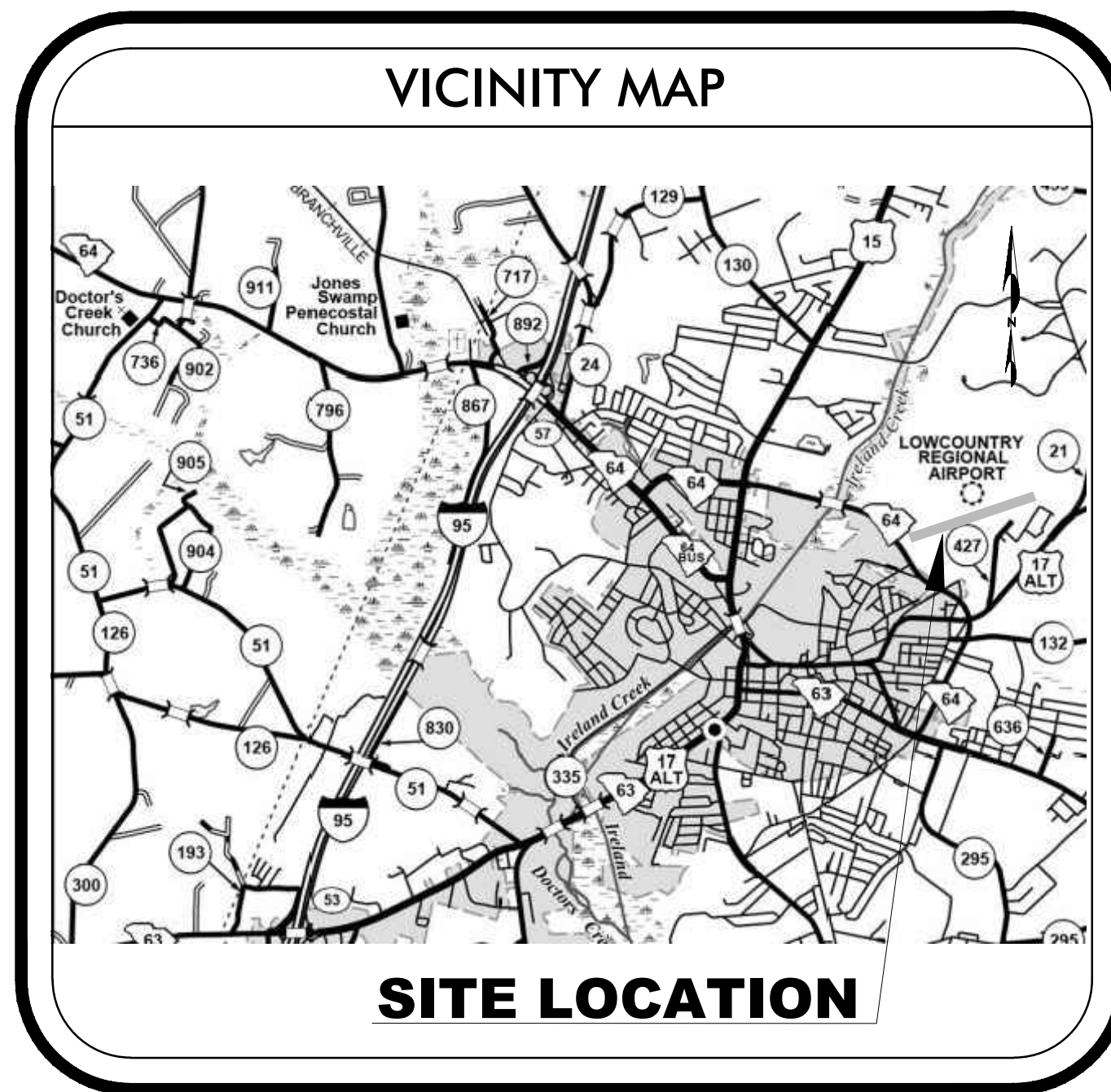


WALTERBORO-COLLETON COUNTY AIRPORT COMMISSION

CONSTRUCTION PLANS FOR

RUNWAY 5-23 REHABILITATION



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LOWCOUNTRY REGIONAL AIRPORT
WALTERBORO, SOUTH CAROLINA
BID #2025-02

Michael Baker

INTERNATIONAL
700 Huger Street
Columbia, South Carolina 29201

MARCH, 2025



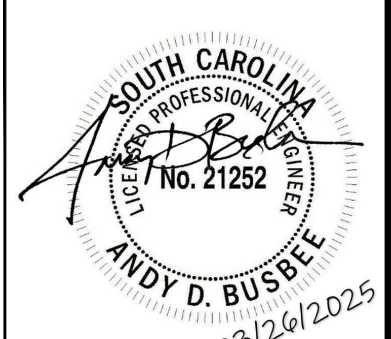
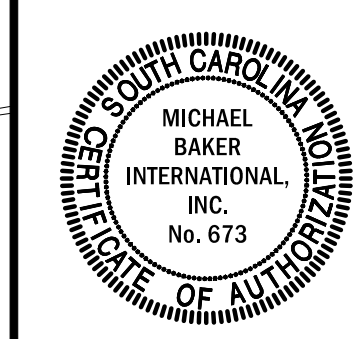
ISSUED FOR BIDS



WALTERBORO, SOUTH CAROLINA

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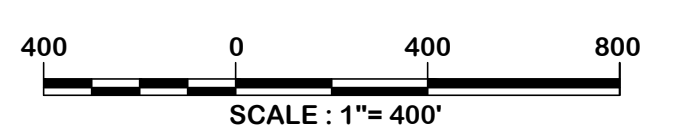


Designer:
BCM / LAW

Checked by:
ADB

Technician:
LAW

Project Number:
192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

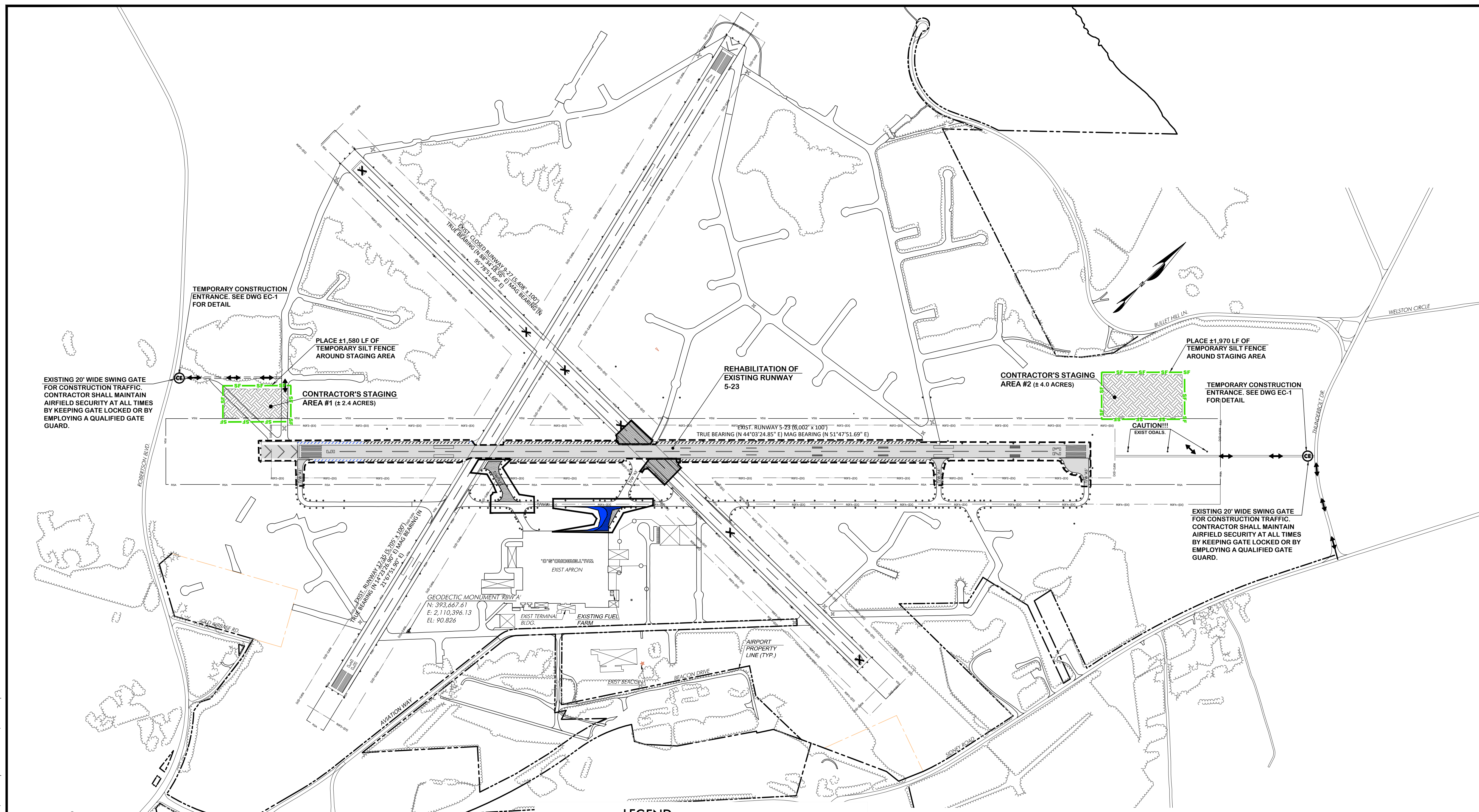
Drawing Name:
PROJECT LAYOUT PLAN

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"=400'** Sheet Number: **-**

Drawing Number:
PL-1



The construction scope of work generally includes the includes the rehabilitation of Runway 5-23 (6,002' long x 100' wide) and the reconfiguration of two mid-field taxiways. The project will be bid with two Base Bid Alternates and one Additive Bid.

Base Bid: Alternate No. 1 (Concrete) - This bid alternate consists primarily of placing a 6-inch or 7-inch layer of FAA P-501 Portland Cement Concrete (PCC) pavement over the existing asphalt pavement after it has been milled 0-6 inches for precise grade correction. This bid alternate also includes placement of nominal 2 inches of SCDOT Type C asphalt pavement on the portion of existing paved shoulders to remain, pavement demolition by milling of remaining failed shoulder pavement, runway grooving, pavement markings and erosion control.

Base Bid: Alternate No. 2 (Asphalt) - This bid alternate consists primarily of placing a 4-inch layer of FAA P-401 bituminous pavement over the existing asphalt pavement after it has been milled 0-4 inches for precise grade correction. This bid alternate also includes placement of nominal 2 inches of SCDOT Type C asphalt pavement on the portion of existing paved shoulders to remain, pavement demolition by milling of remaining failed shoulder pavement, runway grooving, pavement markings and erosion control.

Either bid alternate may be awarded, but not both. It is the preference of the OWNER to award Base Bid: Alternate No. 1, subject to availability of funding.

Additive Bid No. 1 includes pavement removal by milling of Taxiway A-2 and a portion of Taxiway A-3, removal and reinstallation of existing taxiway edge lighting and signage, required earthwork, placement of nominal 6 inches of crushed aggregate base course and nominal 4 inches of FAA P-401 bituminous pavement, pavement markings and erosion control.

LEGEND

	EXISTING PROPERTY LINE
	EXISTING FENCE LINE
	BASE BID - RUNWAY REHABILITATION
	BASE BID - PAVEMENT REMOVAL
	ADDITIVE BID - TAXIWAY RECONSTRUCTION
	ADDITIVE BID - PAVEMENT REMOVAL
	CONTRACTOR STAGING AREA
	CONTRACTOR HAUL ROUTE
	BASE BID - PROJECT WORK AREA
	ADDITIVE BID - PROJECT WORK AREA
	TEMPORARY SILT FENCE

- NOTES:**
1. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING AIRFIELD SECURITY.
 2. CONTRACTOR SHALL NOT USE EXISTING RUNWAY OR TAXIWAY/TAXILANE OR APRON PAVEMENT FOR ANY HEAVY CONSTRUCTION EQUIPMENT/TRUCKS. AVOID DAMAGE TO EXISTING RUNWAY AND TAXIWAY EDGE LIGHTS AND SIGNS.
 3. CONTRACTOR TO SMOOTHLY GRADE, SEED, AND PROVIDE EROSION CONTROL STABILIZATION IN STAGING AND PARKING AREA AND BATCH PLANT LOCATION AT CONCLUSION OF PROJECT.
 4. THERE ARE TWO CONTRACTOR STAGING AREAS INCLUDED TO AVOID THE CONTRACTOR CROSSING THE ACTIVE RUNWAY 17-35.

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2.5 COORDINATION

CONTRACTOR PROGRESS MEETINGS. CONSTRUCTION PROGRESS MEETINGS WILL BE HELD THROUGHOUT THE DURATION OF THE PROJECT. AT A MINIMUM, REQUIRED ATTENDEES WILL INCLUDE AIRPORT ENGINEER, AIRPORT OPERATIONS STAFF, CONSTRUCTION ADMINISTRATION ENGINEER, CONSTRUCTION OBSERVATION STAFF, PROJECT SUPERINTENDENT AND FOREMAN OF PRIME CONTRACTOR, AS WELL AS THE PROJECT FOREMAN FOR EACH SUBCONTRACTOR WITH WORK OCCURRING DURING THE CURRENT PERIOD. CONSTRUCTION PHASING AND SAFETY WILL BE A STANDING AGENDA ITEM AT THE CONSTRUCTION PROGRESS MEETINGS.

SCOPE OR SCHEDULE CHANGES. THE OWNER AND/OR ENGINEER WILL CALL SUCH COORDINATION MEETINGS AS MAY SEEM EXPEDIENT TO HIM FOR THE PURPOSE OF ASSURING COORDINATION OF THE WORK COVERED BY THIS CONTRACT AND/OR SCOPE OR SCHEDULE CHANGES. THE CONTRACTOR SHALL ATTEND ALL SUCH MEETINGS. ALL REVISIONS TO THE CSPP SHOULD BE SUBMITTED TO THE FAA FOR APPROVAL AS SOON AS REQUIRED CHANGES ARE IDENTIFIED.

2.6 PHASING

CONSTRUCTION PHASING FOR THIS PROJECT HAS BEEN COORDINATED WITH THE AIRPORT OPERATIONS PERSONNEL AND AIRPORT USERS. THE SEQUENCED CONSTRUCTION PHASES ESTABLISHED IN THIS CSPP HAVE BEEN INCORPORATED INTO THE PROJECT DESIGN AND ARE REFLECTED IN THE CONTRACT DRAWINGS AND SPECIFICATIONS.

PHASING FOR THE PROJECT HAS BEEN ESTABLISHED TO MINIMIZE IMPACTS TO THE AIRCRAFT OPERATIONS AREA (AOA). ONE (1) GENERAL PHASE OF CONSTRUCTION HAS BEEN ESTABLISHED FOR THE BID SCHEDULE. THE CONTRACTOR STAGING SHALL BE LOCATED AS INDICATED (REFER TO DRAWING CSPP-1). CONSTRUCTION SITE SIGNAGE WILL BE REQUIRED AT THE PROJECT ACCESS GATE. THIS WILL BE THE ONLY PROJECT INGRESS/EGRESS POINT TO/FROM THE AIRPORT PROPERTY.

A. PHASE ELEMENTS. GENERAL ELEMENTS OF PHASING ARE AS FOLLOWS:

CONSTRUCTION STAGING AREA - REFER TO DRAWING CSPP-1, FOR STAGING AREA LOCATIONS AND GENERAL SAFETY AND SECURITY NOTES CONCERNING USE OF THE STAGING AREAS.

CONSTRUCTION ACCESS AND HAUL ROUTES - REFER TO DRAWINGS CSPP-1, FOR ROUTING LAYOUT. APPLICABLE CONTROL ALONG CONTRACTOR HAUL ROUTES FOR BOTH SAFETY AND SECURITY MUST BE MAINTAINED AT ALL TIMES. THIS IS ESPECIALLY CONSIDERED AT THOSE LOCATIONS THAT REQUIRE THE CONTRACTOR TO CROSS OR MOVE THROUGH ACTIVE AIRFIELD SURFACES. REFERENCE SECTION 2.9B, VEHICLE AND PEDESTRIAN OPERATIONS, SECTION 2.19, MARKING AND SIGNS FOR ACCESS ROUTES, AND SECTION 2.22 PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS, ZONES AND SURFACES OF THIS DOCUMENT FOR ADDITIONAL INFORMATION.

REQUIRED HAZARD MARKING AND LIGHTING - LOW PROFILE BARRICADES SHALL BE PROVIDE BY THE CONTRACTOR. PLEASE REFER TO DRAWING CSPP-3. IN ADDITION, REFERENCE SECTION 2.19 MARKING AND SIGNS FOR ACCESS ROUTES, SECTION 2.20 HAZARD MARKING, LIGHTING AND

SIGNAGE, AND SECTION 2.22 PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS, ZONES AND SURFACES OF THIS DOCUMENT FOR ADDITIONAL INFORMATION.

LEAD TIMES FOR REQUIRED NOTIFICATIONS - THE CONTRACTOR IS REQUIRED TO COORDINATE THIS WITH THE CONSTRUCTION MANAGER AND AIRPORT OPERATIONS. LEAD TIMES FOR REQUIRED NOTIFICATIONS SHALL BE ESTABLISHED AT THE PRE-CONSTRUCTION MEETING.

PHASING SCHEDULE -

THE PHASING SCHEDULE FOR THIS PROJECT WILL BE DETERMINED BASED ON THE AWARDED BID SCENARIO.

REFER TO CSPP-1 FOR A PROPOSED PHASING SCHEDULE.

A. CONSTRUCTION SAFETY DRAWINGS. DRAWINGS CSPP-1 THROUGH CSPP-3 SPECIFICALLY INDICATE OPERATIONAL SAFETY PROCEDURES AND METHODS IN AREAS AFFECTED BY CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT.

2.7 AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

THE AIRPORT RUNWAY 5-23 WILL BE CLOSED DURING CONSTRUCTION.

THE AIRPORT RUNWAY 17-35 WILL BE CLOSED FOR INTERSECTION WORK.

2.8 NAVIGATION AID PROTECTION

BEFORE COMMENCING CONSTRUCTION ACTIVITY, PARKING VEHICLES, OR STORING CONSTRUCTION EQUIPMENT AND MATERIALS NEAR A NAVAID, COORDINATION WITH THE APPROPRIATE FAA ATO TO EVALUATE THE EFFECTS OF CONSTRUCTION ACTIVITY AND THE REQUIRED DISTANCES AND DIRECTION FROM THE NAVAID IS REQUIRED. CONSTRUCTION ACTIVITIES, MATERIALS/EQUIPMENT STORAGE, AND VEHICLE PARKING NEAR ELECTRONIC NAVAIDS ARE NOT ANTICIPATED IN THIS PROJECT.

2.9 CONTRACTOR ACCESS

A. HAUL ROUTES, STAGING AREAS AND CONSTRUCTION ACTIVITIES

THE CONTRACTOR SHALL CONDUCT HIS CONSTRUCTION OPERATIONS AS SHOWN ON THE PROJECT LAYOUT PLAN, CONSTRUCTION SAFETY AND PHASING PLANS, AND HIS APPROVED SEQUENCE PLAN.

STOCKPILED MATERIALS AND EQUIPMENT STORAGE SHALL NOT BE PERMITTED WITHIN THE RUNWAY OF TAXIWAY OBJECT FREE AREA. THESE MATERIALS AND EQUIPMENT SHALL BE STORED IN THE APPROPRIATE STAGING AREA. FOR ADDITIONAL INFORMATION, REFER TO SECTION 2.21 PROTECTION OF RUNWAY AND TAXIWAY AREAS, ZONES AND SURFACES.

THE CONTRACTOR'S STAGING AREA(S) AND HAUL ROUTE(S) SHOWN ON THE PLANS ARE GENERAL AND FOR INFORMATION PURPOSES ONLY. THE ACTUAL SIZE AND LOCATION OF STAGING AREAS AND HAUL ROUTES WILL BE APPROVED BY THE AIRPORT PRIOR TO CONSTRUCTION. (SEE DRAWING CSPP-1)

ALL EXISTING GRASSED AREAS WHICH ARE DISTURBED AS PART OF THE CONTRACTOR'S ACCESS ROAD, STAGING AREA, HAUL ROUTE AND STOCKPILES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AFTER COMPLETION OF THE PROJECT. IN ADDITION, ALL EXISTING ROADS AND PAVEMENTS THAT WILL BE USED AS THE CONTRACTOR'S HAUL ROUTE SHALL BE MAINTAINED DURING CONSTRUCTION AND RESTORED TO THEIR PRE-CONSTRUCTION CONDITION. NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE FOR THESE ITEMS. CONTRACTOR IS RESPONSIBLE FOR DOCUMENTING PRE-CONSTRUCTION CONDITION OF EXISTING ROADS TO BE USED AS HAUL ROUTES.

EXCEPT WHERE DESIGNATED ON PLANS OR AS AUTHORIZED BY THE ENGINEER, CONTRACTOR WILL NOT BE ALLOWED TO USE ANY OF THE EXISTING RUNWAY OR TAXIWAY AS PART OF THE HAUL ROAD.

ACTIVE TAXIWAYS, RUNWAYS AND APRONS SHALL BE KEPT FREE OF ALL DEBRIS, DIRT, ETC., AT ALL TIMES. ANY SPILLAGE OF SOIL, DEBRIS OR OTHER MATERIALS SHALL BE CLEANED UP IMMEDIATELY USING APPROPRIATE POWER SWEEPERS, BLOWERS OR OTHER APPROVED EQUIPMENT BY THE CONTRACTOR. DUST GENERATED DUE TO CONSTRUCTION ACTIVITIES SHALL BE ADEQUATELY CONTROLLED AT ALL TIMES. A PROGRAM OF REGULAR APRON AND TAXIWAY INSPECTION SHALL BE IMPLEMENTED BY THE CONTRACTOR AND COORDINATED WITH THE OWNER.

ALL EQUIPMENT MUST BE RETURNED TO THE STAGING AREA AT THE END OF EACH WORK DAY AND WHEN NOT ENGAGED IN THE CONSTRUCTION DURING NON-WORKING DAYS UNLESS APPROVED BY THE OWNER. THE OWNER WILL DESIGNATE AREAS FOR CONTRACTOR'S EMPLOYEE'S AUTO PARKING.

THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PROVIDING ALL PERMANENT AND TEMPORARY UTILITY CONNECTIONS TO THE STAGING AREA.

CONTRACTOR SHALL MAINTAIN ALL AIRFIELD SAFETY DEVICES SUCH AS STAKED LIMIT LINES, FOR THE DURATION OF THE PROJECT AS REQUIRED. DAMAGED STAKES OR FLAGGING SHALL BE REPLACED IMMEDIATELY. CONTRACTOR TO SUBMIT PLAN SHOWING LOCATION OF LIMIT LINES FOR EACH PHASE AND FOR PROJECT DURATION TO THE ENGINEER FOR APPROVAL.

BURNING OF DEBRIS WILL NOT BE ALLOWED ON AIRPORT PROPERTY.

CONTRACTOR TO FIELD VERIFY EXISTING CONDITIONS PRIOR TO BEGINNING CONSTRUCTION AND REPORT TO THE ENGINEER ANY VARIATIONS FROM THE INFORMATION SHOWN ON CONSTRUCTION PLANS.

A. VEHICLE AND PEDESTRIAN OPERATIONS

ACCESS TO AIRPORT OPERATIONS AREA (AOA) - THE AIRPORT OPERATIONS AREA IS DEFINED BY THE PERIMETER FENCE SURROUNDING THE AIRFIELD. ACCESS ONTO THE AOA IS THROUGH THE GATE ALONG THE FENCE OR DOORS THROUGH BUILDINGS. CONTRACTOR ACCESS ONTO THE AOA IS LIMITED TO THE GATE SHOWN ON THE PROJECT PHASING SHEETS. NO PERSON SHALL ENTER INTO THE AIR OPERATIONS AREA (AOA), OR ANY OTHER RESTRICTED AREA EXCEPT AUTHORIZED PERSONNEL ASSIGNED TO DUTY THEREIN.

MECHANISMS TO PREVENT IMPROPER MOVEMENT - CONTRACTOR OPERATIONS WITHIN THE AOA ARE LIMITED TO THE AREAS SHOWN ON THE PROJECT PHASING PLANS. A VISUAL BOUNDARY WILL BE INSTALLED BY THE CONTRACTOR AROUND ALL WORK AREAS ON THE APRON. BOUNDARY WILL CONSIST OF LOW PROFILE BARRICADES ON PAVEMENT SURFACES. THE PROJECT PHASING PLANS SHOW LOCATIONS OF WORK AREA BOUNDARIES, WHICH GENERALLY FOLLOW OBJECT FREE AREAS OF ADJACENT OPEN PAVEMENT. CONSTRUCTION VEHICLES AND PERSONNEL MUST NOT CROSS BARRICADES AT ANY TIME.

PARKING AREAS FOR PERSONAL VEHICLES AND EQUIPMENT - PERSONAL VEHICLES MAY NOT BE PARKED OR DRIVEN IN THE AOA. CONTRACTOR VEHICLES AND EQUIPMENT ARE ALLOWED INSIDE OF THE PROJECT WORK AREA WITHIN THE AOA. EQUIPMENT STAGING AND PARKING AREAS ARE AS SHOWN IN THE PROJECT PHASING PLANS. ADDITIONALLY, CONTRACTOR MAY PARK VEHICLES ANYWHERE WITHIN THE PROJECT WORK AREA AS DEFINED FOR EACH PHASE WITHIN THE PHASING PLANS.

HAUL ROUTES - THE PHASING PLAN SHEETS DEPICT HAUL ROUTES FOR BOTH OVERALL SITE ACCESS FROM SURROUNDING PUBLIC ROADWAYS AND HAUL ROUTES TO THE INDIVIDUAL PHASED WORK AREAS THROUGH THE AIRPORT PERIMETER FENCE. CONTRACTOR ACCESS AND HAULING OPERATIONS ARE STRICTLY LIMITED TO THE HAUL ROUTES SHOWN. CONTRACTOR IS RESPONSIBLE FOR ANY IMPROVEMENTS AND MAINTENANCE TO HAUL ROUTES AS NEEDED TO EFFICIENTLY PERFORM CONSTRUCTION ACTIVITIES. FOLLOWING COMPLETION OF CONSTRUCTION CONTRACTOR IS REQUIRED TO RESTORE HAUL ROUTE TO ORIGINAL CONDITION.

AIRPORT RULES FOR GROUND VEHICLE OPERATIONS - THE FOLLOWING RULES OF OPERATION MUST BE FOLLOWED AT ALL TIMES WHEN DRIVING ON THE AIRPORT. READ EACH RULE CAREFULLY AND MAKE SURE YOU UNDERSTAND YOUR RESPONSIBILITIES AS A DRIVER ON THE AIRPORT.

NO PERSON SHALL OPERATE MOTORIZED VEHICLES OR EQUIPMENT OF ANY KIND ON THE AIRPORT UNLESS IN POSSESSION OF VALID OPERATOR'S LICENSE AS REQUIRED BY THE STATE FOR THE TYPE OF VEHICLE BEING OPERATED.

NO PERSON SHALL OPERATE A MOTOR VEHICLE OR OTHER MOTORIZED EQUIPMENT OF ANY KIND ON THE AIRPORT IN A RECKLESS OR NEGLIGENT MANNER OR WITHOUT CAUTION OR IN ANY MANNER THAT ENDANGERS OR IS LIKELY TO ENDANGER PERSONS OR PROPERTY, OR IN EXCESS OF EITHER THE 20 MPH SPEED LIMIT PRESCRIBED BY THE AIRPORT.

NO PERSON SHALL FAIL TO GIVE PEDESTRIANS AND AIRCRAFT THE RIGHT-OF-WAY OVER VEHICULAR TRAFFIC. ALL GROUND VEHICLES SHALL PASS TO THE REAR OF TAXIING AIRCRAFT.

NO PERSON OPERATING A MOTOR VEHICLE ON THE AIRPORT SHALL FAIL TO GIVE PROPER SIGNALS OR FAIL TO OBSERVE THE DIRECTIONS OF POSTED TRAFFIC SIGNS OR TRAFFIC LANES.

NO PERSON UNDER THE INFLUENCE OF ALCOHOL OR DRUGS SHALL OPERATE A MOTOR VEHICLE ON THE AIRPORT.

CONTRACTOR WILL NOT BE ALLOWED TO OPERATE MOTOR VEHICLES OUTSIDE OF THE DESIGNATED WORK AREAS AS IDENTIFIED BY BARRICADES.

DRIVING PRIVILEGES TO OPERATE IN MOVEMENT AREAS ARE LIMITED TO VEHICLES WITH AN OPERATIONAL NECESSITY. TO DRIVE ON ANY PART OF THE AIRPORT OTHER THAN THE WORK AREAS AS DEFINED IN THE PROJECT PHASING PLANS THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS PERSONNEL, AND MUST OBEY THE FOLLOWING RESTRICTIONS:

VEHICLES OPERATING IN THE MOVEMENT AREAS MUST BE EQUIPPED WITH A YELLOW BEACON THAT IS TURNED ON AND OPERATING AND HAVE PROPER MARKINGS. REFER TO CONTRACTOR VEHICLE MARKING AND LIGHTING BELOW FOR VEHICLE MARKING AND LIGHTING REQUIREMENTS.

VEHICLE OPERATORS ARE EXPECTED TO FAMILIARIZE THEMSELVES WITH AIRPORT SIGNS AND MARKINGS.

THE AIRPORT STAFF SHALL HAVE THE AUTHORITY TO TOW OR OTHERWISE MOVE MOTOR VEHICLES THAT ARE PARKED BY THEIR OWNERS OR OPERATORS ON THE AIRPORT IN VIOLATION OF THE REGULATIONS OF THE AIRPORT, AT THE OPERATOR'S EXPENSE AND WITHOUT LIABILITY FOR DAMAGE THAT MAY RESULT IN THE COURSE OF OR BY REASON OF SUCH MOVING.

ALL VEHICLES OPERATING ON THE AIRPORT MUST HAVE THEIR HEAD / TAIL LIGHTS TURNED ON DURING DARKNESS AND LOW VISIBILITY CONDITIONS.

CONTRACTOR VEHICLE MARKING AND LIGHTING - MARKING AND LIGHTING OF CONTRACTOR VEHICLES SHALL BE IN ACCORDANCE WITH AC 150/521-5, PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT. EACH CONTRACTOR LICENSED VEHICLE MUST DISPLAY A COMPANY LOGO ON BOTH SIDES OF SUFFICIENT SIZE TO BE RECOGNIZABLE TO PERSONNEL. SIGNS MUST BE A MINIMUM OF 200 SQUARE INCHES AND BE APPROVED BY THE AIRPORT. SPECIALIZED CONSTRUCTION EQUIPMENT DOES NOT REQUIRE SIGNS. EACH CONTRACTOR LICENSED VEHICLE MUST HAVE A YELLOW/AMBER ROTATING BEACON AFFIXED TO THE UPPERMOST PART OF THE VEHICLE. LIGHT MUST BE VISIBLE FROM ANY DIRECTION, DAY AND

NIGHT, INCLUDING THE AIR. SPECIALIZED CONSTRUCTION EQUIPMENT DOES NOT REQUIRE ROTATING BEACON LIGHTS. CONTRACTOR VEHICLE MARKING AND LIGHTING IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE AIRPORT WILL NOT PROVIDE MARKINGS OR LIGHTS.

SITUATIONAL AWARENESS - VEHICLE DRIVERS MUST CONFIRM BY PERSONAL OBSERVATION THAT NO AIRCRAFT IS APPROACHING THEIR POSITION (EITHER IN THE AIR OR ON THE GROUND) WHEN GIVEN CLEARANCE TO CROSS A RUNWAY, TAXIWAY, OR ANY OTHER AREA OPEN TO AIRPORT OPERATIONS. IN ADDITION, IT IS THE RESPONSIBILITY OF THE ESCORT VEHICLE DRIVER TO VERIFY THE MOVEMENT/POSITION OF ALL ESCORTED VEHICLES AT ANY GIVEN TIME.

TWO-WAY RADIO COMMUNICATION PROCEDURES

GENERAL - THE AIRPORT OPERATOR MUST ENSURE THAT TENANT AND CONSTRUCTION CONTRACTOR PERSONNEL ENGAGED IN ACTIVITIES INVOLVING UNESCORTED OPERATION ON AIRCRAFT MOVEMENT AREAS OBSERVE THE PROPER PROCEDURES FOR COMMUNICATIONS, INCLUDING USING APPROPRIATE RADIO FREQUENCIES AT AIRPORTS. WHEN OPERATING VEHICLES ON OR NEAR OPEN RUNWAYS OR TAXIWAYS, CONSTRUCTION PERSONNEL MUST UNDERSTAND THE CRITICAL IMPORTANCE OF MAINTAINING RADIO CONTACT, AS DIRECTED BY THE AIRPORT OPERATOR, WITH:

- AIRPORT OPERATIONS
COMMON TRAFFIC ADVISORY FREQUENCY (CTAF), WHICH MAY INCLUDE UNICOM, MULTICOM.
AUTOMATIC TERMINAL INFORMATION SERVICE (ATIS). THIS FREQUENCY IS USEFUL FOR MONITORING CONDITIONS ON THE AIRPORT. LOCAL AIR TRAFFIC WILL BROADCAST INFORMATION REGARDING CONSTRUCTION RELATED RUNWAY CLOSURES AND "SHORTENED" RUNWAYS ON THE ATIS FREQUENCY.

FREQUENCY TO BE USED - CTAF/UNICOM: 122.800 MHZ

PROPER RADIO USAGE, INCLUDING READ BACK REQUIREMENTS.

PROPER PHRASEOLOGY, INCLUDING THE INTERNATIONAL PHONETIC ALPHABET.

MAINTENANCE OF THE SECURED AREA OF THE AIRPORT - THE PROJECT ACCESS GATE SHALL BE CLOSED AT ALL TIMES WHEN NOT BEING USED FOR THE CONTRACTORS' ACCESS TO THE PROJECT.

2.10 WILDLIFE MANAGEMENT

TRASH - FOOD SCRAPS MUST BE COLLECTED FROM CONSTRUCTION PERSONNEL ACTIVITY. THE CONTRACTOR WILL EMPTY ALL CONTAINERS OFF-SITE.

STANDING WATER. ANY ACTIVITY TAKING PLACE THAT CREATES A STANDING BODY OF WATER MUST BE REMEDIATED IMMEDIATELY.

FENCING AND GATES. AIRPORT OPERATORS AND CONTRACTORS MUST TAKE CARE TO MAINTAIN SECURITY DURING CONSTRUCTION WHEN ACCESS POINTS ARE CREATED IN THE SECURITY FENCING TO PERMIT THE PASSAGE OF CONSTRUCTION VEHICLES OR PERSONNEL. TEMPORARY GATES SHOULD BE EQUIPPED SO THEY CAN BE SECURELY CLOSED AND LOCKED TO PREVENT ACCESS BY ANIMALS AND UNAUTHORIZED PEOPLE.

DISRUPTION OF WILDLIFE HABITAT. THE CONTRACTOR WILL IMMEDIATELY NOTIFY DIRECTOR OF OPERATIONS BY PHONE OF WILDLIFE SITED ON THE AIRFIELD.

2.11. FOREIGN OBJECT DEBRIS

DESCRIPTION OF FOD - FOREIGN OBJECT DEBRIS (FOD) AT AIRPORTS INCLUDES ANY OBJECT FOUND IN AN INAPPROPRIATE LOCATION THAT CAN DAMAGE AIRCRAFT, EQUIPMENT, OR AIRPORT PERSONNEL. ON CONSTRUCTION SITES FOD TYPICALLY IS COMPRISED OF LOOSE GRAVEL, BLOWING SAND, WIRE BRISTLES FROM SWEEPER HEADS, FOOD WRAPPERS, MATERIAL PACKAGING. THE PRESENCE OF FOD ON AN AIRPORT'S AIR OPERATIONS AREA (AOA) POSES A SIGNIFICANT THREAT TO THE SAFETY OF AIR TRAVEL. FOD HAS THE POTENTIAL TO DAMAGE AIRCRAFT DURING CRITICAL PHASES OF FLIGHT, WHICH CAN LEAD TO CATASTROPHIC LOSS OF LIFE AND AIRFRAME, AND AT THE VERY LEAST INCREASED MAINTENANCE AND OPERATING COSTS.

METHODS OF FOD CONTROL:

TRAINING - CONTRACTOR SHALL PROVIDE TRAINING TO ALL EMPLOYEES WORKING WITHIN THE AOA ON EFFECTIVE FOD MANAGEMENT. TRAINING SHALL INCLUDE DESCRIPTION AND CONSEQUENCES OF FOD, FOD AWARENESS, AND HOUSEKEEPING PROCEDURES.

HOUSEKEEPING - PREVENTING FOD FROM OCCURRING IS THE MOST EFFECTIVE FORM OF FOD MANAGEMENT. CONTRACTOR MUST MONITOR CONSTRUCTION ACTIVITIES AND PROACTIVELY DEVELOP A PLAN TO PREVENT FOD FROM OCCURRING. TYPICAL FOD PREVENTION MEASURES INCLUDE THE USE OF COVERED TRASH CONTAINERS, COVERED LOADS, ZERO TOLERANCE OF LITTERING, AND TYING DOWN ITEMS WHICH MAY BE EASILY WIND-BLOWN.

GROUND VEHICLE TIRE INSPECTIONS - PRIOR TO CROSSING ACTIVE AIRFIELD PAVEMENT THE CONTRACTOR MUST PERFORM A VEHICLE TIRE CHECK FOR ANY LOOSE ROCKS THAT MAY BE IN THE TREAD. TIRES COVERED IN MUD MUST BE CLEANED PRIOR TO CROSSING ACTIVE PAVEMENT IN ORDER TO PREVENT TRACKING OF DIRT.

PAVEMENT SWEEPS - PRIOR TO OPENING SECTIONS OF PAVEMENT WITHIN A WORK AREA TO AIRCRAFT TRAFFIC, THE CONTRACTOR WILL BE REQUIRED TO SWEEP THE ENTIRE PAVEMENT SURFACE (INCLUDING SHOULDERS). METAL BRISTLED BROOMS ARE KNOWN TO CREATE FOD, AND THE CONTRACTOR WILL BE REQUIRED TO CLEAN ALL BRISTLES FROM THE PAVEMENT. COMPRESSED AIR AND VACUUMS CAN BE USED TO CLEAN PAVEMENT SURFACES AS WELL.

2.12 HAZARDOUS MATERIAL MANAGEMENT

HAZARDOUS MATERIAL CONTROLS - HAZMAT PROCEDURES TO BE DEVELOPED BY THE CONTRACTOR PRIOR TO THE ISSUANCE OF THE NOTICE-TO-PROCEED INCLUDING BUT NOT LIMITED TO:

FUEL OR HYDRAULIC FLUID SPILLS:

- THE CONTRACTOR WILL HAVE AVAILABLE A SPILL KIT CAPABLE OF CONTAINING AND REMOVING LEAKED FLUIDS.
THE CONTRACTOR WILL IMMEDIATELY NOTIFY THE DIRECTOR OF OPERATIONS BY PHONE OF ALL SPILLS.

FUELING - ALL CONSTRUCTION VEHICLES WILL BE FUELED OFF-SITE.

OTHER HAZMAT - NO OTHER HAZARDOUS MATERIAL IS EXPECTED TO BE TRANSPORTED ON-SITE DURING CONSTRUCTION.

2.13 NOTIFICATION OF CONSTRUCTION ACTIVITIES

LIST OF RESPONSIBLE REPRESENTATIVES - THE CONTRACTOR SHALL PROVIDE A LIST OF RESPONSIBLE REPRESENTATIVES/POINTS OF CONTACT FOR ALL INVOLVED PARTIES, AND PROCEDURES FOR CONTACTING EACH OF THEM, INCLUDING AFTER HOURS. PERSONS, WHO HAVE QUESTIONS CONCERNING POLICIES, PROCEDURES, OR REQUIREMENTS OF THE AIRPORT SECURITY PROGRAM, SHOULD CONTACT AIRPORT OPERATIONS. PERSONS WHO OBSERVE A SECURITY VIOLATION, SUSPICIOUS ACT OR ANY SERIOUS ACT THAT MAY ENDANGER PERSONS OR PROPERTY, SHOULD IMMEDIATELY CONTACT AIRPORT OPERATIONS.

- AIRPORT MANAGER (OFFICE) - (843) 549-2549
CONSTRUCTION ENGINEER (OFFICE) - (803) 254-2211

NOTAMS - CONTRACTOR SHALL COORDINATE WITH AIRPORT OPERATIONS PERSONNEL FOR THE ISSUANCE OF ALL NOTAMS RELATED TO THE PROJECT CONSTRUCTION. AIRPORT OPERATIONS AND FAA SHALL GENERATE AND ISSUE NOTAMS BASED ON CONTRACTOR CONSTRUCTION SCHEDULE AND FACILITY IMPACTS.

EMERGENCY NOTIFICATION PROCEDURES - IN THE CASE OF A LIFE-THREATENING SITUATION, DIAL 911 AND AIRPORT MANAGER IMMEDIATELY THEREAFTER. AIRPORT MANAGER WILL COORDINATE ANY EMERGENCY RESPONSE.

COORDINATION WITH ARFF - NO ARFF REQUIREMENTS.

NOTIFICATION TO THE FAA - THE CONTRACTOR SHALL ENSURE, THROUGH THE ENGINEER THAT ALL CONSTRUCTION EQUIPMENT IS AIR SPACED THROUGH THE APPROPRIATE FAA REGIONAL OR DISTRICT OFFICE PRIOR TO USING SUCH EQUIPMENT ON SITE.

2.14 INSPECTION REQUIREMENTS

FOD INSPECTION - THE CONTRACTOR SHALL KEEP THE PROJECT SITE AND VEHICLES CLEAN, EMPLOYING A "CLEAN AS YOU GO" APPROACH THROUGHOUT THE PROJECT.



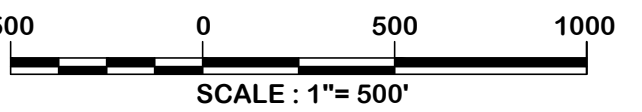
WALTERBORO, SOUTH CAROLINA

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Table with Designer: ADB, Checked by: ADB, Technician: NWM, Project Number: 192930



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Table with 4 columns: No., Description, Date, By. Title: REVISIONS

Project Name:

RUNWAY 5-23 REHABILITATION

Drawing Name:

CONSTRUCTION SAFETY AND PHASING PLAN NO. 2

FAA A.I.P. Project Number: 3-45-0057-025-2025

Date: MARCH, 2025 Division: AIRPORTS

Scale: N.T.S. Sheet Number: -

Drawing Number: CSPP-2

CONTRACTOR INSPECTIONS -

- DAILY INSPECTIONS** – INSPECTIONS SHOULD BE CONDUCTED AT LEAST DAILY, BUT MORE FREQUENTLY IF NECESSARY TO ENSURE COFORMANCE WITH THE CSPP. DAILY INSPECTIONS MUST BE COMPLETED TO ASSURE ALL TRAFFIC CONTROL DEVICES ARE IN PROPER LOCATION AND WORKING ORDER.
- INTERIM INSPECTIONS** - PRIOR TO OPENING WORK AREAS AND PAVEMENT TO AIRCRAFT OPERATIONS THE CONTRACTOR MUST COORDINATE WITH AIRPORT MANAGER FOR INSPECTION OF WORK AREA. PAVEMENTS MUST BE FREE OF ALL DIRT, SAND, GRAVE, WIRE BRISTLES OR ANY OTHER OBJECTS THAT COULD CAUSE DAMAGE TO AIRCRAFT ENGINES. ALL SOIL AREAS MUST BE FREE OF DIRT CLOUDS, RUTS, OR SURFACE IRREGULARITIES THAT COULD DAMAGE AN AIRCRAFT SHOULD IT LEAVE THE PAVEMENT.
- FINAL INSPECTION** - A FINAL INSPECTION WILL BE PERFORMED AT SUBSTANTIAL COMPLETION TO ENSURE FINAL COMPLIANCE WITH PLANS AND SPECIFICATIONS AS WELL AS ENSURE THAT ALL AIRFIELD PAVEMENT AND SAFETY AREA ARE READY TO BE RE-OPENED FOR USE.

2.15 UNDERGROUND UTILITIES

AN ATTEMPT HAS BEEN MADE TO LOCATE ALL UTILITIES ON THE PLANS; HOWEVER, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND IDENTIFICATION OF ALL EXISTING UTILITIES AND PIPELINES IN THE CONSTRUCTION AREA PRIOR TO ANY TYPE OF WORK. THE CONTRACTOR SHALL MAINTAIN THE UTILITY LOCATION MARKING UNTIL THEY ARE NO LONGER NECESSARY. ANY DAMAGE TO EXISTING UTILITIES OR PIPELINES (ON OR OFF AIRPORT PROPERTY) DAMAGED BY CONTRACTOR SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL REPAIR ALL UTILITY/PIPELINE DAMAGED BY CONTRACTOR AT NO ADDITIONAL COST TO OWNER.

ANY LIGHTING CABLES DAMAGED DURING CONSTRUCTION WILL BE REPLACED BY CONTRACTOR. CONTRACTOR SHALL PAY ALL COSTS ASSOCIATED WITH THE REPAIR OF DAMAGED CABLES AT NO COST TO OWNER.

CONTRACTOR SHALL PROTECT EXISTING RUNWAY AND TAXIWAY LIGHTING SYSTEMS AND APPROACH LIGHTING SYSTEMS AND SHALL REPAIR ANY DAMAGED SYSTEMS AT NO COST TO THE OWNER.

ANY UNPLANNED, UNAPPROVED OR ACCIDENTAL SHUTDOWN OR INTERRUPTION OF SERVICE TO ANY LIGHTING CIRCUIT OR NAVIGATIONAL AID REQUIRES IMMEDIATE NOTIFICATION OF THE OWNER BY THE CONTRACTOR. ALL NECESSARY REPAIRS WILL BE AT CONTRACTORS EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING AND GRADE CONTROL OF ALL ELEMENTS OF THE PROJECT AT NO ADDITIONAL COST TO THE OWNER.

2.16 PENALTIES

CONSTRUCTION SUSPENSION -

- AIRPORT MANAGER WILL IMMEDIATELY SUSPEND ALL CONSTRUCTION IF AND WHEN:
 - A CONTRACTOR OR SUBCONTRACTOR EMPLOYEE ENTERS THE AIR OPERATIONS AREA (AOA) OUTSIDE OF THE DESIGNATED WORK AREA.
 - ANY UNESCORTED CONSTRUCTION VEHICLE OPERATES ON ANY ACTIVE AOA SURFACE.
- AIRPORT MANAGER WILL ALLOW CONSTRUCTION WORK TO RESUME ONLY WHEN THE DISCREPANCY IS CORRECTED TO HER SATISFACTION.
- EXPULSION OF NON-COMPLIANT EMPLOYEES -
 - AIRPORT MANAGER MAY PERMANENTLY PROHIBIT ANY CONSULTANT OR CONTRACT EMPLOYEE ACTING IN VIOLATION WITH AIRPORT RULES AND REGULATIONS FROM ENTERING OR WORKING ON AIRPORT PROPERTY.

2.17 SPECIAL CONDITIONS

AIRCRAFT IN DISTRESS - AIRPORT MANAGER, THE RESIDENT ENGINEER, AND/OR THE CONTRACTOR SUPERINTENDENT WILL IMMEDIATELY CLEAR ALL CONSTRUCTION PERSONNEL OF ALL RUNWAYS AND APPROACH AREAS UPON MONITORING A DISTRESS CALL ON CTAF.

AIRCRAFT ACCIDENT - ALL CONSTRUCTION PERSONNEL WILL IMMEDIATELY VACATE AIRPORT PROPERTY AND REMAIN OFF UNTIL CLEARED BY THE DIRECTOR OF OPERATIONS (OR EQUIVALENT).

VEHICLE / PEDESTRIAN DEVIATION (V/PD) - AIRPORT MANAGER MAY TEMPORARILY SUSPEND CONSTRUCTION ON THE AIR OPERATIONS AREA (AOA) IN THE EVENT OF A NON-CONSTRUCTION RELATED V/PD.

2.18 RUNWAY AND TAXIWAY VISUAL AIDS

GENERAL - AIRPORT MARKINGS, LIGHTING, SIGNS, AND VISUAL NAVAIDS MUST BE CLEARLY VISIBLE TO PILOTS, NOT MISLEADING, CONFUSING, OR DECEPTIVE. ALL MUST BE SECURED IN PLACE TO PREVENT MOVEMENT BY PROP WASH, JET BLAST, WING VORTICES, OR OTHER WIND CURRENTS AND CONSTRUCTED OF MATERIALS THAT WOULD MINIMIZE DAMAGE TO AN AIRCRAFT IN THE EVENT OF INADVERTENT CONTACT.

MARKINGS - MARKINGS MUST BE IN COMPLIANCE WITH THE STANDARDS OF AC 150/5340-1, STANDARDS FOR AIRPORT MARKINGS. RUNWAYS AND RUNWAY EXIT TAXIWAYS CLOSED TO AIRCRAFT OPERATIONS ARE MARKED WITH A YELLOW X.

TEMPORARILY CLOSED RUNWAYS – MARKERS TO BE LOCATED OVER EXISTING RUNWAY NUMERALS EXCEPT WHEN CONTRACTOR IS WORKING IN THE AREA. IN SUCH CASES, MARKER SHALL BE LOCATED JUST OFF THRESHOLD PAVEMENT ON PAVED BLAST PAD. (SEE DRAWING CSPP-3)

LIGHTING AND VISUAL NAVAIDS - LIGHTING MUST BE IN CONFORMANCE WITH AC 150/5340-30, DESIGN AND INSTALLATION DETAILS FOR AIRPORT VISUAL AIDS, AND AC 150/5345-50, SPECIFICATION FOR PORTABLE RUNWAY AND TAXIWAY LIGHTS. WHEN DISCONNECTING RUNWAY AND TAXIWAY LIGHTING FIXTURES, DISCONNECT THE ASSOCIATED ISOLATION TRANSFORMERS. ALTERNATELY, COVER

FIXTURES BECAUSE AN EXCESSIVE NUMBER OF ISOLATION TRANSFORMERS WITH OPEN SECONDARIES MAY DAMAGE THE REGULATORS AND/OR INCREASE THE CURRENT ABOVE ITS NORMAL VALUE. SECURE, IDENTIFY, AND PLACE ANY ABOVE GROUND TEMPORARY WIRING IN CONDUIT TO PREVENT ELECTROCUTION AND FIRE IGNITION SOURCES.

2.19 MARKING AND SIGNS FOR ACCESS ROUTES

NO PAVEMENT MARKING OR SIGN INSTALLATION ON AIRPORT ACCESS ROUTES IS ANTICIPATED AS PART OF THE PROJECT.

2.20 HAZARD MARKING, LIGHTING AND SIGNING

AREAS IMPACTED BY CONSTRUCTION OPERATIONS – RUNWAY 5-23, TAXIWAY ‘A’, TAXIWAY ‘A2’, TAXIWAY ‘A3’ AND RUNWAY 17-35 ARE GOING TO BE IMPACTED IN THIS PROJECT.

EQUIPMENT -

- TYPE I (LOW) BARRICADES (AIRFIELD)** - 10" WIDE BY 10" HIGH BY 8' LONG HIGH IMPACT UV-RESISTANT POLYETHYLENE AND TWO BATTERY POWERED FLASHING OR STEADY-BURN RED LIGHTS. OVERALL BARRICADE HEIGHT SHALL NOT EXCEED 36". (SEE DRAWING CSPP-3).
- AIR OPERATIONS AREA** - USE HIGHLY REFLECTIVE BARRICADES WITH LIGHTS TO CLOSE PROJECT AREA. EVALUATE ALL OPERATING FACTORS WHEN DETERMINING HOW TO MARK TEMPORARY CLOSURES THAT CAN LAST FROM 10 TO 15 MINUTES TO A MUCH LONGER PERIOD OF TIME. GENERALLY, PLACE TYPE I BARRICADES A MAXIMUM OF 20 FEET ON CENTERS AND NOT LESS THAN THREE (3) PER TAXIWAY. ANCHOR BARRICADES WITH SANDBAGS OR OTHER METHODS APPROVED BY ENGINEER. MAINTAIN TEMPORARY BARRICADES UNTIL REMOVAL IS DIRECTED BY ENGINEER.
- MAINTENANCE** - THE CONSTRUCTION SPECIFICATIONS MUST INCLUDE A PROVISION REQUIRING THE CONTRACTOR TO HAVE A PERSON ON CALL 24 HOURS A DAY FOR EMERGENCY MAINTENANCE OF AIRPORT HAZARD LIGHTING AND BARRICADES. THE CONTRACTOR MUST FILE THE CONTACT PERSON'S INFORMATION WITH THE AIRPORT OPERATOR. THE CONTRACTOR SHALL CHECK THE BARRICADE FLASHER BATTERIES DAILY TO ENSURE THAT FLASHERS ARE OPERATIONAL. THE CONTRACTOR SHALL REPLACE BATTERIES AS REQUIRED IN ORDER TO HAVE THE FLASHERS WORKING ON A DAILY BASIS.

2.21 WORK ZONE LIGHTING FOR NIGHT-TIME CONSTRUCTION – NIGHT-TIME WORK WILL BE ALLOWED.

2.22 PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS, ZONES AND SURFACES

GENERAL - SAFETY AREA ENCROACHMENTS, IMPROPER GROUND VEHICLE OPERATIONS AND UNMARKED OR UNCOVERED HOLES AND TRENCHES IN THE VICINITY OF AIRCRAFT OPERATION SURFACES AND CONSTRUCTION AREAS ARE THE THREE MOST RECURRING THREATS TO SAFETY DURING CONSTRUCTION. PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS, OBJECT FREE AREAS, OBSTACLE FREE ZONES, AND APPROACH/DEPARTURE SURFACES SHALL BE A STANDING REQUIREMENT FOR THE DURATION OF CONSTRUCTION OPERATIONS.

RUNWAY SAFETY AREA (RSA) - A RUNWAY SAFETY AREA IS THE DEFINED SURFACE SURROUNDING THE RUNWAY PREPARED OR SUITABLE FOR REDUCING THE RISK OF DAMAGE TO AIRPLANES IN THE EVENT OF AN UNDERSHOOT, OVERSHOOT, OR EXCURSION FROM THE RUNWAY BY AIRCRAFT. NO CONSTRUCTION MAY OCCUR WITHIN THE EXISTING RSA WHEN RUNWAY 5-23 AND RUNWAY 17-35 IS OPEN FOR OPERATIONS.

RUNWAY 5-23			
ADG/ADG RUNWAY	RSA DISTANCE FROM CENTERLINE (FT) RSA HOLD LINE (FT)	RSA WIDTH (FT)	RSA LENGTH FROM END OF RUNWAY (FT)
C-II	250; 200	500	1,000
RUNWAY 17-35			
ADG/ADG RUNWAY	RSA DISTANCE FROM CENTERLINE (FT) RSA HOLD LINE (FT)	RSA WIDTH (FT)	RSA LENGTH FROM END OF RUNWAY (FT)
B-II	75; 200	150	300

RUNWAY OBJECT FREE AREA (ROFA) - NO CONSTRUCTION MAY OCCUR WITHIN THE EXISTING ROFA.

RUNWAY 5-23			
ADG/ADG RUNWAY	ROFA DISTANCE FROM CENTERLINE (FT)	ROFA WIDTH (FT)	ROFA LENGTH FROM END OF RUNWAY (FT)
C-II	200	800	1,000
RUNWAY 17-35			
ADG/ADG RUNWAY	ROFA DISTANCE FROM CENTERLINE (FT)	ROFA WIDTH (FT)	ROFA LENGTH FROM END OF RUNWAY (FT)
B-II	200	500	300

TAXIWAY SAFETY AREA (TSA) - THE TAXIWAY SAFETY AREA IS A DEFINED SURFACE ALONGSIDE THE TAXIWAY PREPARED OR SUITABLE FOR REDUCING THE RISK OF DAMAGE TO AN AIRPLANE UNINTENTIONALLY DEPARTING THE TAXIWAY. NO CONSTRUCTION MAY OCCUR WITHIN THE TSA WHILE THE TAXIWAY IS OPEN FOR AIRCRAFT OPERATIONS.

TDG - TAXIWAY	TSA DISTANCE FROM CENTERLINE (FT)	TSA WIDTH (FT)
B-II - ALL	39.5	79

- OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN THE TSA WHILE THE TAXIWAY IS OPEN. THE CONTRACTOR MUST BACKFILL TRENCHES BEFORE THE TAXIWAY IS OPENED. COVERINGS ARE NOT ALLOWED IN TAXIWAY SAFETY AREAS.

- AFTER THE TAXIWAY HAS BEEN CLOSED, CONTRACTORS MUST PROMINENTLY MARK OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE WITH RED OR ORANGE FLAGS, AS APPROVED BY THE AIRPORT OPERATOR, AND LIGHT THEM WITH RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS.

- SOIL EROSION MUST BE CONTROLLED TO MAINTAIN TSA STANDARDS, THAT IS, THE TSA MUST BE CLEARED AND GRADED AND HAVE NO POTENTIALLY HAZARDOUS RUTS, HUMPS, DEPRESSIONS, OR OTHER SURFACE VARIATIONS, AND CAPABLE, UNDER DRY CONDITIONS, OF SUPPORTING SNOW REMOVAL EQUIPMENT, AIRCRAFT RESCUE AND FIREFIGHTING EQUIPMENT, AND THE OCCASIONAL PASSAGE OF AIRCRAFT WITHOUT CAUSING STRUCTURAL DAMAGE TO THE AIRCRAFT.

TAXIWAY AND TAXILANE OBJECT FREE AREA (TOFA) - UNLIKE THE RUNWAY OBJECT FREE AREA, AIRCRAFT WINGS REGULARLY PENETRATE THE TAXIWAY/TAXILANE OBJECT FREE AREA DURING NORMAL OPERATIONS. THUS, THE RESTRICTIONS ARE MORE STRINGENT. NO CONSTRUCTION EQUIPMENT MAY BE PARKED WITHIN THE TOFA WHILE THE TAXIWAY/TAXILANE IS OPEN FOR AIRCRAFT OPERATIONS.

TDG - TAXIWAY	TOFA DISTANCE FROM CENTERLINE (FT)	TOFA WIDTH (FT)
B-II - ALL	62	131

TDG - TAXIWAY	TLOFA DISTANCE FROM CENTERLINE (FT)	TOFA WIDTH (FT)
B-II - ALL	57.5	115

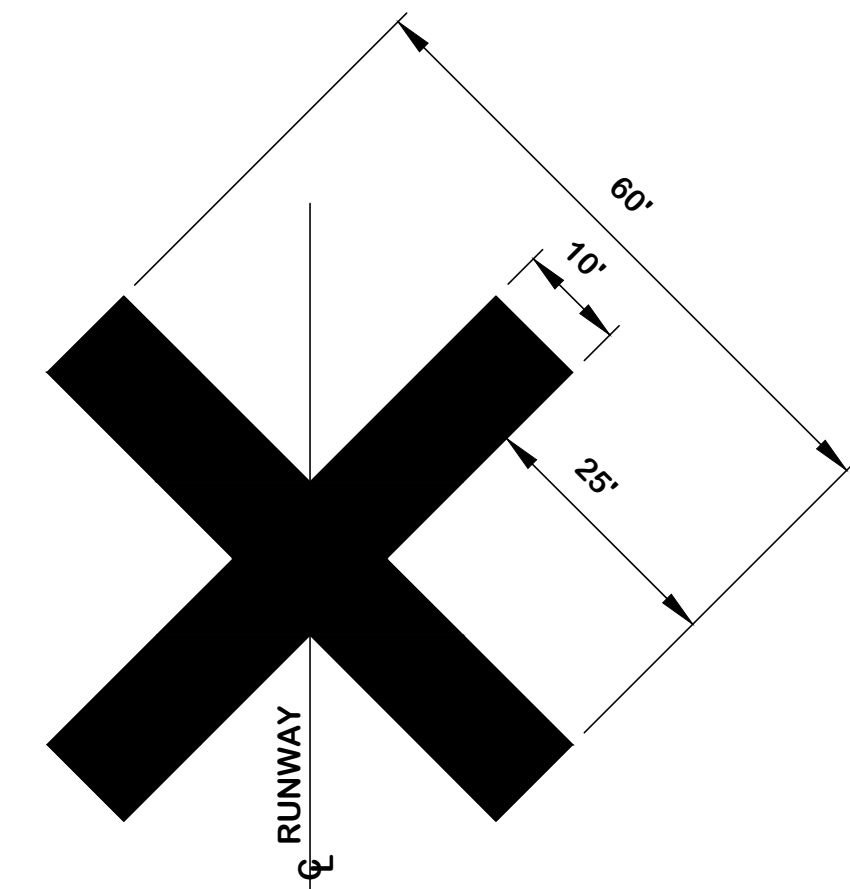
OBSTACLE FREE ZONE (OFZ) - CONSTRUCTION PERSONNEL, MATERIAL, AND/OR EQUIPMENT MAY NOT PENETRATE THE OFZ WHILE THE RUNWAY IS OPEN FOR AIRCRAFT OPERATIONS. THE OFZ IS A DEFINED VOLUME OF AIRSPACE CENTERED ABOUT AND ABOVE THE RUNWAY CENTERLINE.

APPROACH/DEPARTURE SURFACES - ALL PERSONNEL, MATERIALS, AND/OR EQUIPMENT MUST REMAIN CLEAR OF THE APPLICABLE THRESHOLD SITING SURFACES. OBJECTS THAT DO NOT PENETRATE THESE SURFACES MAY STILL BE OBSTRUCTIONS TO AIR NAVIGATION AND MAY AFFECT STANDARD INSTRUMENT APPROACH PROCEDURES. COORDINATE WITH THE FAA THROUGH THE APPROPRIATE FAA AIRPORTS REGIONAL OR DISTRICT OFFICE. NO CONSTRUCTION ACTIVITY MAY OCCUR IN THE RUNWAY APPROACH/DEPARTURE AREA.

2.23. OTHER LIMITS ON CONSTRUCTION

PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE THE CONSULTANT WITH SPECIFIC EQUIPMENT HEIGHTS IN ORDER TO COMPLETE FAA FORM 7460-1 AND SUBMIT FOR FAA AIRSPACE REVIEW. A DETERMINATION LETTER FROM THE FAA ACKNOWLEDGING EQUIPMENT HEIGHTS WITHIN THE PROJECT AREA IS REQUIRED BEFORE CONSTRUCTION CAN COMMENCE.

- PROHIBITIONS** - THE FOLLOWING PROHIBITIONS ARE IN EFFECT FOR THE DURATION OF THIS PROJECT:
 - NO USE OF OPEN FLAME WELDING OR TORCHES UNLESS FIRE SAFETY PRECAUTIONS ARE PROVIDED AND THE AIRPORT OPERATOR HAS APPROVED THEIR USE.
 - NO USE OF ELECTRICAL BLASTING CAPS OR EXPLOSIVES OF ANY KIND ON OR WITHIN 1,000 FT OF THE AIRPORT PROPERTY.
 - NO USE OF FLARE POTS WITHIN THE AOA.
- RESTRICTIONS** -
 - CONSTRUCTION SUSPENSION REQUIRED DURING SPECIFIC AIRPORT OPERATIONS - N.A.
 - AREAS THAT CANNOT BE WORKED ON SIMULTANEOUSLY - PHASE 1 WORK MUST BE COMPLETED IN ITS ENTIRETY BEFORE PHASE 2 WORK MAY BEGIN.

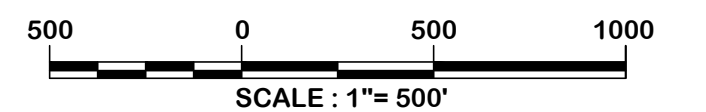


NOTE:

CLOSED RUNWAY MARKERS SHALL BE AVIATION YELLOW, HIGH STRENGTH VINYL MATERIAL WITH BALLAST TUBES FOR ANCHORING. CONTRACTOR SHALL PLACE AND SECURE THE MARKERS OVER RUNWAY NUMERALS OR AT THE RUNWAY ENDS. IF THE MARKERS ARE PLACED AT THE RUNWAY ENDS, THE NUMBERS SHALL BE OBLITERATED. THERE SHALL BE NO MEASUREMENT OR PAYMENT FOR THIS ITEM.

CLOSED RUNWAY MARKER

N.T.S.
PROVIDED BY OWNER



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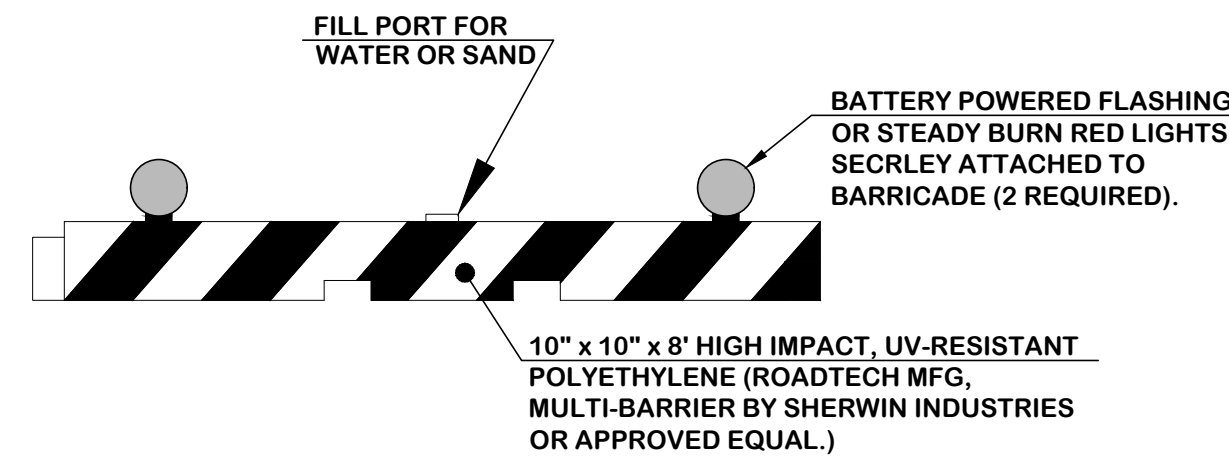
Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
CONSTRUCTION SAFETY AND PHASING PLAN NO. 3

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: N.T.S.	Sheet Number: -
Drawing Number: CSPP-3	

TYPE I LOW PROFILE BARRICADE DETAIL (AIRFIELD)

N.T.S.
PROVIDED BY THE CONTRACTOR - SEE SPECIFICATION 01530.

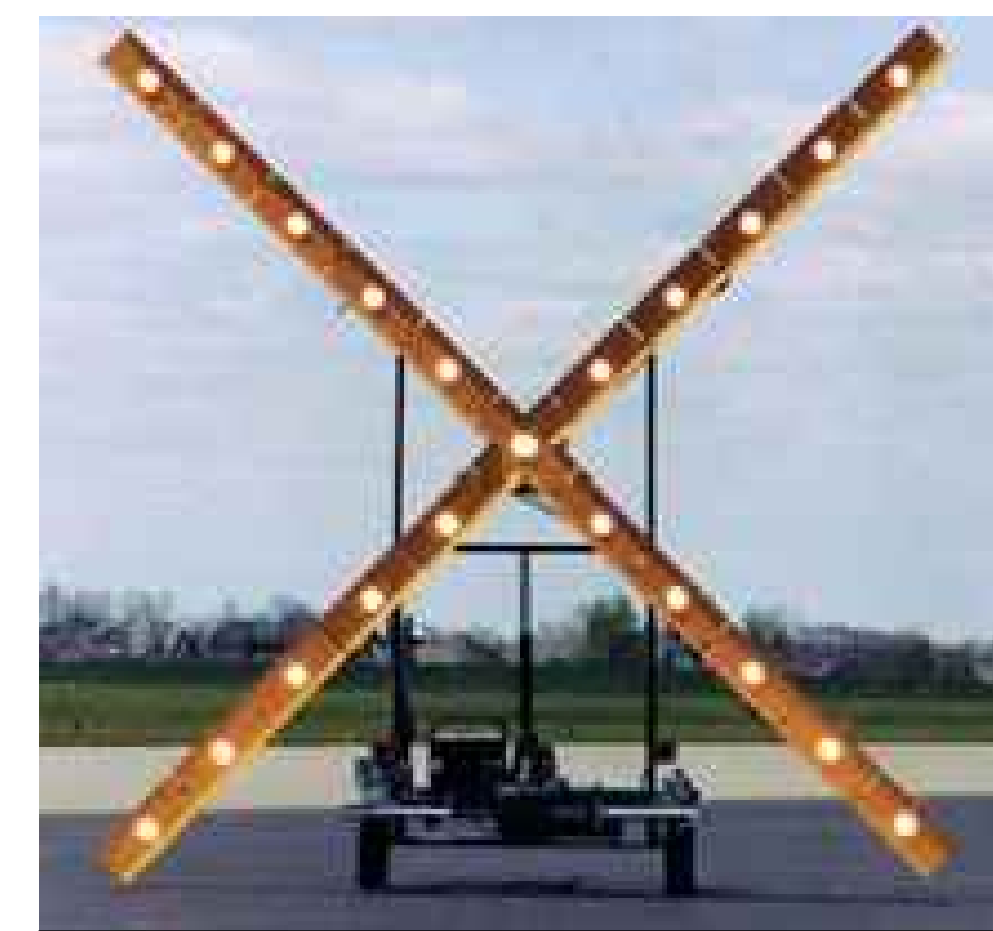


NOTE:

BARRICADES SHALL BE LOCATED 20' EDGE TO EDGE (MAXIMUM) TYPICAL. CONTRACTOR SHALL UTILIZE A MINIMUM OF THREE BARRICADES PER TAXIWAY UNLESS DIRECTED OTHERWISE BY THE ENGINEER. THE CONTRACTOR HAS TO PROVIDE 10 SAND BAGS FOR THE BARRICADES.

LIGHTED CLOSED RUNWAY MARKER DETAIL

N.T.S.
PROVIDED BY CONTRACTOR - SPECIFICATION 01530.



MARKERS TO BE LOCATED OVER EXISTING RUNWAY NUMERALS EXCEPT WHEN CONTRACTOR IS WORKING IN THE AREA. IN SUCH CASES MARKER SHALL BE LOCATED JUST OFF THRESHOLD PAVEMENT.

PROJECT GENERAL NOTES

- EACH CONTRACTOR SHALL OBTAIN, HAVE KNOWLEDGE OF, AND INCORPORATE THE FOLLOWING SAFETY PROVISIONS INTO THE CONSTRUCTION PROJECT:
 - OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION - AC 150/5370-2G.
 - AIRPORT SAFETY SELF-INSPECTION - AC 150/5200-18D.
 - STANDARDS FOR AIRPORT MARKINGS - AC 150/5340-1M.
 - PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AIRPORTS - AC 150/5210-5D.
- FOR OTHER CONDITIONS RELATING TO SAFETY, SEE SPECIFICATIONS.
- NOTICE TO AIRMEN (NOTAMS) - THE CONTRACTOR WILL PROVIDE THE NECESSARY INFORMATION ON CONSTRUCTION CONDITIONS TO THE OWNER MINIMUM OF SEVEN (7) DAYS PRIOR SO THAT NOTAMS CAN BE ISSUED IN ACCORDANCE WITH ESTABLISHED CRITERIA BY THE OWNER TO THE F.A.A. FLIGHT SERVICE STATION.
- ALL NON-PAVED AREAS OUTSIDE LIMITS OF CONSTRUCTION AND DESIGNATED STAGING AREA THAT ARE DISTURBED BY THE CONTRACTOR'S WORK, HAUL ROADS, ETC. SHALL BE RESEEDD AND RESTORED TO ORIGINAL CONDITION BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. THERE WILL BE NO SEPARATE PAY ITEM FOR THIS WORK.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR THE STORAGE AND SECURITY OF HIS MATERIAL AND EQUIPMENT AND SHALL ERECT STORAGE FACILITIES AND FENCING AS NECESSARY. THE CONTRACTOR'S STORAGE AND STAGING AREA SHALL BE IN THE LOCATION SHOWN ON DRAWING PL-1. THE STAGING AREA SHALL BE COVERED WITH 6" MINIMUM OF SCDOT GRADED AGGREGATE BASE COURSE OR NO. 57 STONE AT THE BEGINNING OF THE PROJECT. THE COST OF THIS WORK SHALL BE CONSIDERED INCIDENTAL TO MOBILIZATION. THE STAGING AREA SHALL BE RETURNED TO ORIGINAL CONDITION AT THE CONCLUSION OF THE PROJECT AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR WILL NOT BE ALLOWED TO USE ANY OF THE EXISTING RUNWAYS OR TAXIWAYS AS PART OF THE HAUL ROAD UNLESS SPECIFICALLY AUTHORIZED BY THE ENGINEER.
- ALL CONSTRUCTION TRAFFIC SHALL ENTER AND EXIT THE PROJECT AREA THROUGH THE PROJECT ACCESS ROUTES AS SHOWN ON DRAWING PL-1.
- CONSTRUCTION STAKE-OUT SHALL BE PERFORMED BY CONTRACTOR IN ACCORDANCE WITH ARTICLE 50-06 OF THE SPECIFICATIONS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE ALL MEASUREMENTS THAT MAY BE REQUIRED TO LAY OUT THE CONSTRUCTION. THE COST OF STAKING WILL NOT BE PAID FOR DIRECTLY AND WILL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS ITEMS OF WORK.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND IDENTIFICATION OF ALL EXISTING UTILITIES AND UNDERGROUND PIPELINES IN CONSTRUCTION AREA. ANY DAMAGES TO EXISTING UTILITIES OR UNDERGROUND PIPELINES ON OR OFF AIRPORT PROPERTY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ALL REPAIR WORK SHALL MEET THE APPROVAL OF THE OWNER OF THE DAMAGED UTILITY. NO REIMBURSEMENT WILL BE ALLOWED FOR UTILITY/PIPE REPAIR OR REPLACEMENT. CONTRACTOR SHALL EXERCISE EXTREME CAUTION OVER AND AROUND THE EXISTING FAA AND AIRFIELD ELECTRICAL CABLES THAT WILL BE FLAGGED PRIOR TO THE START OF PRODUCTION.
- CONTRACTOR SHALL MAINTAIN RADIO COMMUNICATION WITH THE UNICOM GROUND AT ALL TIMES DURING CONSTRUCTION (122.8 MHZ). AIRPORT MANAGER FOR CONTACT IS BERT DUFFIE (843)-549-2549. CONTRACTOR WILL HAVE A WORKING RADIO ON SITE AT ALL TIMES DURING CONSTRUCTION AND SHALL ASSIGN RESPONSIBLE PERSONNEL TO CONTINUOUSLY MONITOR THE APPROPRIATE FREQUENCY. THE CONTRACTOR WILL ALSO PROVIDE THE RESIDENT INSPECTOR WITH APPROVED RADIOS FOR USE DURING THE DURATION OF CONSTRUCTION. THE RADIOS WILL REMAIN THE PROPERTY OF THE CONTRACTOR WHEN THE PROJECT IS COMPLETE.
- ALL VEHICLES USED ON THE AIRFIELD SHALL MEET FAA REQUIREMENTS FOR MARKINGS AND LIGHTING. (SEE AC 150/5210-5D).
- THE CONTRACTOR WILL BE RESPONSIBLE FOR THE CLEANUP AND DISPOSAL OF ALL TRASH AND DEBRIS CREATED BY HIS WORK OR PERSONNEL. ALL TRASH AND DEBRIS MUST BE DISPOSED OF OFFSITE.
- CONTRACTOR WILL PROTECT ALL EXISTING RUNWAY AND TAXIWAY LIGHTS IN THE VICINITY OF THE WORK AREAS THAT ARE DESIGNATED TO REMAIN AND ANY DAMAGE WILL BE REPAIRED AT THE EXPENSE OF THE CONTRACTOR.
- CONTRACTOR WILL PROVIDE SUFFICIENT BARRICADES ON SITE TO BLOCK OFF AREAS IN WHICH CURRENT WORK IS TAKING PLACE. COORDINATION WITH THE OWNER AND ENGINEER WILL BE NECESSARY IN ORDER TO PLAN DAILY CONSTRUCTION AND GROUND ACTIVITY AND WILL BE REQUIRED PRIOR TO COMMENCING WORK IN EACH SUCCESSIVE PHASE.
- IN THE EVENT THE CONTRACTOR SERVICES HIS EQUIPMENT ON AIRPORT PROPERTY, ALL OIL AND FLUIDS REMOVED FROM THE EQUIPMENT MUST BE COLLECTED AND DISPOSED OF IN ACCORDANCE WITH THE LOCAL, STATE AND FEDERAL ENVIRONMENTAL LAWS. IF A HAZARDOUS, OR REGULATED MATERIAL IS SPILLED, IT MUST BE PROMPTLY REPORTED TO THE AIRPORT AND CLEANED UP BY THE CONTRACTOR AT HIS EXPENSE.
- CONTRACTOR SHALL VISIT THE SITE TO DETERMINE EXISTING CONDITIONS PRIOR TO SUBMITTING BID.
- PRIOR TO DIGGING ANY TRENCHES, CONTRACTOR SHALL NOTIFY ALL UTILITIES (ELECTRIC, GAS, TELEPHONE, WATER, SEWER) AND OBTAIN LOCATIONS OF UNDERGROUND UTILITIES.
- ANY DAMAGES DONE TO AIRPORT PROPERTY OR UTILITIES (SUCH AS RUNWAY, TAXIWAYS, APRONS, FENCING, EXISTING AIRPORT AND/OR FAA CABLES) WILL BE REPAIRED BY THE CONTRACTOR TO THE APPROVAL OF THE OWNER OF THE FACILITY IN A SATISFACTORY MANNER. THE CONTRACTOR WILL BEAR ALL COSTS FOR REPAIRS.
- APPROVED CUTS IN PAVEMENT OR CONCRETE SHALL BE MADE USING A PAVEMENT SAW, AND SHALL BE PATCHED TO MATCH THE EXISTING SURFACE IN A MANNER APPROVED BY THE ENGINEER.
- EXISTING EASEMENTS TO OTHER PROPERTIES SHALL BE MAINTAINED AT ALL TIMES.
- THE EXISTING AIRPORT PAVEMENTS, ACCESS ROADS, AND HAUL ROUTES MAY NOT BE CAPABLE OF SUPPORTING CERTAIN TYPES OF CONSTRUCTION EQUIPMENT. PRIOR TO BIDDING, THE CONTRACTOR SHALL FULLY SATISFY HIMSELF AS TO THE ABILITY OF THE EXISTING AIRPORT PAVEMENTS TO SATISFACTORILY SUSTAIN THE TYPE OF EQUIPMENT HE PLANS TO USE. CONTRACTOR SHALL SIZE THE EQUIPMENT USED FOR CONSTRUCTION ACCORDINGLY. ANY DAMAGE CAUSED BY HAULING OR ANY OTHER CONSTRUCTION ACTIVITY TO EXISTING PAVEMENT, EXCLUDING SPECIFIED REPAIR WORK, SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- AREAS OUTSIDE THE PROJECT LIMITS ARE DESIGNATED AS RESTRICTED AREAS. THE CONTRACTOR'S FORCES ARE PROHIBITED FROM ENTERING RESTRICTED AREAS AT ANY TIME, UNLESS SPECIFICALLY AUTHORIZED BY THE ENGINEER OR AIRPORT OPERATIONS DEPARTMENT.
- THE ENGINEER SHALL DESIGNATE AREAS TO BE USED BY THE CONTRACTOR FOR THE PARKING OF CONSTRUCTION EQUIPMENT AND VEHICLES WHEN NOT ENGAGED IN THE CONSTRUCTION DURING NON-WORKING DAYS AND NIGHTS AS WELL AS AREAS FOR CONTRACTOR'S EMPLOYEES AUTO PARKING.

- RUNWAY, TAXIWAY, AND EXISTING APRONS SHALL BE KEPT FREE OF ALL DEBRIS, DIRT, ETC., AT ALL TIMES. ANY SPILLAGE OF EXCAVATION OR OTHER MATERIAL SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR WITH A MOTOR DRIVEN SWEEPER OR VACUUM AS REQUIRED BY THE ENGINEER. A PROGRAM OF REGULAR RUNWAY, TAXIWAY AND APRON INSPECTION WILL BE PLANNED BY THE CONTRACTOR, THE AIRPORT OPERATIONS DEPARTMENT AND THE ENGINEER.
- ALL ACTIVE AIRPORT OPERATIONAL AREAS WHICH ARE ADJACENT TO A CONSTRUCTION WORK AREA SHALL BE SEPARATED BY BARRIERS WITH FLASHING OR STEADY RED LIGHTS. NO CONSTRUCTION TRAFFIC WILL CROSS ACTIVE AIRPORT OPERATIONAL AREAS. THE CONTRACTOR SHALL PROVIDE AND INSTALL THE BARRIERS.
- ANY UNPLANNED, UNAPPROVED, OR ACCIDENTAL SHUTDOWN OR INTERRUPTION OF SERVICE TO ANY LIGHTING CIRCUIT OR NAVIGATIONAL AID REQUIRES IMMEDIATE NOTIFICATION TO THE AIRPORT MANAGER AND THE ENGINEER AND SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR. THE COST OF MATERIALS AND LABOR REQUIRED TO REPAIR THE LIGHTING CIRCUIT SHALL BE BORNE BY THE CONTRACTOR.

STAKING AND GRADING PLAN NOTES

- ALL SURVEY INFORMATION IS BASED ON STATE PLANE COORDINATES AND USGS 1983 HORIZONTAL DATUM AND 1988 VERTICAL DATUM.
- STAKING AND GRADING INFORMATION SHOWN IS TO ASSIST CONTRACTOR IN PROPERLY PRICING HIS BID FOR THIS PROJECT. ACTUAL CAD FILE INFORMATION WILL BE SUPPLIED TO THE SELECTED CONTRACTOR PRIOR TO CONSTRUCTION TO ASSIST IN CONSTRUCTION STAKING AND ELEVATION CONTROL.
- DIMENSIONS SHOWN REFERENCE EDGE OF PAVEMENT.
- SPOT ELEVATIONS SHOWN REFERENCE, EDGE OF PAVEMENT, BOTTOM OF WALL, FACE OF BUILDING OR CONCRETE SLAB CORNERS.
- SEE APPLICABLE PAVING SPECIFICATIONS FOR GRADE QUALITY CONTROL REQUIREMENTS THAT WILL BE ENFORCED ON THIS PROJECT.

DEMOLITION NOTES

- THE EXISTING INFORMATION AND CONDITION STATUS PROVIDED IN THESE PLANS IS SOLELY TO ASSIST THE CONTRACTOR IN ASSESSING THE NATURE AND EXTENT OF CONDITIONS WHICH MAY BE ENCOUNTERED DURING THE COURSE OF WORK. ALL CONTRACTORS ARE DIRECTED PRIOR TO BIDDING, TO CONDUCT WHATEVER INVESTIGATIONS THEY MAY DEEM NECESSARY TO ARRIVE AT THEIR OWN CONCLUSION REGARDING THE ACTUAL CONDITIONS THAT WILL BE ENCOUNTERED, AND UPON WHICH THEIR BIDS WILL BE BASED.
- EXISTING UTILITIES SHOWN ARE TAKEN FROM FIELD SURVEYS, UTILITY LOCATION SERVICE FIELD MARKED LOCATIONS AND AIRPORT AS-BUILT DRAWINGS. HOWEVER, THESE LOCATIONS ARE NOT TO BE CONSIDERED ALL-INCLUSIVE. THE CONTRACTOR SHALL VERIFY EXACT LOCATION, CHARACTER AND NATURE OF ALL EXISTING AND PROPOSED UTILITIES PRIOR TO BEGINNING CONSTRUCTION.
- THE CONTRACTOR SHALL MAKE EXPLORATORY EXCAVATIONS AT ALL INTERSECTIONS OF PROPOSED WORK AND EXISTING UTILITIES. THE EXPLORATORY EXCAVATIONS SHALL BE MADE AT LEAST FORTY-EIGHT (48) HOURS IN ADVANCE OF THE WORK. IF THERE IS A POTENTIAL CONFLICT, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY. INFORMATION ON THE CONFLICT SHALL BE FURNISHED BY THE CONTRACTOR AND SHALL INCLUDE LOCATIONS, ELEVATIONS, UTILITY TYPE, MATERIAL AND SIZE.
- DEMOLITION OF ALL ITEMS SHALL COMPLY WITH THE REQUIREMENTS OF THE PHASING PLAN.
- THE CONTRACTOR IS RESPONSIBLE FOR UNCOVERING AND SUPPORTING EXISTING UTILITIES. CARE SHALL BE TAKEN SO AS NOT TO INTERRUPT SERVICE DURING CONSTRUCTION EXCEPT UPON APPROVAL OF THE ENGINEER AND THE UTILITY OWNER AND WITH PROPER TENANT NOTIFICATION.
- SEE STAKING PLANS FOR ADDITIONAL INFORMATION REGARDING LOCATING DEMOLITION LIMITS.



WALTERBORO, SOUTH CAROLINA



INTERNATIONAL

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Columbia, South Carolina 29201



Designer: GBV/NWM	Checked by: ADB
Technician: NWM	Project Number: 192930

ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
GENERAL NOTES

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: N.T.S.	Sheet Number: GN-1

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SUMMARY OF QUANTITIES

BASE BID - ALTERNATE NO. 1

SUMMARY OF QUANTITIES - BASE BID: ALTERNATE #1					
Item No.	Spec. No.	Item Description	Unit	Estimated Quantity	Actual Quantity
1	01530	Airfield Barricades	L.S.	1	
2	C-100A	Contractor Quality Control Plan (CQCP) - Base Bid: Alternate #1	L.S.	1	
3	C-102A	Temporary Inlet Protection	Each	2	
4	C-102B	Temporary Silt Fence	L.F.	16,800	
5	C-105A	Mobilization - Base Bid: Alternate #1	L.S.	1	
6	P-101A	Asphalt Pavement Milling	S.Y.	76,530	
7	P-101B	Pavement Removal by Milling	S.Y.	14,130	
8	P-101C	Remove Abandoned Light Cans	Each	45	
9	P-101D	Medium Cracks Repair	L.F.	54,018	
10	P-401	FAA Asphalt Surface Course	TON	110	
11	P-403	SCDOT Type C Asphalt Surface Course	TON	1,320	
12	P-501A	6-Inch Thick Concrete Pavement	S.Y.	61,200	
13	P-501B	7-Inch Thick Concrete Pavement	S.Y.	5,600	
14	P-603	Emulsified Asphalt Tack Coat	GAL	849	
15	P-620A	Temporary Pavement Marking, White	S.F.	83,300	
16	P-620B	Permanent Reflective Pavement Marking, White	S.F.	83,300	
17	P-620C	Temporary Pavement Marking, Yellow	S.F.	3,392	
18	P-620D	Permanent Reflective Pavement Marking, Yellow	S.F.	3,392	
19	P-620E	Permanent Non-Reflective Pavement Marking, Black	S.F.	23,100	
20	P-620F	Obliterate Markings	S.F.	1,570.0	
21	P-620G	Pavement Surface Cleaning	S.F.	40,040	
22	P-621	Saw-Cut Grooves	S.Y.	56,020	
23	P-632	Asphalt Pavement Rejuvenation	S.Y.	1,500	
24	T-901	Seeding	Acre	4.0	
25	T-905B	Offsite Topsoil	C.Y.	1,770	
26	L-125D	Temporary Airfield Lighting - Complete	L.S.	1	

BASE BID - ALTERNATE NO. 2

SUMMARY OF QUANTITIES - BASE BID: ALTERNATE #2					
Item No.	Spec. No.	Item Description	Unit	Estimated Quantity	Actual Quantity
1	01530	Airfield Barricades	L.S.	1	
2	C-100B	Contractor Quality Control Plan (CQCP) - Base Bid: Alternate #2	L.S.	1	
3	C-102A	Temporary Inlet Protection	Each	2	
4	C-102B	Temporary Silt Fence	L.F.	16,800	
5	C-105B	Mobilization - Base Bid: Alternate #2	L.S.	1	
6	P-101A	Asphalt Pavement Milling	S.Y.	82,420	
7	P-101B	Pavement Removal by Milling	S.Y.	14,130	
8	P-101C	Remove Abandoned Light Cans	Each	45	
9	P-101D	Medium Cracks Repair	L.F.	54,018	
10	P-401	FAA Asphalt Surface Course	TON	18,510	
11	P-403	SCDOT Type C Asphalt Surface Course	TON	1,320	
12	P-403	SCDOT Type C Asphalt Surface Course	GAL	10,764	
13	P-620A	Temporary Pavement Marking, White	S.F.	83,300	
14	P-620B	Permanent Reflective Pavement Marking, White	S.F.	83,300	
15	P-620C	Temporary Pavement Marking, Yellow	S.F.	3,392	
16	P-620D	Permanent Reflective Pavement Marking, Yellow	S.F.	3,392	
17	P-620E	Permanent Non-Reflective Pavement Marking, Black	S.F.	380	
18	P-620F	Obliterate Markings	S.F.	1,570	
19	P-620G	Pavement Surface Cleaning	S.F.	40,040	
20	P-621	Saw-Cut Grooves	S.Y.	56,020	
21	P-632	Asphalt Pavement Rejuvenation	S.Y.	1,500	
22	T-901	Seeding	Acre	4.0	
23	T-905B	Offsite Topsoil	C.Y.	1,770	
24	L-125D	Temporary Airfield Lighting - Complete	L.S.	1	

ADDITIVE BID NO. 1

SUMMARY OF QUANTITIES - ADDITIVE BID #1					
Item No.	Spec. No.	Item Description	Unit	Estimated Quantity	Actual Quantity
1	01530	Airfield Barricades	L.S.	1	
2	C-100C	Contractor Quality Control Plan (CQCP) - Additive Bid #1	L.S.	1	
3	C-102A	Temporary Inlet Protection	Each	3	
4	C-102B	Temporary Silt Fence	L.F.	1,100	
5	C-105C	Mobilization - Additive Bid #1	L.S.	1	
6	P-101B	Pavement Removal by Milling	S.Y.	11,270	
7	P-152	Unclassified Excavation	C.Y.	370	
8	P-209	Crushed Aggregate Base Course	C.Y.	500	
9	P-401	FAA Asphalt Surface Course	TON	680	
10	P-602	Emulsified Asphalt Prime Coat	GAL	575	
11	P-403	SCDOT Type C Asphalt Surface Course	GAL	128	
12	P-620C	Temporary Pavement Marking, Yellow	S.F.	260	
13	P-620D	Permanent Reflective Pavement Marking, Yellow	S.F.	260	
14	P-620F	Obliterate Markings	S.F.	670	
15	T-901	Seeding	Acre	2.6	
16	T-905A	Onsite Topsoil	C.Y.	221	
17	T-905B	Offsite Topsoil	C.Y.	750	
18	L-108A	No. 8 AWG, 5kV, L-824C Cable, Installed in Trench, Duct or Conduit	L.F.	3,100	
19	L-108B	No. 6 AWG, Bare Solid Copper Counterpoise Wire, Installed in Trench or with Duct Bank or Conduit, Including Ground Rods and Ground Connections	L.F.	2,000	
20	L-108C	Cable Trench in Earth for Direct Buried Cables	L.F.	1,800	
21	L-125A	Relocate Existing Taxiway Light - Base Mounted	Each	5	
22	L-125B	Relocate Existing Taxiway Light - Stake Mounted	Each	24	
23	L-125C	L-858 LED, 3 MOD Guidance Sign on Concrete Foundation, Size 1	Each	6	
24	L-128A	Remove and Store Existing Taxiway Lights - Base Mounted	Each	7	
25	L-128B	Remove and Store Existing Taxiway Lights - Stake Mounted	Each	35	
26	L-128C	Remove and Store Existing Sign and Demolish Concrete Foundation	Each	1	

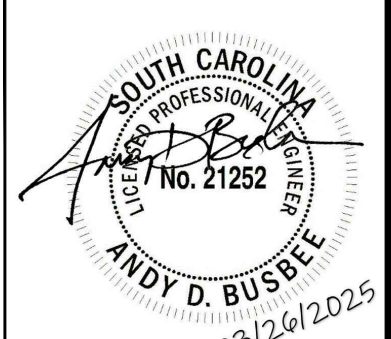


WALTERBORO, SOUTH CAROLINA

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Designer: GBV/LAW/NWM

Checked by: ADB

Technician:

Project Number: 192930

ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

RUNWAY 5-23
REHABILITATION

SUMMARY OF QUANTITIES

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: AIRPORTS
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Scale: N.T.S.	Sheet Number:
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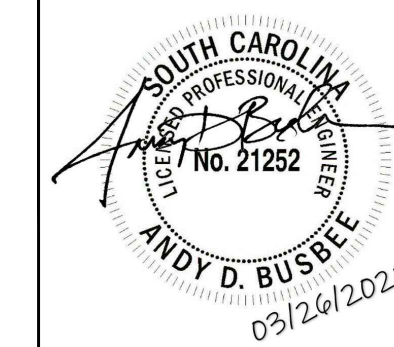
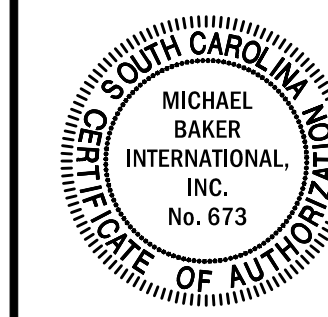
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WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

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Designer: GBV
Checked by: ADB
Technician: NWM
Project Number: 192930

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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

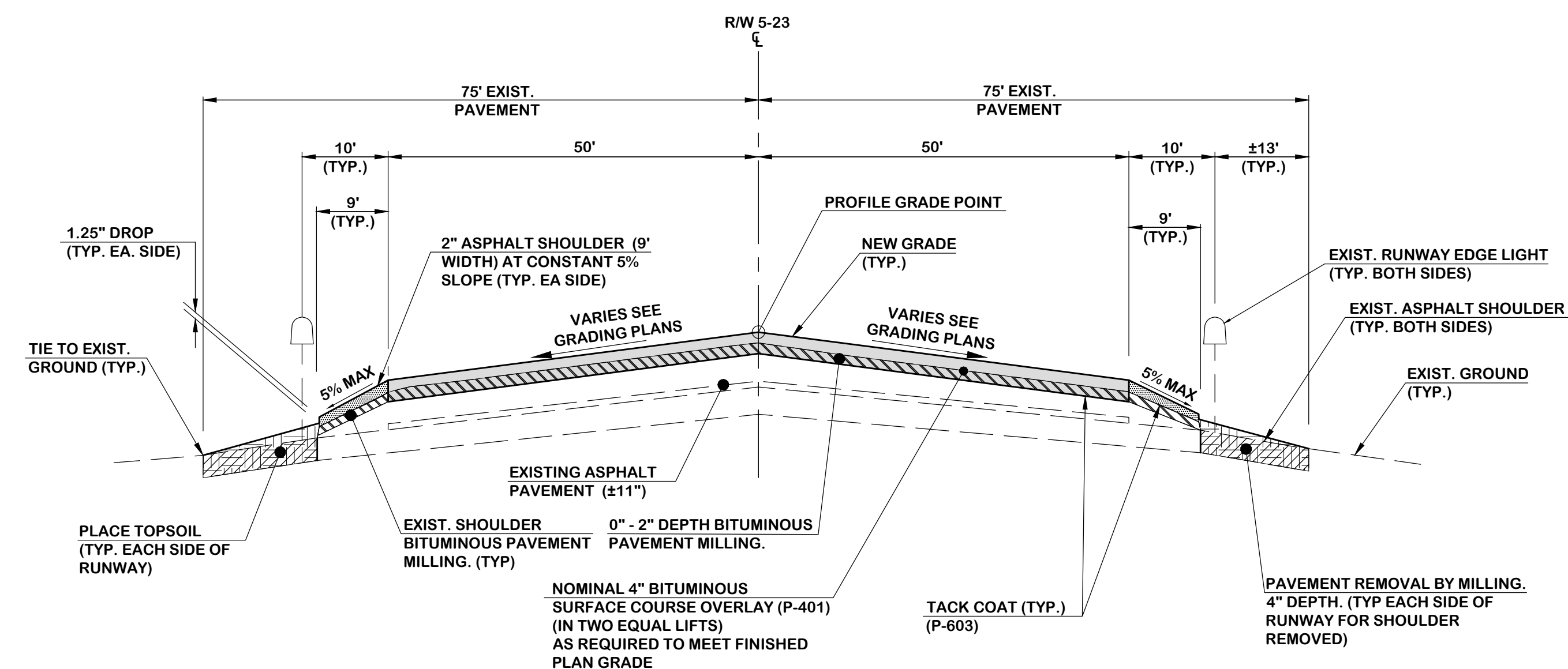
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TYPICAL SECTIONS AND PAVEMENT DETAILS

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

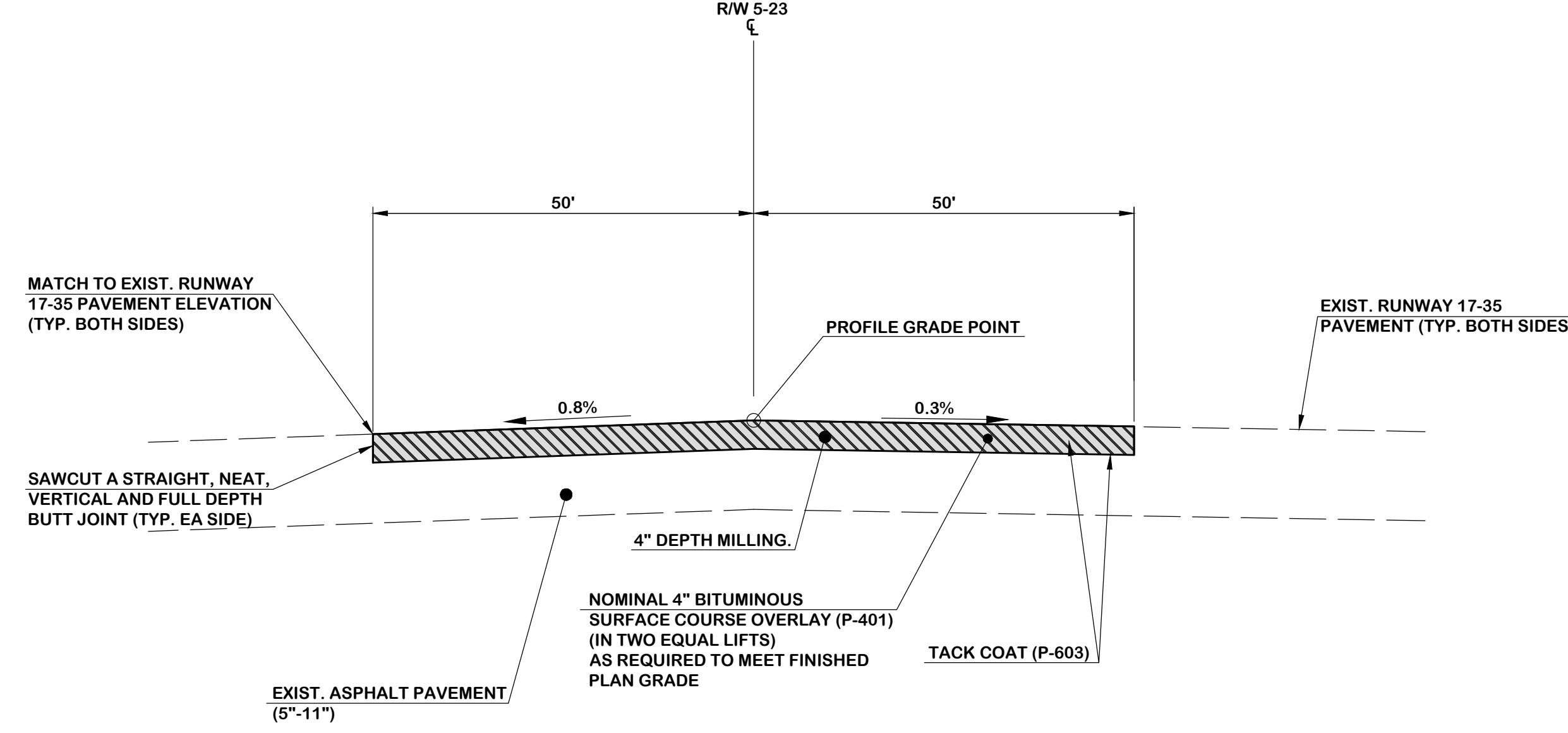
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Drawing Number:
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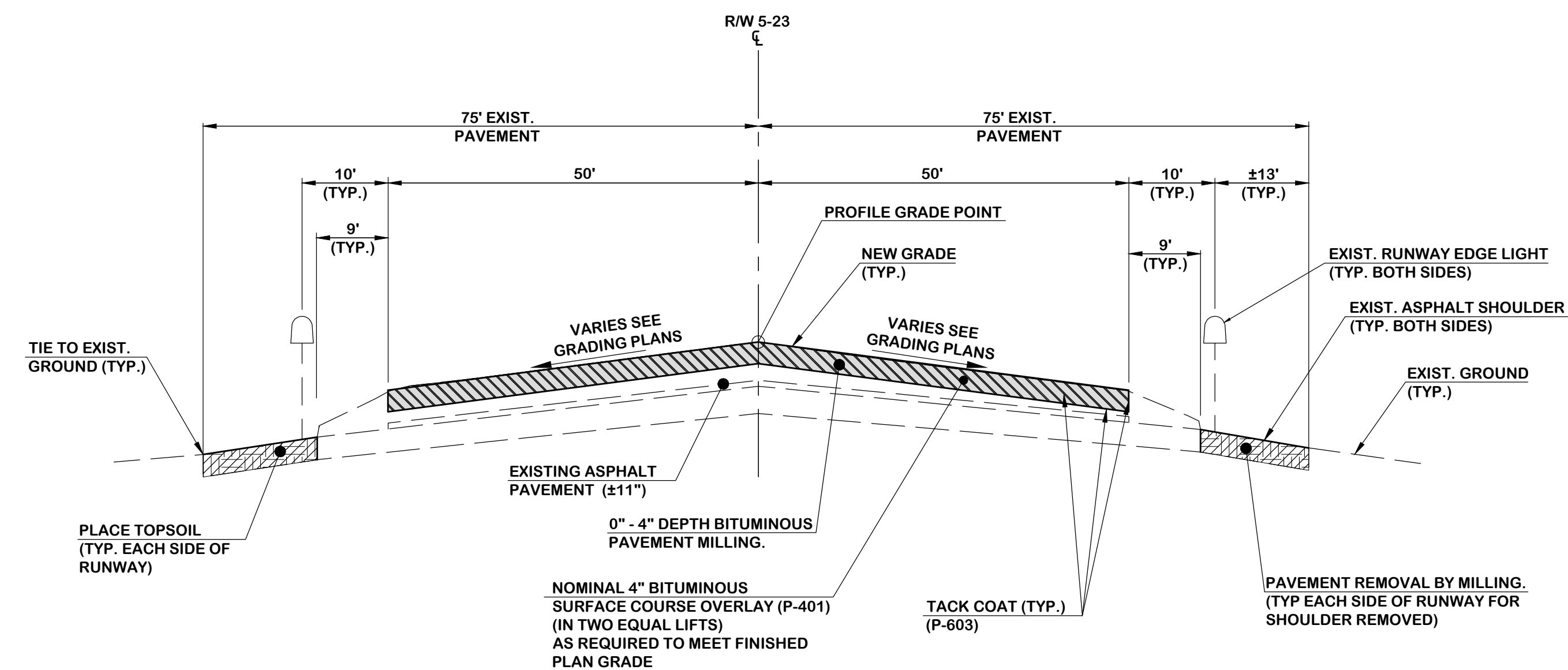


1 **BASE BID ALTERNATE NO. 2**
TYPICAL SECTION - RUNWAY 5-23 2" MILL AND 4" FILL
N.T.S.

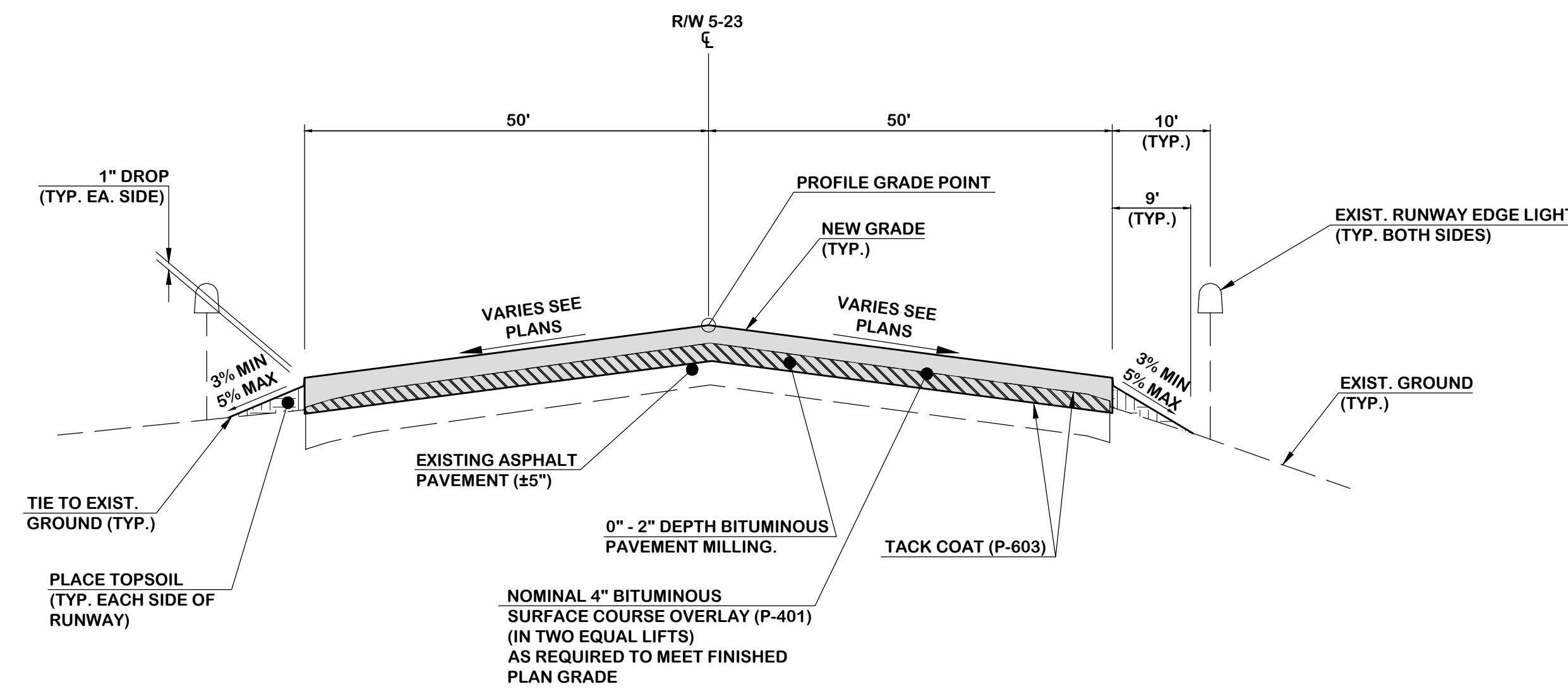
- LEGEND**
- NEW ASPHALT SURFACE COURSE (P-401)
 - VARIABLE DEPTH MILLING (S-180)
 - NEW ASPHALT SHOULDER PAVEMENT (SCDOT TYPE C, SC-403)
 - PAVEMENT REMOVAL BY MILLING
 - TOPSOIL PLACEMENT (T-905)



2 **BASE BID ALTERNATE NO. 2 (MATCH GRADE AT EDGE AT RW)**
TYPICAL SECTION - RUNWAY 5-23 AT RUNWAY 17-35 INTERSECTION
N.T.S.

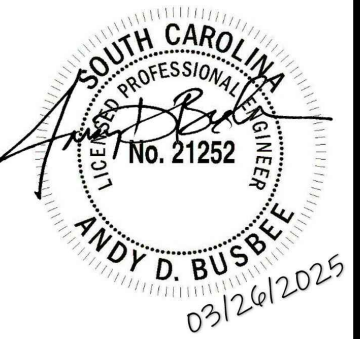


3 **BASE BID ALTERNATE NO. 2**
TYPICAL SECTION - RUNWAY 5-23 4" MILL AND FILL
N.T.S.



4 **BASE BID ALTERNATE NO. 2**
(EXISTING EXTENSION SECTION)
TYPICAL SECTION - RUNWAY 5-23 MILL AND FILL
N.T.S.
STA. 53+75.16 TO STA. 65+02.00

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Designer: GBV	Checked by: ADB
Technician: NWM	Project Number: 192930

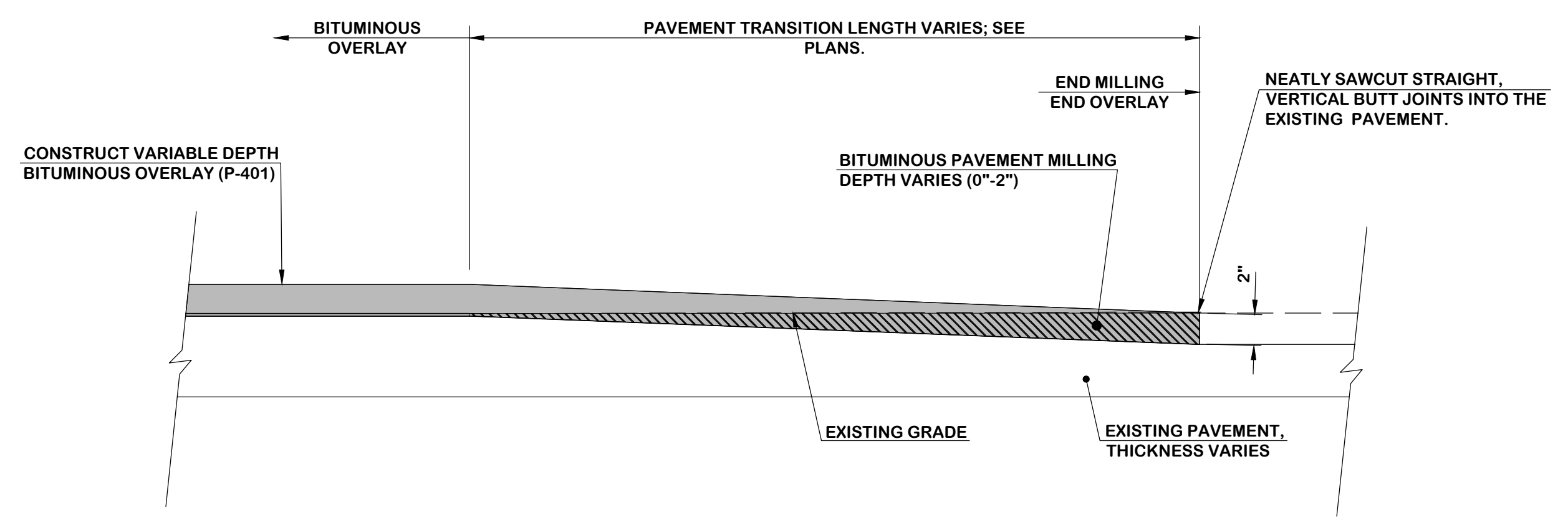
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

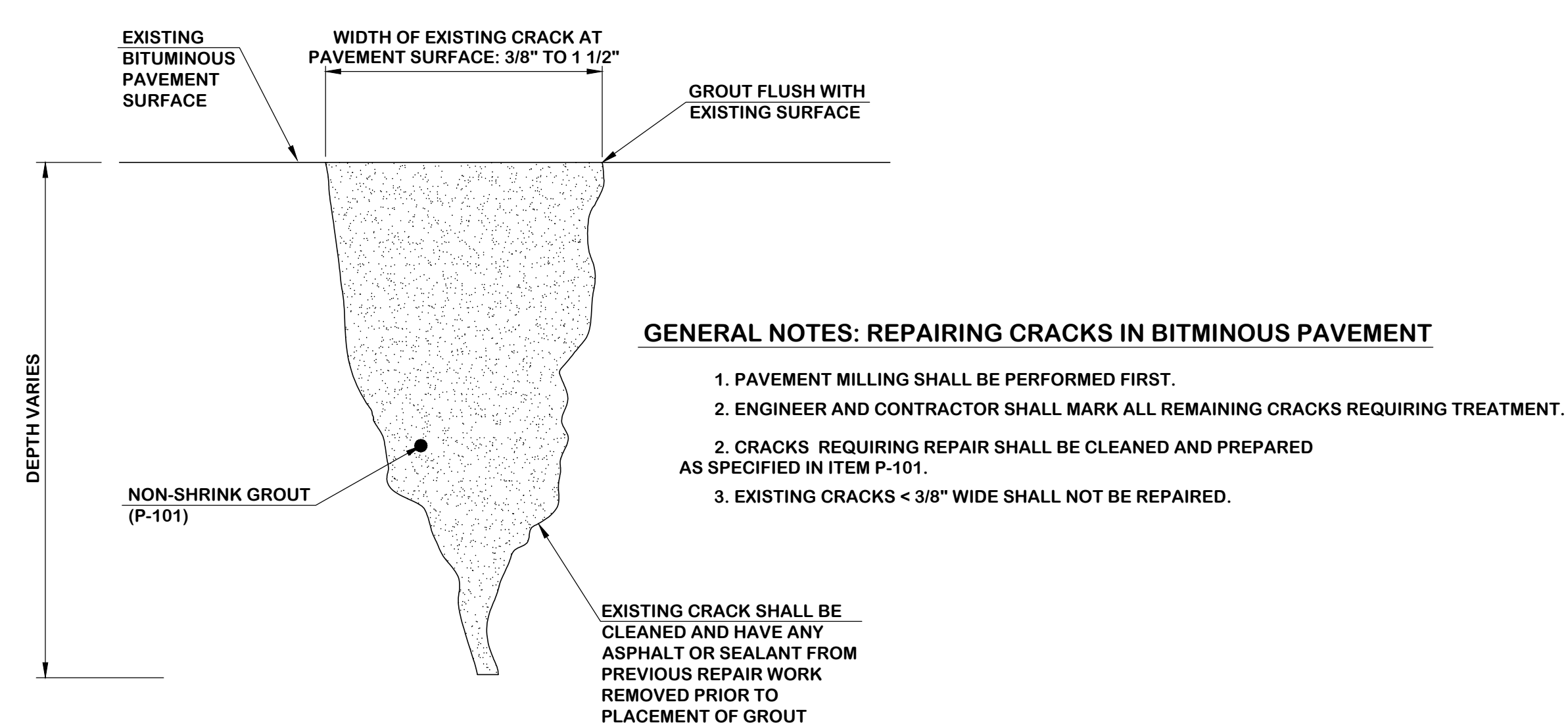
Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
TYPICAL SECTIONS AND PAVEMENT DETAILS

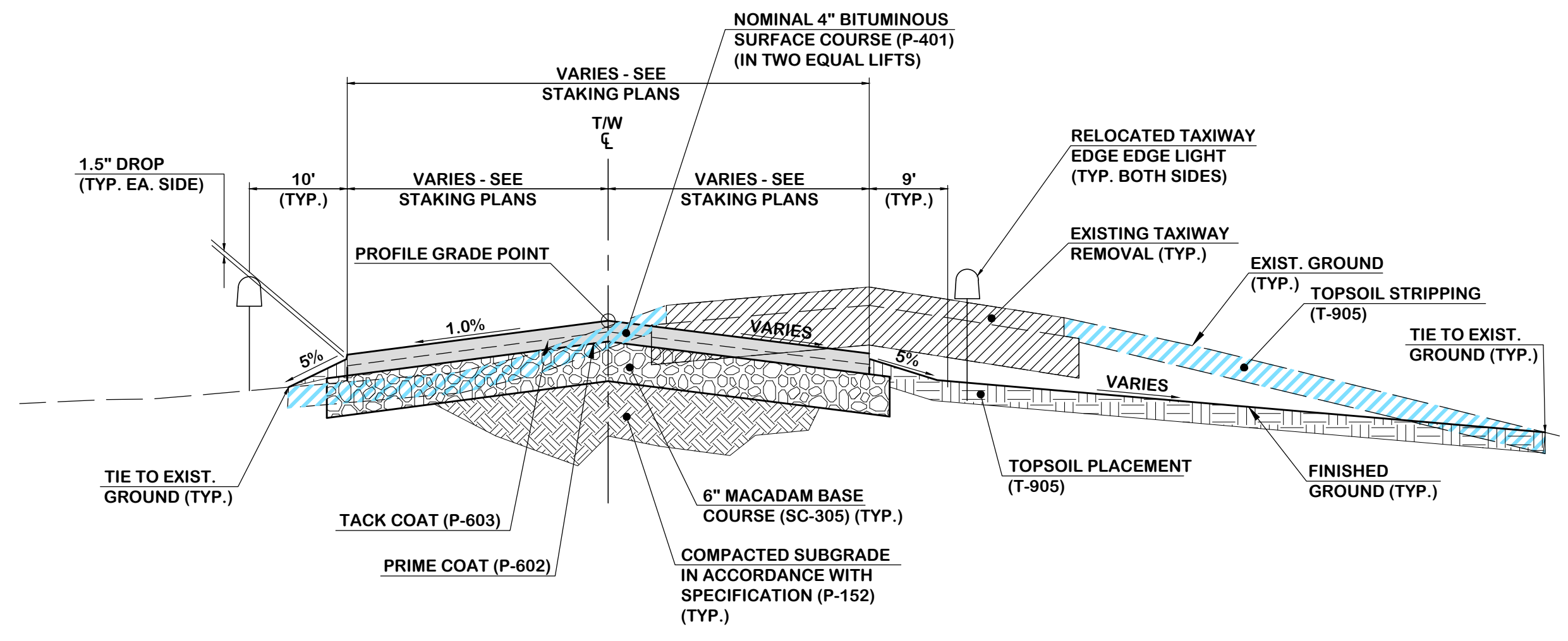
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Date: MARCH, 2025	Division: AIRPORTS
Scale: N.T.S.	Sheet Number: TS-2
Drawing Number: TS-2	



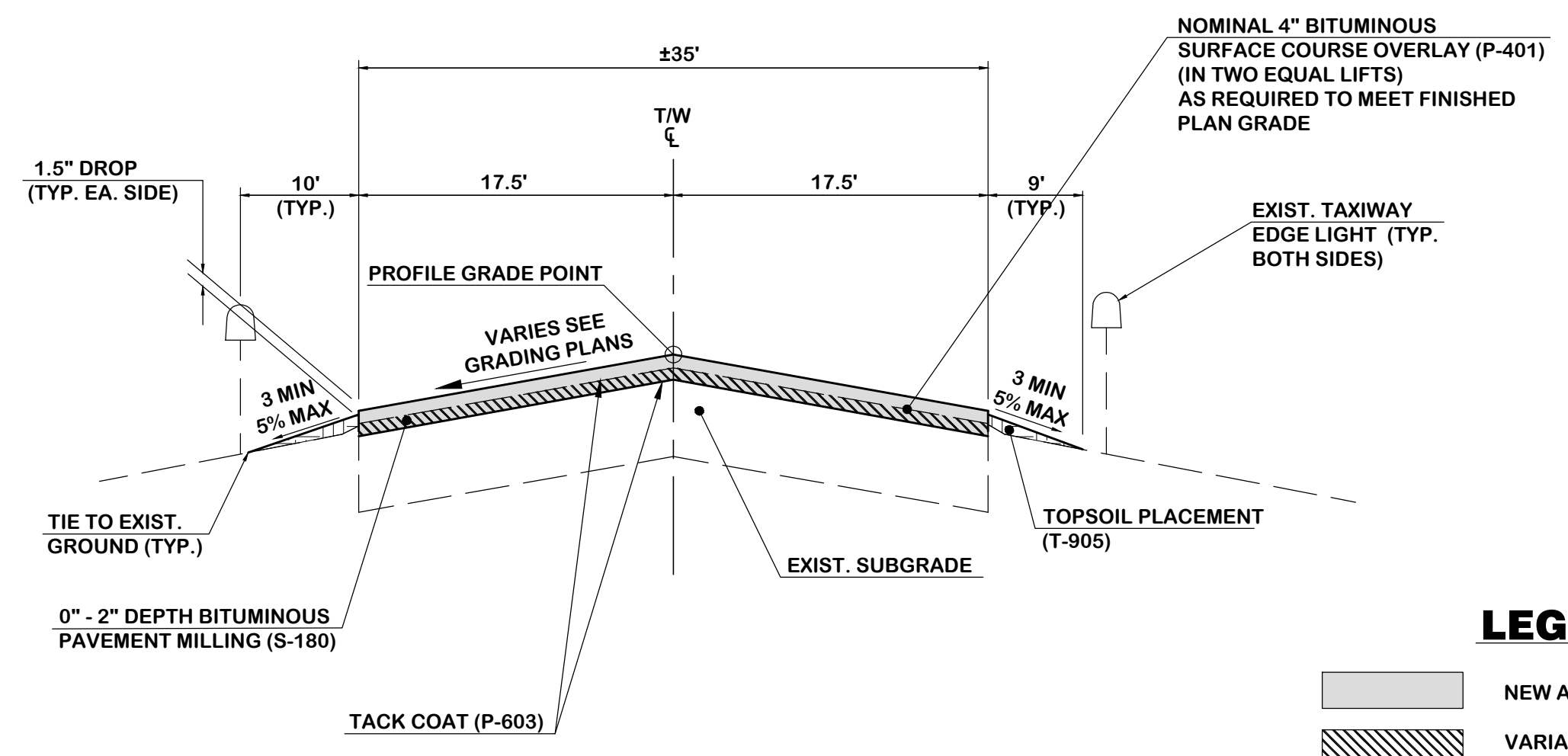
2 TYPICAL TAXIWAY ASPHALT SECTION - MILLING TIE-IN
N.T.S.



5 REPAIRING MEDIUM CRACK IN BITUMINOUS PAVEMENT
N.T.S.



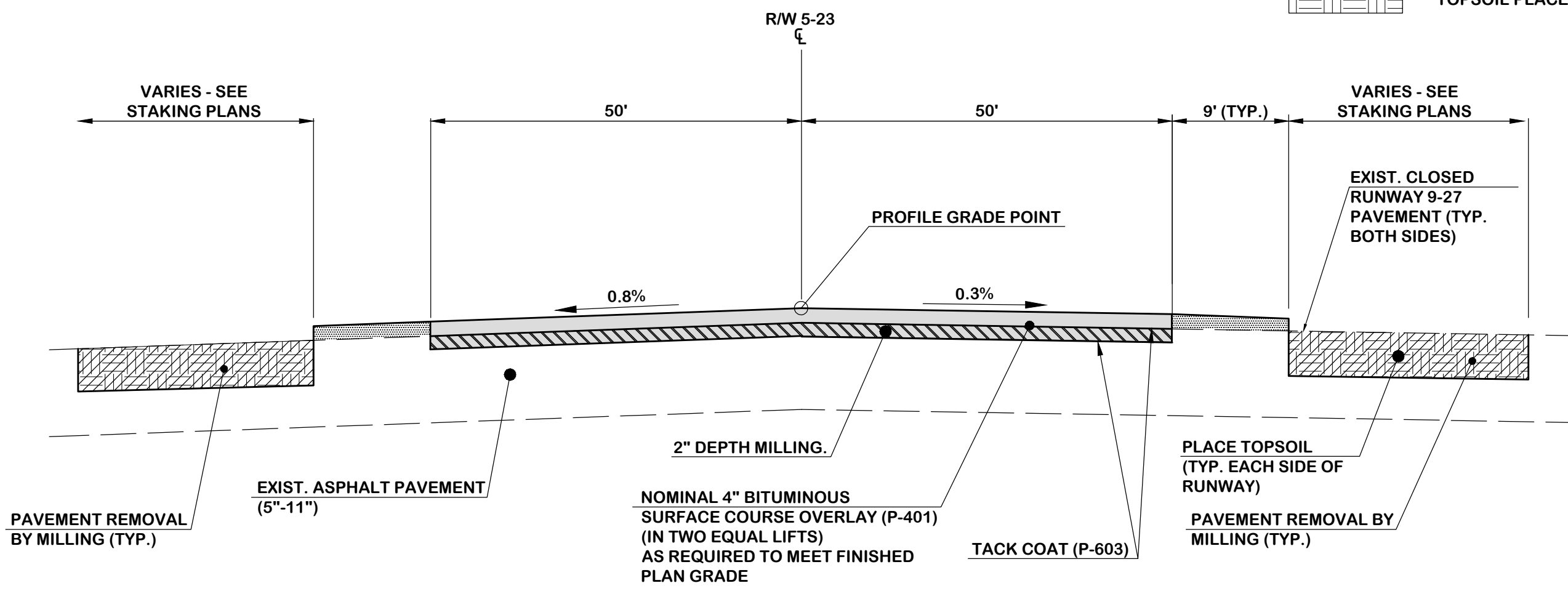
6 ADDITIVE BID NO. 1 TYPICAL SECTION - MIDFIELD CONNECTOR TAXIWAY A3
N.T.S. (STA. 1+90 SHOWN)



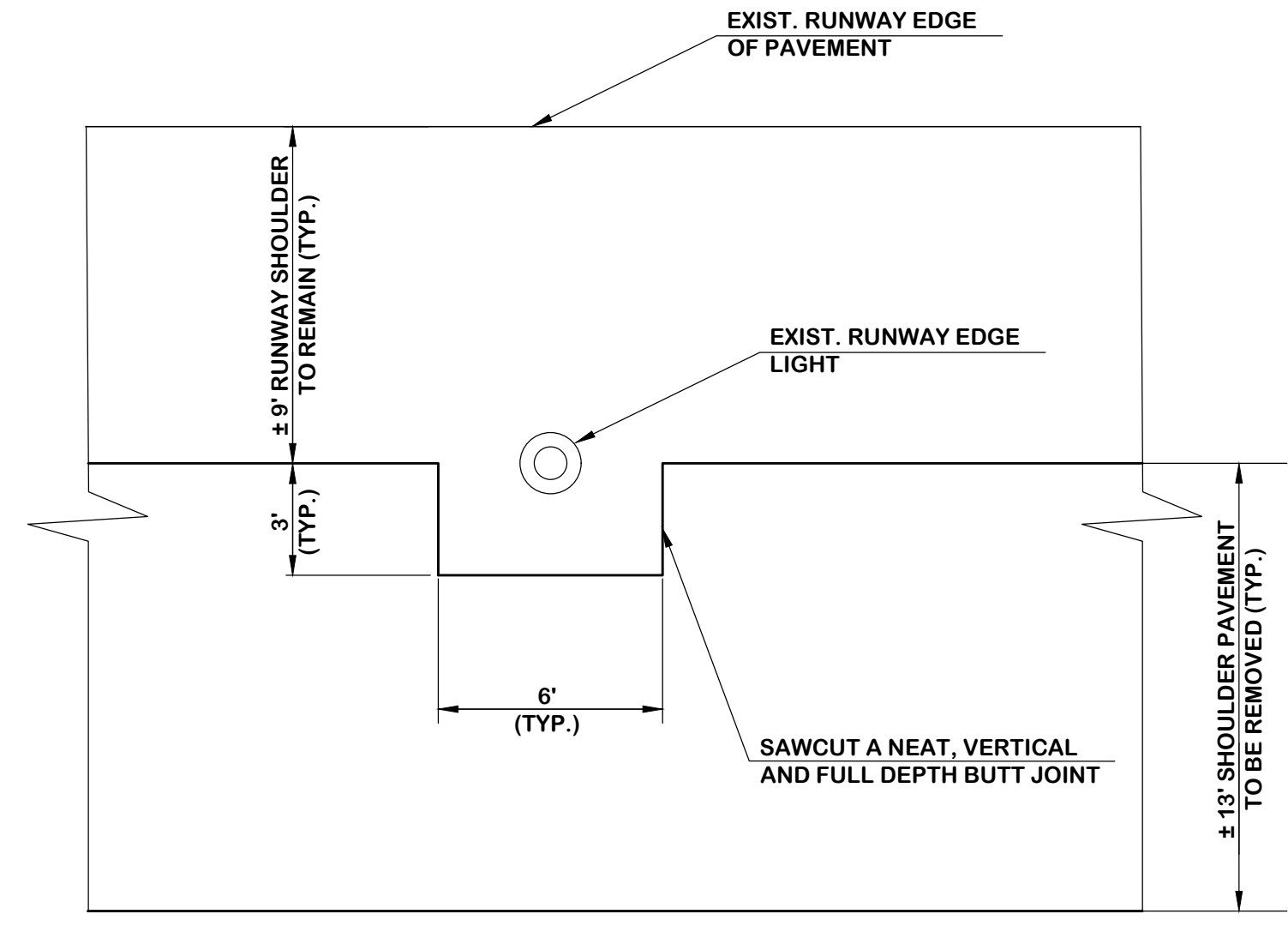
1 BASE BID ALTERNATE NO. 2 TYPICAL SECTION - TAXIWAY
N.T.S.

LEGEND

	NEW ASPHALT SURFACE COURSE (P-401)
	VARIABLE DEPTH MILLING (S-180)
	NEW ASPHALT SHOULDER PAVEMENT (SCDDOT TYPE C, SC-403)
	PAVEMENT REMOVAL BY MILLING
	TOPSOIL STRIPPING (T-905)
	TOPSOIL PLACEMENT (T-905)

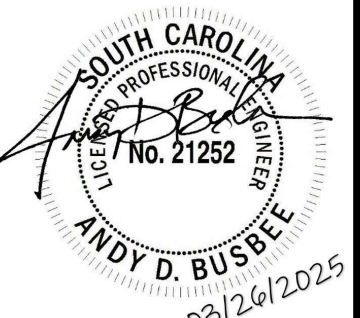


3 BASE BID ALTERNATE NO. 2 TYPICAL SECTION - RUNWAY 5-23 AT CLOSED RUNWAY 9-27 INTERSECTION
N.T.S.



4 PAVEMENT REMOVAL AROUND RUNWAY EDGE LIGHT DETAIL
N.T.S.

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Designer: GBV	Checked by: ADB
Technician: NWM	Project Number: 192930

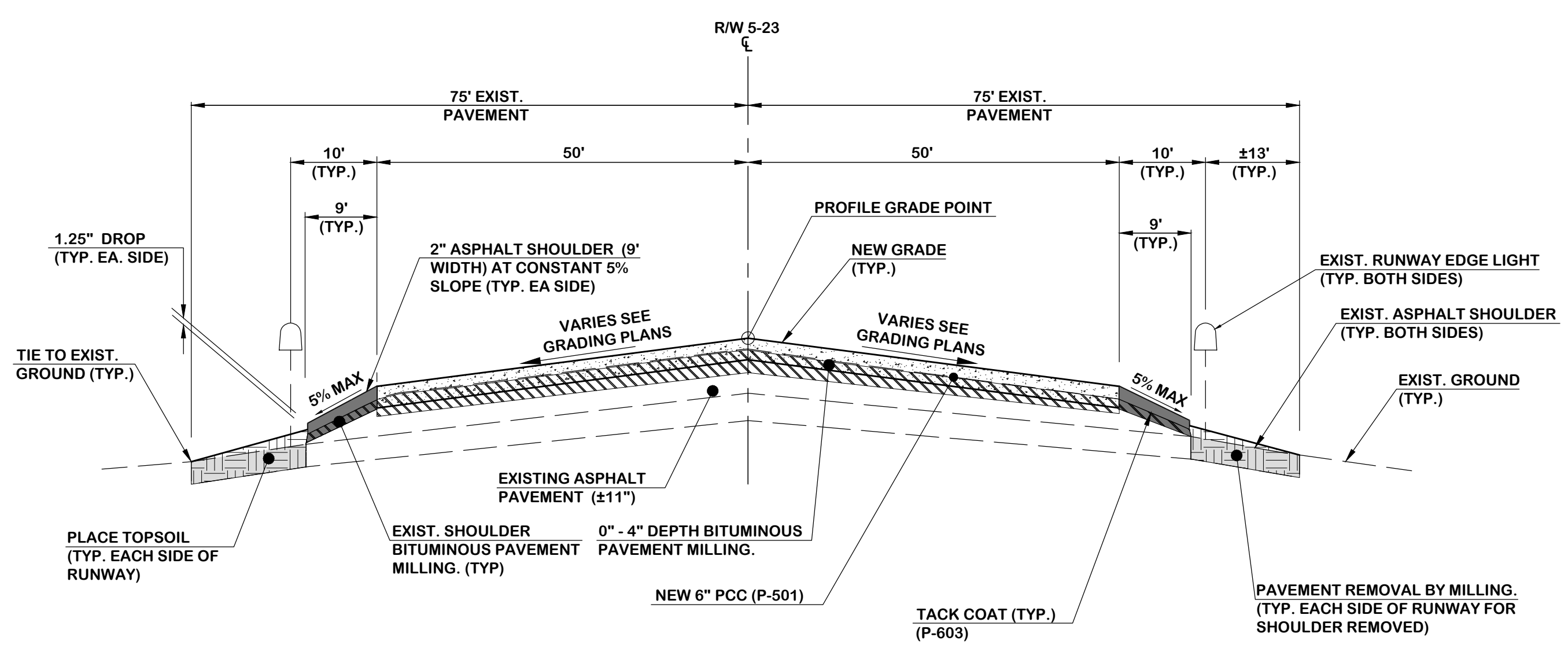
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

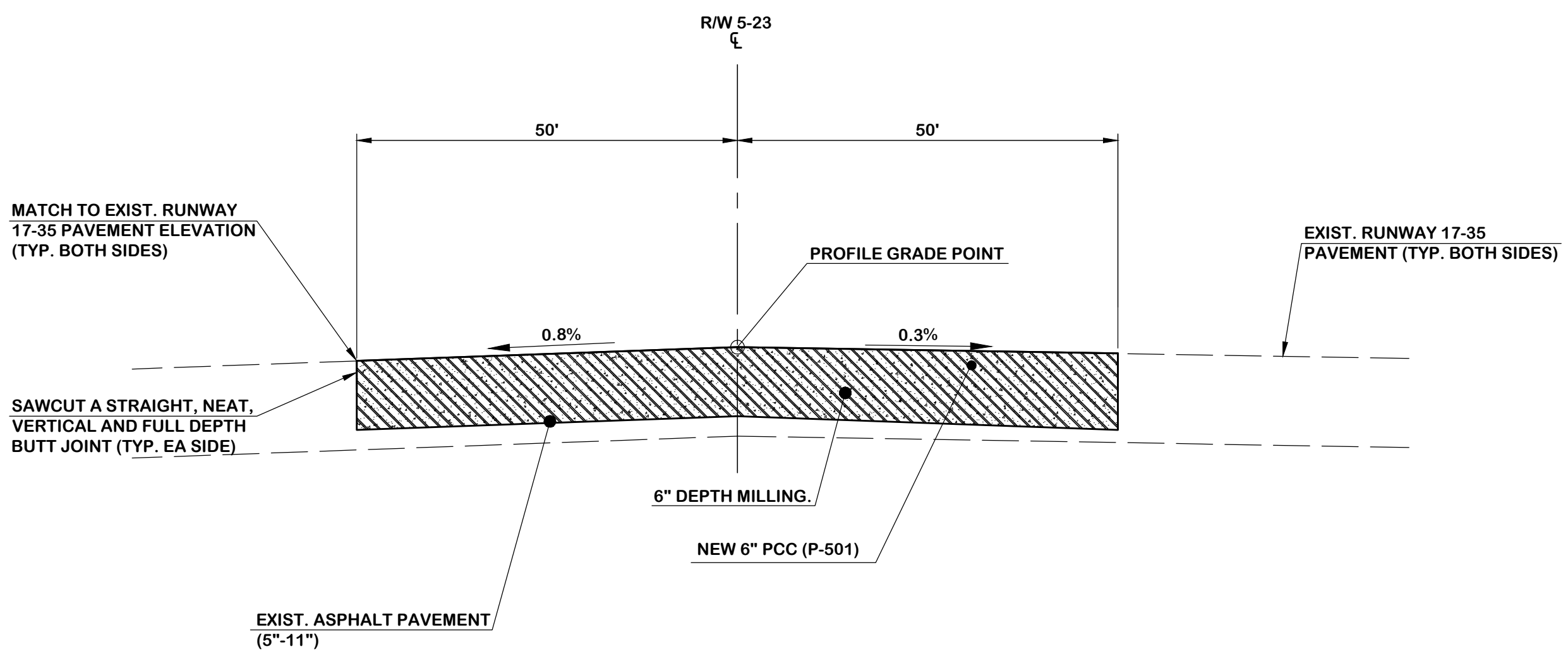
Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
TYPICAL SECTIONS AND PAVEMENT DETAILS

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: N.T.S.	Sheet Number: TS-3



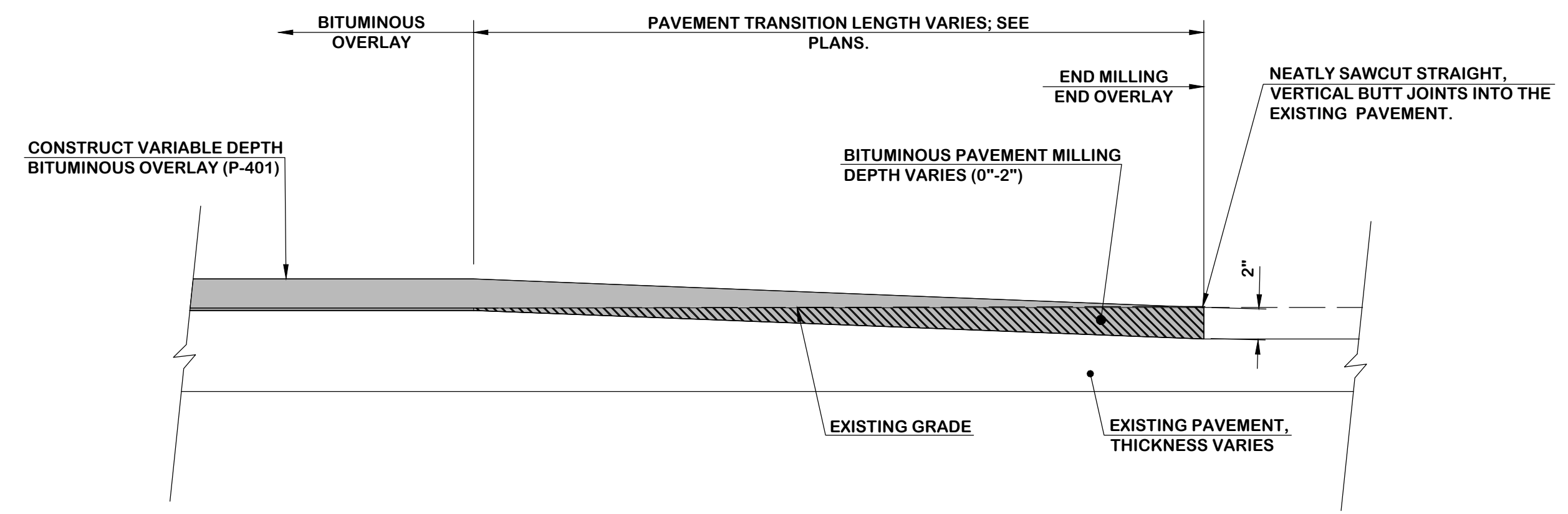
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BASE BID ALTERNATE NO. 1 (CONCRETE)
TYPICAL SECTION (4" MILL AND 6" OVERLAY)
N.T.S.



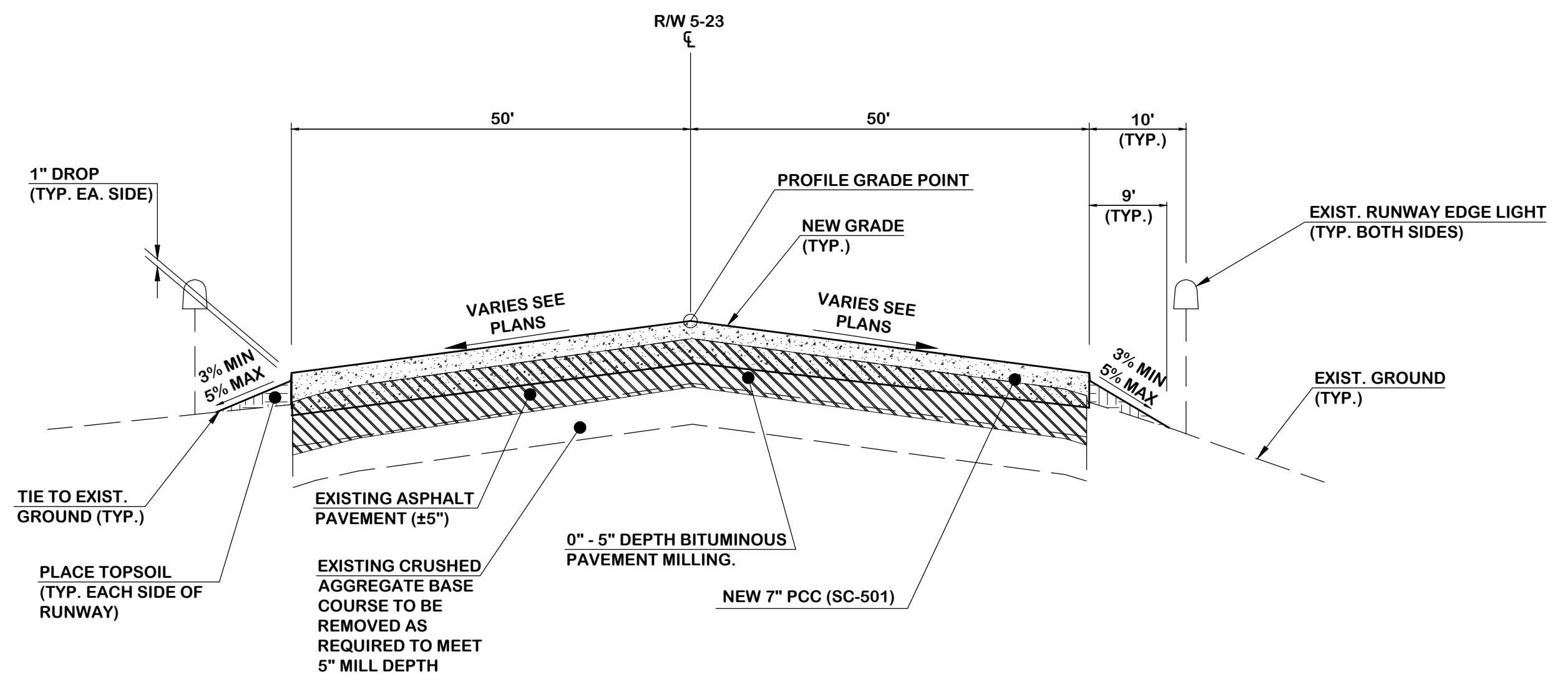
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BASE BID ALTERNATE NO. 1 (CONCRETE)
(MATCH GRADE AT EDGE OF RW)
PCC TYPICAL SECTION - RUNWAY 5-23 AT
RUNWAY 17-35 INTERSECTION
N.T.S.

LEGEND

	NEW PCC (P-501)
	VARIABLE DEPTH MILLING (S-180)
	NEW ASPHALT SHOULDER PAVEMENT (SCDOT TYPE C, SC-403)
	PAVEMENT REMOVAL BY MILLING
	TOPSOIL PLACEMENT (T-905)



3
TYPICAL TAXIWAY ASPHALT SECTION - MILLING TIE-IN
N.T.S.



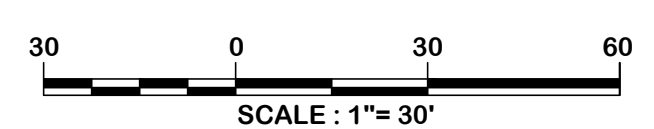
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BASE BID ALTERNATE NO. 1 (CONCRETE)
TYPICAL SECTION (5" MILL AND 7" OVERLAY)
N.T.S.
STA. 53+75.16 TO STA. 65+02.00

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Designer: BCM / LAW	Checked by: ADB
Technician: LAW	Project Number: 192930

DEMOLITION NOTES
AFTER MILLING THE RUNWAY ASPHALT. THE CONTRACTOR AND ENGINEER SHALL INSPECT THE REMAINING ASPHALT PAVEMENT CRACKS. CRACKS GREATER THAN 3/8" SHALL BE TREATED WITH NON-SHRINK GROUT AS DETAILED ON DRAWING TS-2. QUANTITIES WILL BE AGREED UPON BY ENGINEER AND CONTRACTOR PRIOR TO THE WORK BEING ACCOMPLISHED. AMOUNTS THAT WILL EXCEED THE BUDGETED CRACK REPAIR AMOUNT SHALL REQUIRE ENGINEER AND OWNER APPROVAL PRIOR TO PERFORMANCE.



ISSUED FOR BIDS

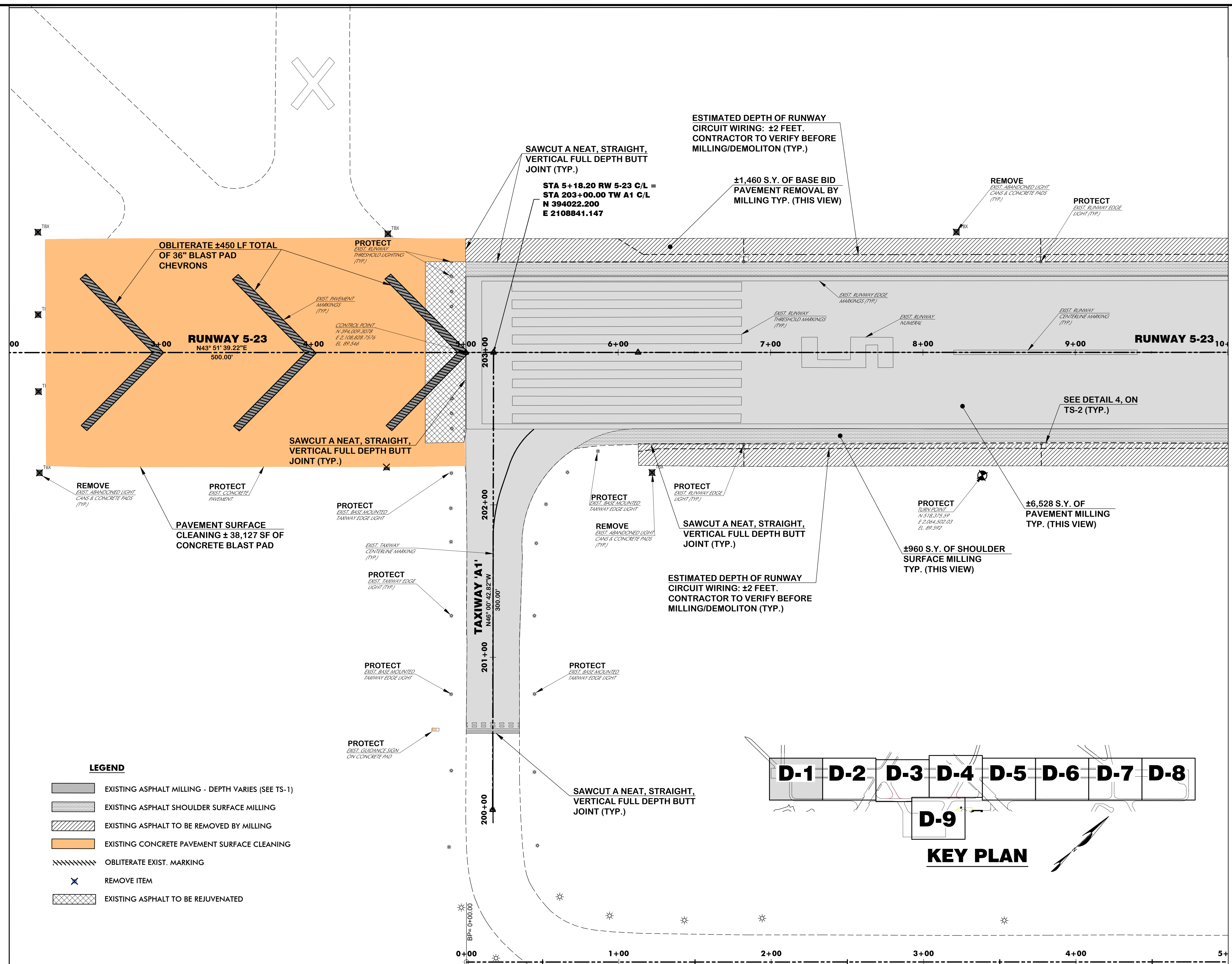
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
DEMOLITION PLAN NO. 1

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
	Drawing Number: D-1

MATCHLINE STA. 10+00, SEE DWG. D-2



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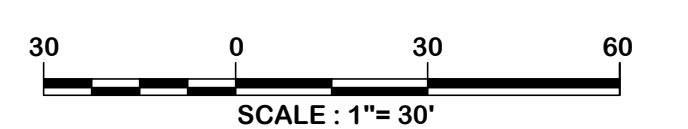
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INTERNATIONAL

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Designer: BCM / LAW
Checked by: ADB
Technician: LAW
Project Number: 192930

DEMOLITION NOTES
AFTER MILLING THE RUNWAY ASPHALT. THE CONTRACTOR AND ENGINEER SHALL INSPECT THE REMAINING ASPHALT PAVEMENT CRACKS. CRACKS GREATER THAN 3/8" SHALL BE TREATED WITH NON-SHRINK GROUT AS DETAILED ON DRAWING TS-2. QUANTITIES WILL BE AGREED UPON BY ENGINEER AND CONTRACTOR PRIOR TO THE WORK BEING ACCOMPLISHED. AMOUNTS THAT WILL EXCEED THE BUDGETED CRACK REPAIR AMOUNT SHALL REQUIRE ENGINEER AND OWNER APPROVAL PRIOR TO PERFORMANCE.



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

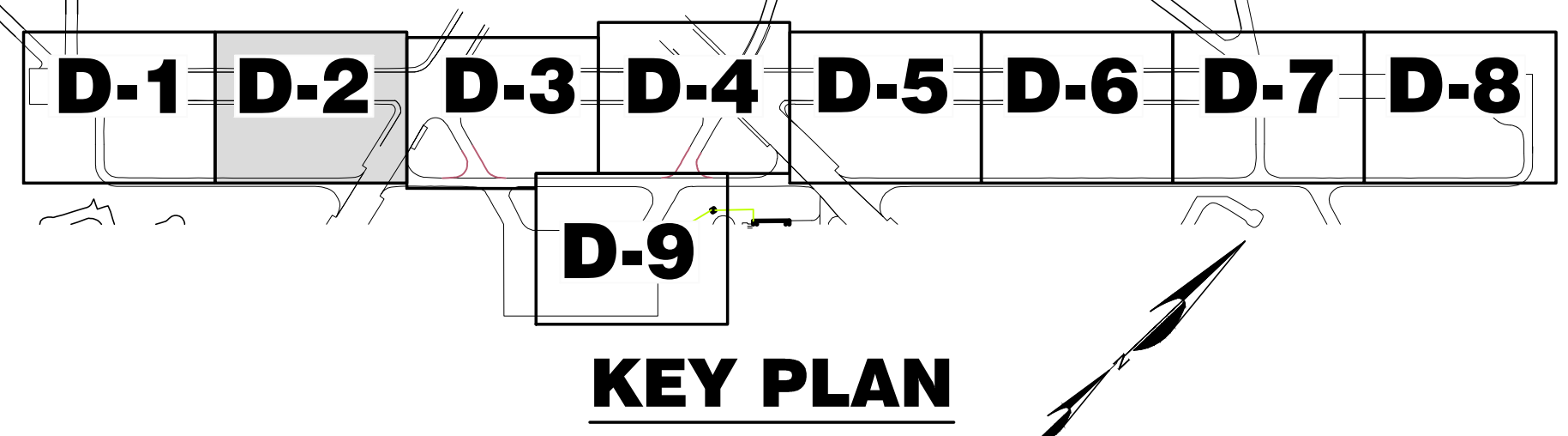
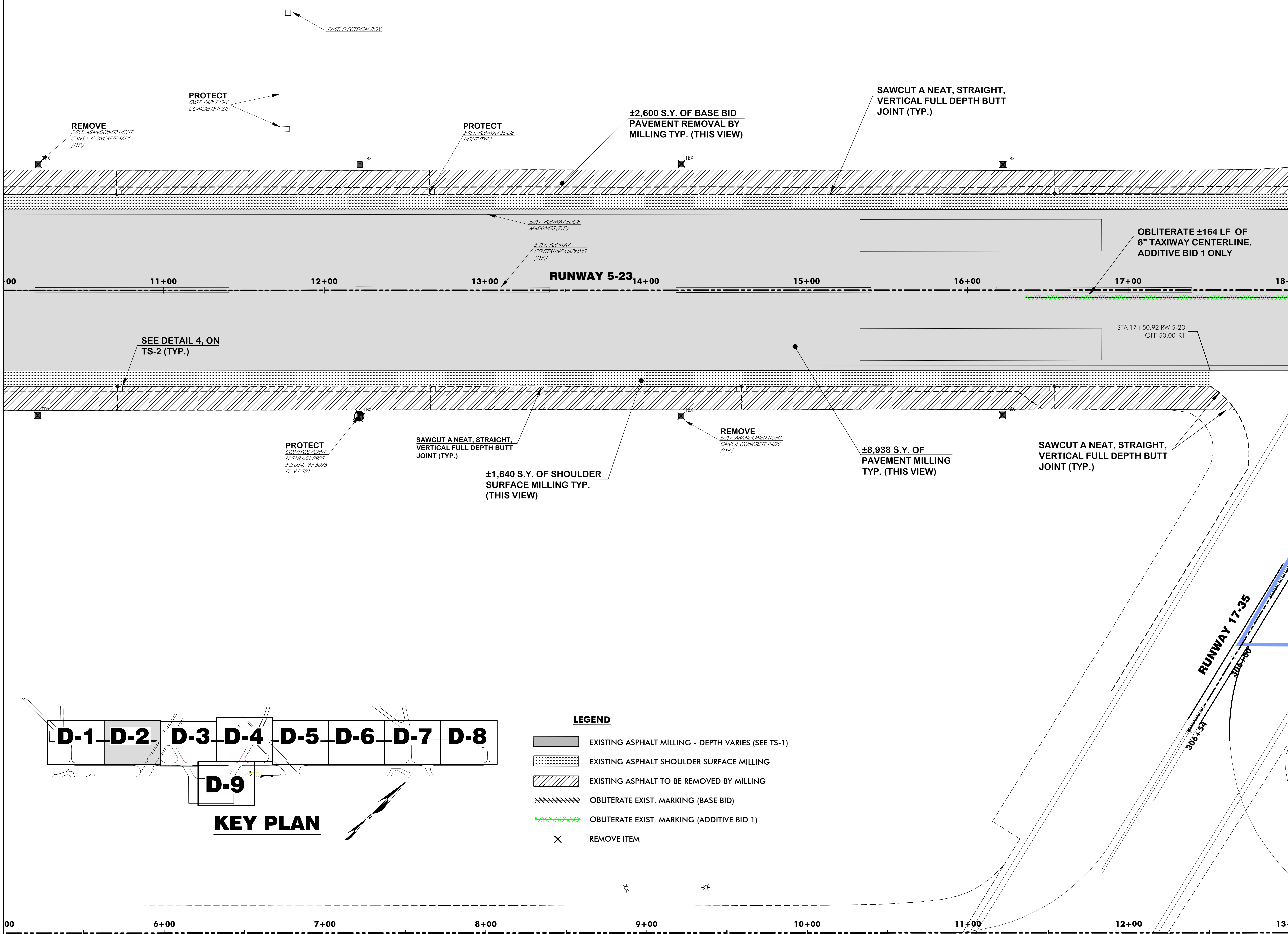
Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
DEMOLITION PLAN NO. 2

FAA A.I.P. Project Number:
3-45-0057-025-2025
Date: **MARCH, 2025** Division: **AIRPORTS**
Scale: **1"= 30'** Sheet Number: **-**
Drawing Number: **D-2**

MATCHLINE STA. 10+00, SEE DWG. D-1

MATCHLINE STA. 18+00, SEE DWG. D-3



LEGEND

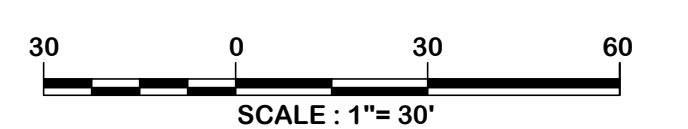
	EXISTING ASPHALT MILLING - DEPTH VARIES (SEE TS-1)
	EXISTING ASPHALT SHOULDER SURFACE MILLING
	EXISTING ASPHALT TO BE REMOVED BY MILLING
	OBLITERATE EXIST. MARKING (BASE BID)
	OBLITERATE EXIST. MARKING (ADDITIVE BID 1)
	REMOVE ITEM

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Designer: BCM / LAW	Checked by: ADB
Technician: LAW	Project Number: 192930

DEMOLITION NOTES
AFTER MILLING THE RUNWAY ASPHALT. THE CONTRACTOR AND ENGINEER SHALL INSPECT THE REMAINING ASPHALT PAVEMENT CRACKS. CRACKS GREATER THAN 3/8" SHALL BE TREATED WITH NON-SHRINK GROUT AS DETAILED ON DRAWING TS-2. QUANTITIES WILL BE AGREED UPON BY ENGINEER AND CONTRACTOR PRIOR TO THE WORK BEING ACCOMPLISHED. AMOUNTS THAT WILL EXCEED THE BUDGETED CRACK REPAIR AMOUNT SHALL REQUIRE ENGINEER AND OWNER APPROVAL PRIOR TO PERFORMANCE.



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

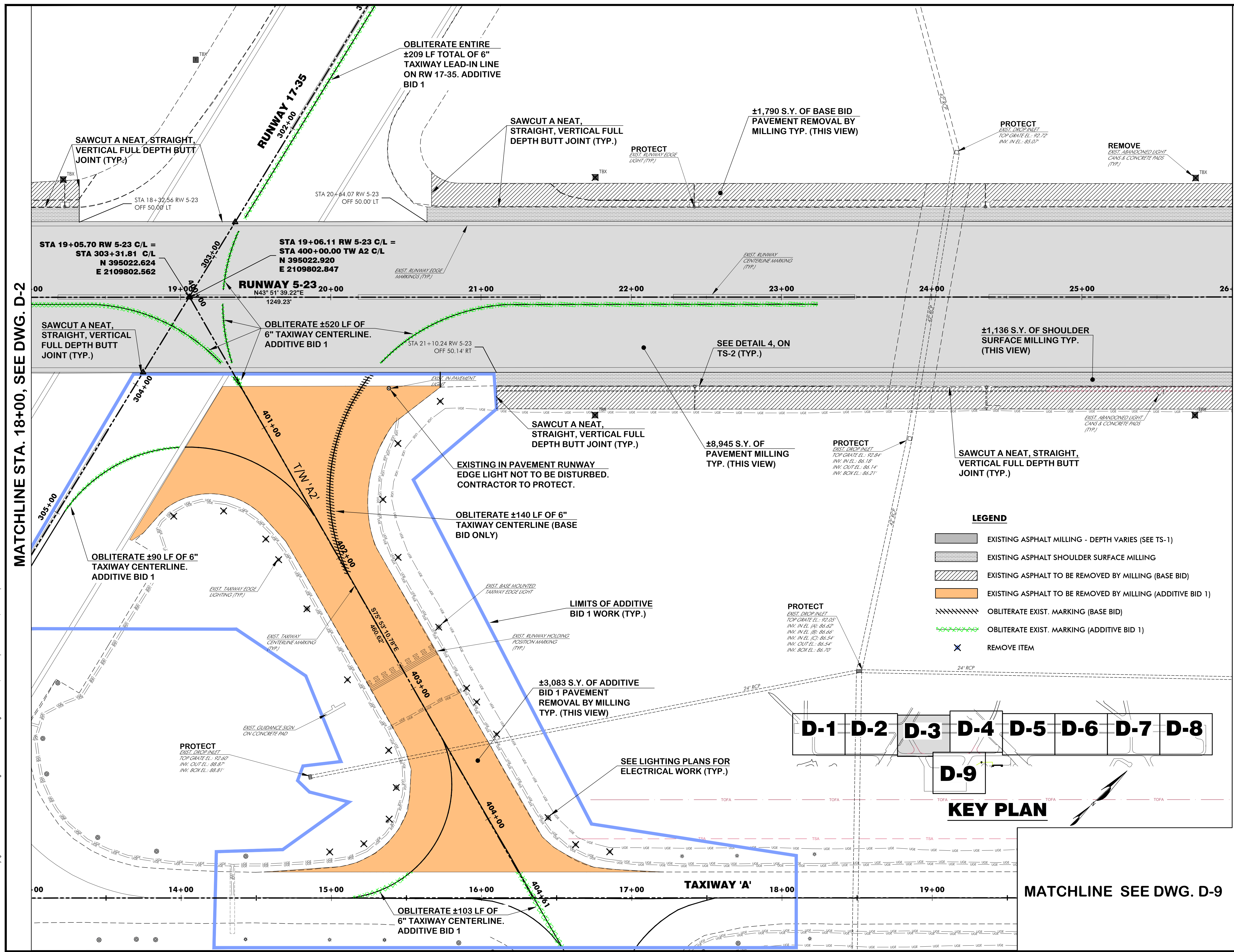
Drawing Name:
DEMOLITION PLAN NO. 3

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025 Division: AIRPORTS

Scale: 1"=30' Sheet Number: -

Drawing Number:
D-3



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WALTERBORO, SOUTH CAROLINA

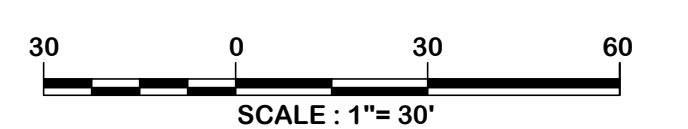
Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / LAW
Checked by: ADB
Technician: LAW
Project Number: 192930

DEMOLITION NOTES
AFTER MILLING THE RUNWAY ASPHALT. THE CONTRACTOR AND ENGINEER SHALL INSPECT THE REMAINING ASPHALT PAVEMENT CRACKS. CRACKS GREATER THAN 3/8" SHALL BE TREATED WITH NON-SHRINK GROUT AS DETAILED ON DRAWING TS-2. QUANTITIES WILL BE AGREED UPON BY ENGINEER AND CONTRACTOR PRIOR TO THE WORK BEING ACCOMPLISHED. AMOUNTS THAT WILL EXCEED THE BUDGETED CRACK REPAIR AMOUNT SHALL REQUIRE ENGINEER AND OWNER APPROVAL PRIOR TO PERFORMANCE.



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

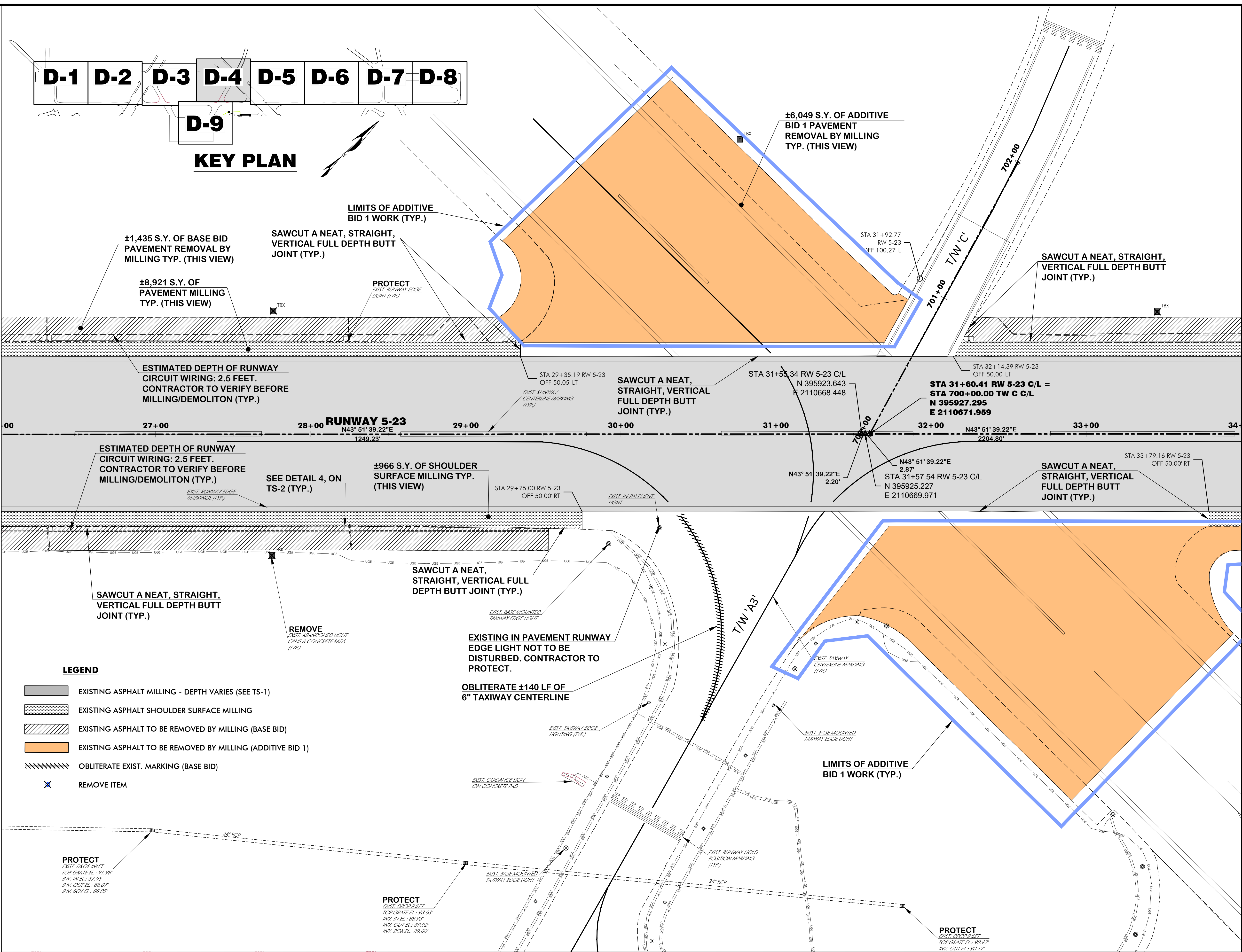
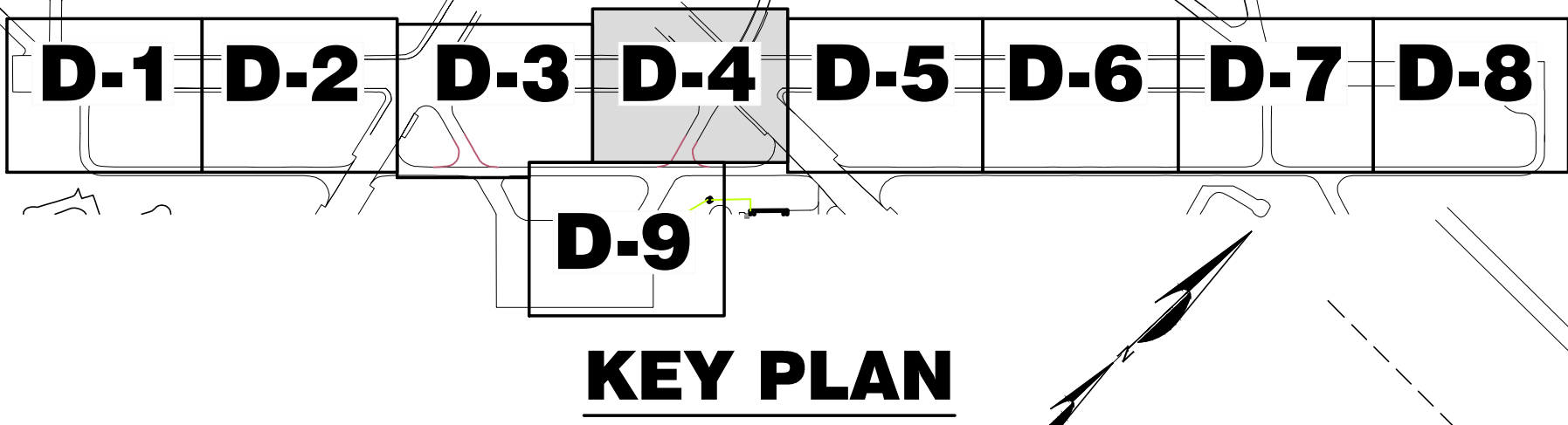
Drawing Name:
DEMOLITION PLAN NO. 4

FAA A.I.P. Project Number:
3-45-0057-025-2025
Date: **MARCH, 2025** Division: **AIRPORTS**
Scale: **1"= 30'** Sheet Number: **-**
Drawing Number: **D-4**

MATCHLINE STA. 26+00, SEE DWG. D-3

MATCHLINE STA. 34+00, SEE DWG. D-5

MATCHLINE SEE DWG. D-9



- LEGEND**
- EXISTING ASPHALT MILLING - DEPTH VARIES (SEE TS-1)
 - EXISTING ASPHALT SHOULDER SURFACE MILLING
 - EXISTING ASPHALT TO BE REMOVED BY MILLING (BASE BID)
 - EXISTING ASPHALT TO BE REMOVED BY MILLING (ADDITIVE BID 1)
 - OBLITERATE EXIST. MARKING (BASE BID)
 - REMOVE ITEM

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WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / LAW	Checked by: ADB
Technician: LAW	Project Number: 192930

DEMOLITION NOTES
AFTER MILLING THE RUNWAY ASPHALT, THE CONTRACTOR AND ENGINEER SHALL INSPECT THE REMAINING ASPHALT PAVEMENT CRACKS. CRACKS GREATER THAN 3/8" SHALL BE TREATED WITH NON-SHRINK GROUT AS DETAILED ON DRAWING TS-2. QUANTITIES WILL BE AGREED UPON BY ENGINEER AND CONTRACTOR PRIOR TO THE WORK BEING ACCOMPLISHED. AMOUNTS THAT WILL EXCEED THE BUDGETED CRACK REPAIR AMOUNT SHALL REQUIRE ENGINEER AND OWNER APPROVAL PRIOR TO PERFORMANCE.



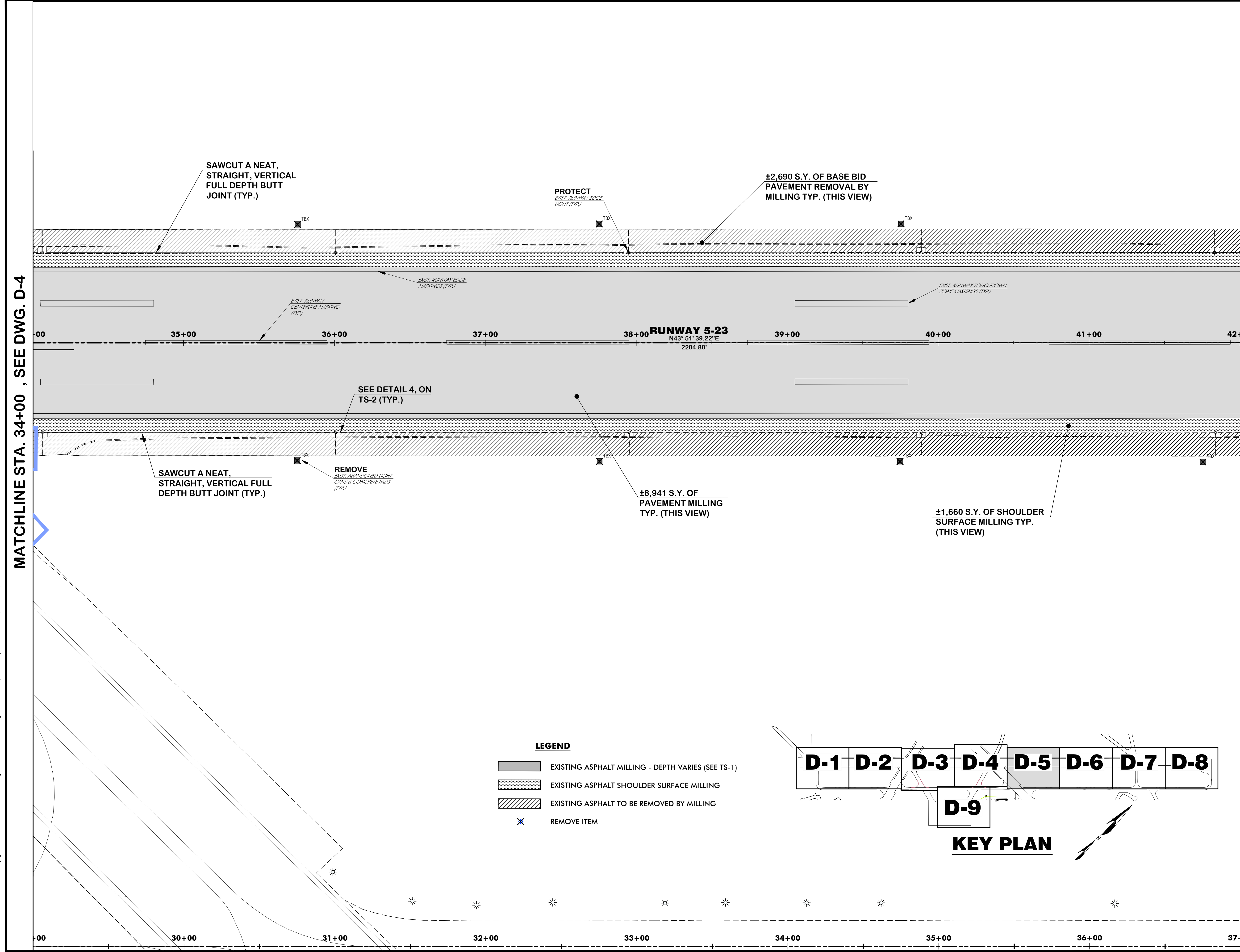
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
DEMOLITION PLAN NO. 5

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1" = 30'	Sheet Number: -
Drawing Number: D-5	



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WALTERBORO, SOUTH CAROLINA

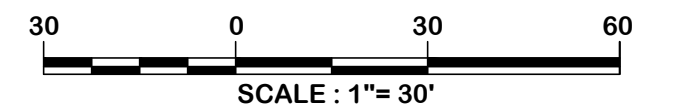
Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / LAW
Checked by: ADB
Technician: LAW
Project Number: 192930

DEMOLITION NOTES
AFTER MILLING THE RUNWAY ASPHALT. THE CONTRACTOR AND ENGINEER SHALL INSPECT THE REMAINING ASPHALT PAVEMENT CRACKS. CRACKS GREATER THAN 3/8" SHALL BE TREATED WITH NON-SHRINK GROUT AS DETAILED ON DRAWING TS-2. QUANTITIES WILL BE AGREED UPON BY ENGINEER AND CONTRACTOR PRIOR TO THE WORK BEING ACCOMPLISHED. AMOUNTS THAT WILL EXCEED THE BUDGETED CRACK REPAIR AMOUNT SHALL REQUIRE ENGINEER AND OWNER APPROVAL PRIOR TO PERFORMANCE.



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

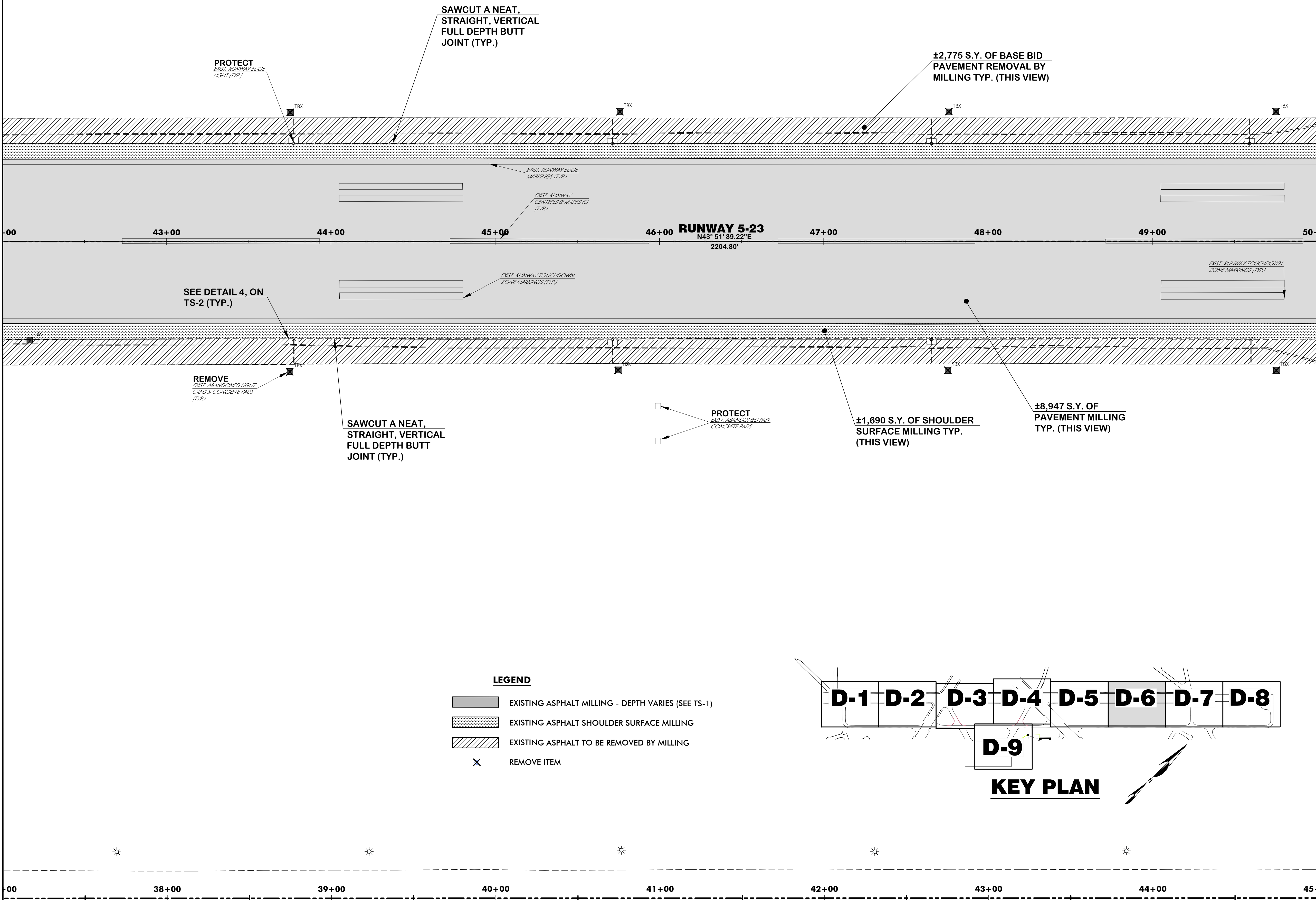
Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
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FAA A.I.P. Project Number:
3-45-0057-025-2025
Date: **MARCH, 2025** Division: **AIRPORTS**
Scale: **1"= 30'** Sheet Number: **-**
Drawing Number: **D-6**

MATCHLINE STA. 42+00, SEE DWG. D-5

MATCHLINE STA. 50+00, SEE DWG. D-7



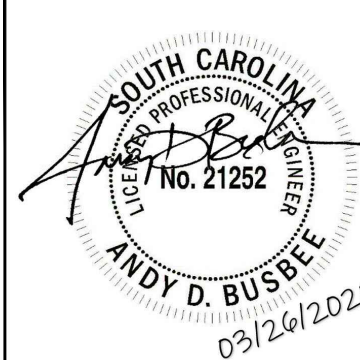
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WALTERBORO, SOUTH CAROLINA

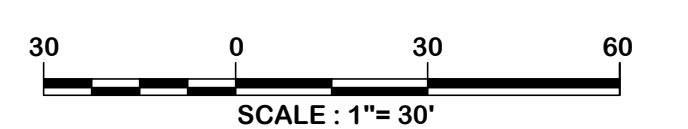
Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / LAW
Checked by: ADB
Technician: LAW
Project Number: 192930

DEMOLITION NOTES
AFTER MILLING THE RUNWAY ASPHALT, THE CONTRACTOR AND ENGINEER SHALL INSPECT THE REMAINING ASPHALT PAVEMENT CRACKS. CRACKS GREATER THAN 3/8" SHALL BE TREATED WITH NON-SHRINK GROUT AS DETAILED ON DRAWING TS-2. QUANTITIES WILL BE AGREED UPON BY ENGINEER AND CONTRACTOR PRIOR TO THE WORK BEING ACCOMPLISHED. AMOUNTS THAT WILL EXCEED THE BUDGETED CRACK REPAIR AMOUNT SHALL REQUIRE ENGINEER AND OWNER APPROVAL PRIOR TO PERFORMANCE.



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

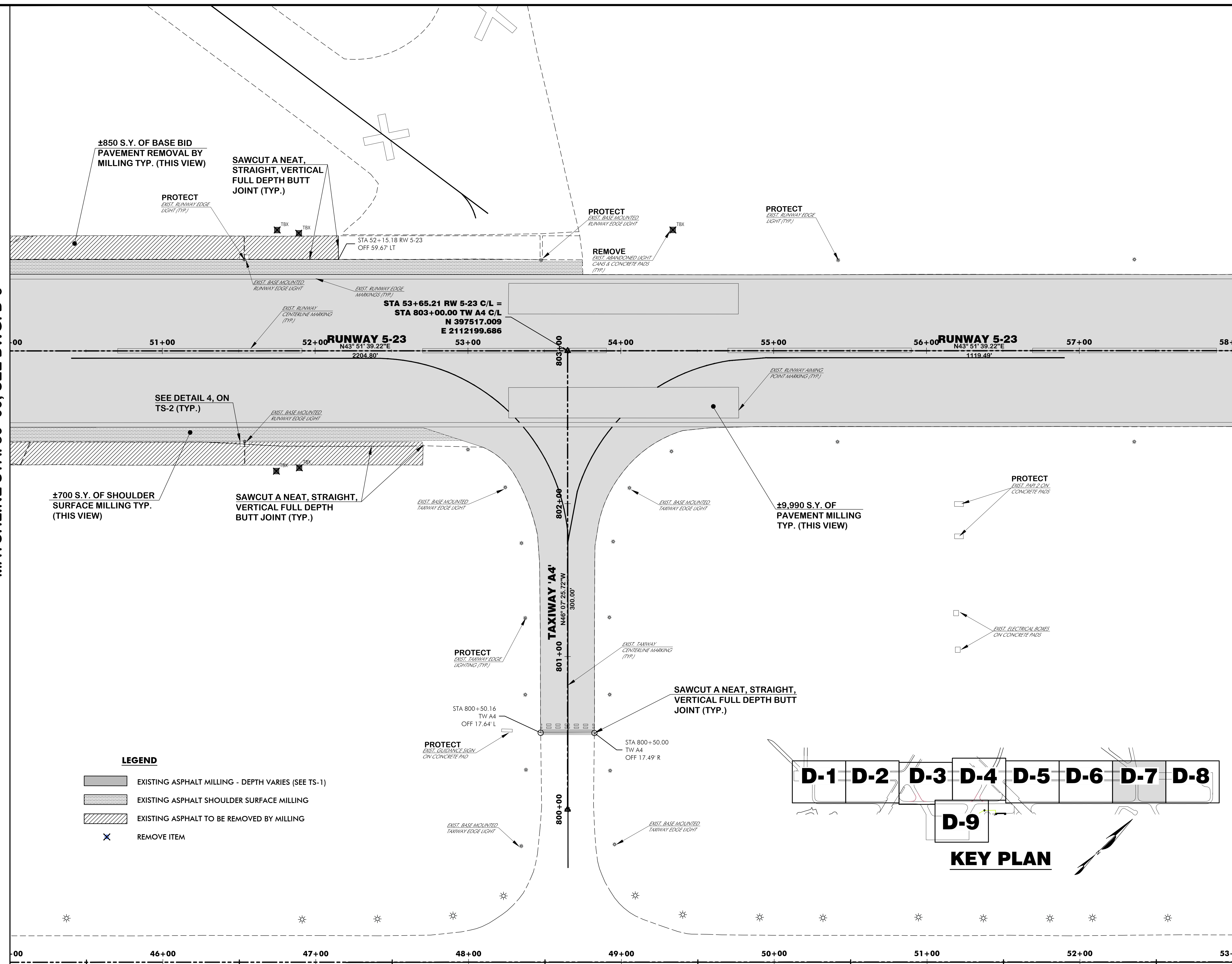
Drawing Name:
DEMOLITION PLAN NO. 7

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: AIRPORTS
Scale: 1" = 30'	Sheet Number: -
	Drawing Number: D-7

MATCHLINE STA. 50+00, SEE DWG. D-6

MATCHLINE STA. 58+00, SEE DWG. D-8

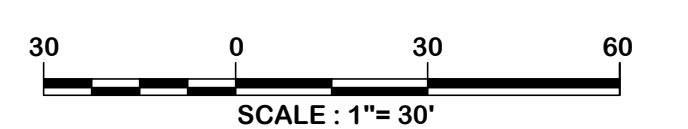


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Designer: BCM / LAW	Checked by: ADB
Technician: LAW	Project Number: 192930

DEMOLITION NOTES
AFTER MILLING THE RUNWAY ASPHALT. THE CONTRACTOR AND ENGINEER SHALL INSPECT THE REMAINING ASPHALT PAVEMENT CRACKS. CRACKS GREATER THAN 3/8" SHALL BE TREATED WITH NON-SHRINK GROUT AS DETAILED ON DRAWING TS-2. QUANTITIES WILL BE AGREED UPON BY ENGINEER AND CONTRACTOR PRIOR TO THE WORK BEING ACCOMPLISHED. AMOUNTS THAT WILL EXCEED THE BUDGETED CRACK REPAIR AMOUNT SHALL REQUIRE ENGINEER AND OWNER APPROVAL PRIOR TO PERFORMANCE.



ISSUED FOR BIDS

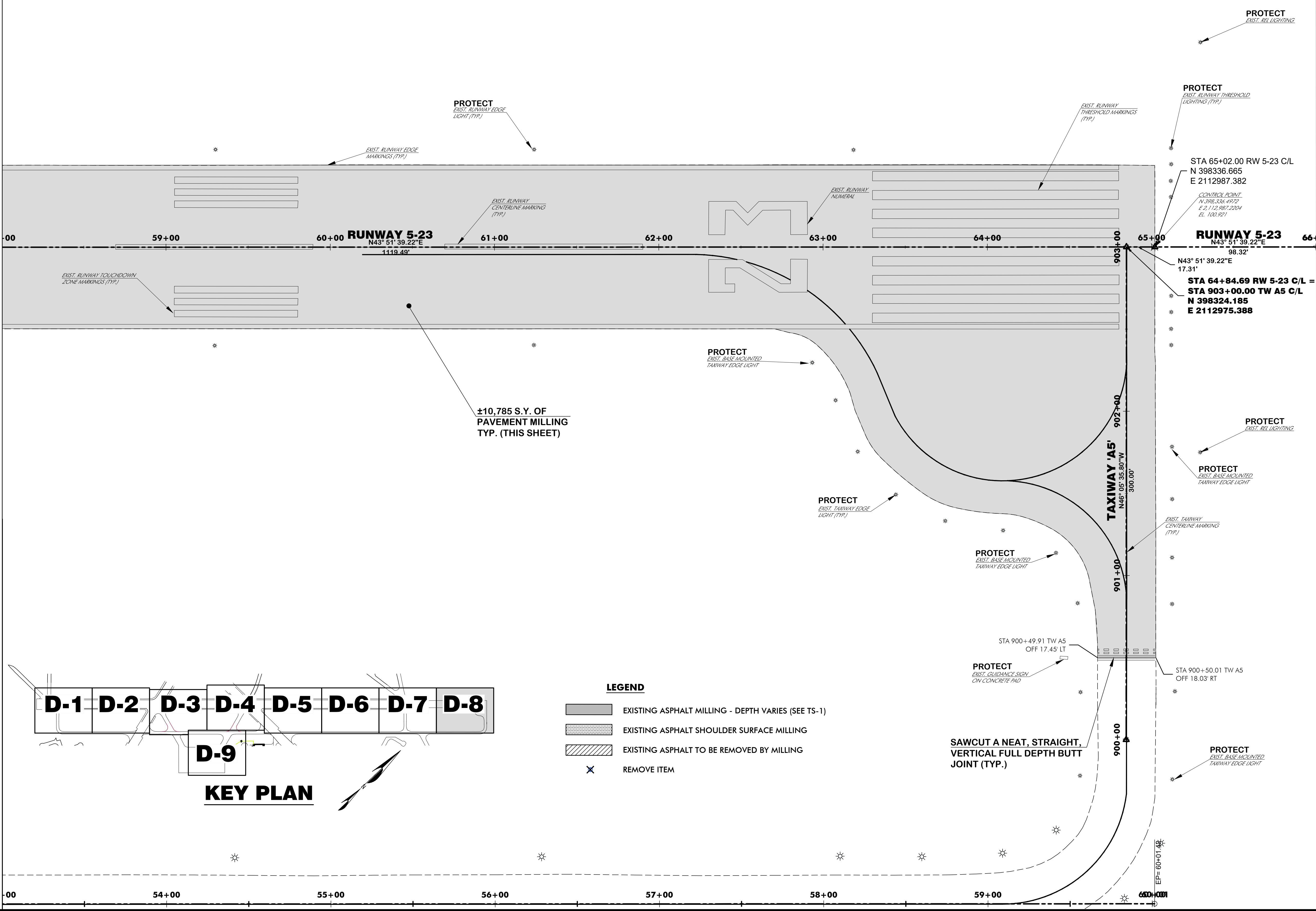
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
DEMOLITION PLAN NO. 8

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
Drawing Number: D-8	

MATCHLINE STA. 58+00, SEE DWG. D-7



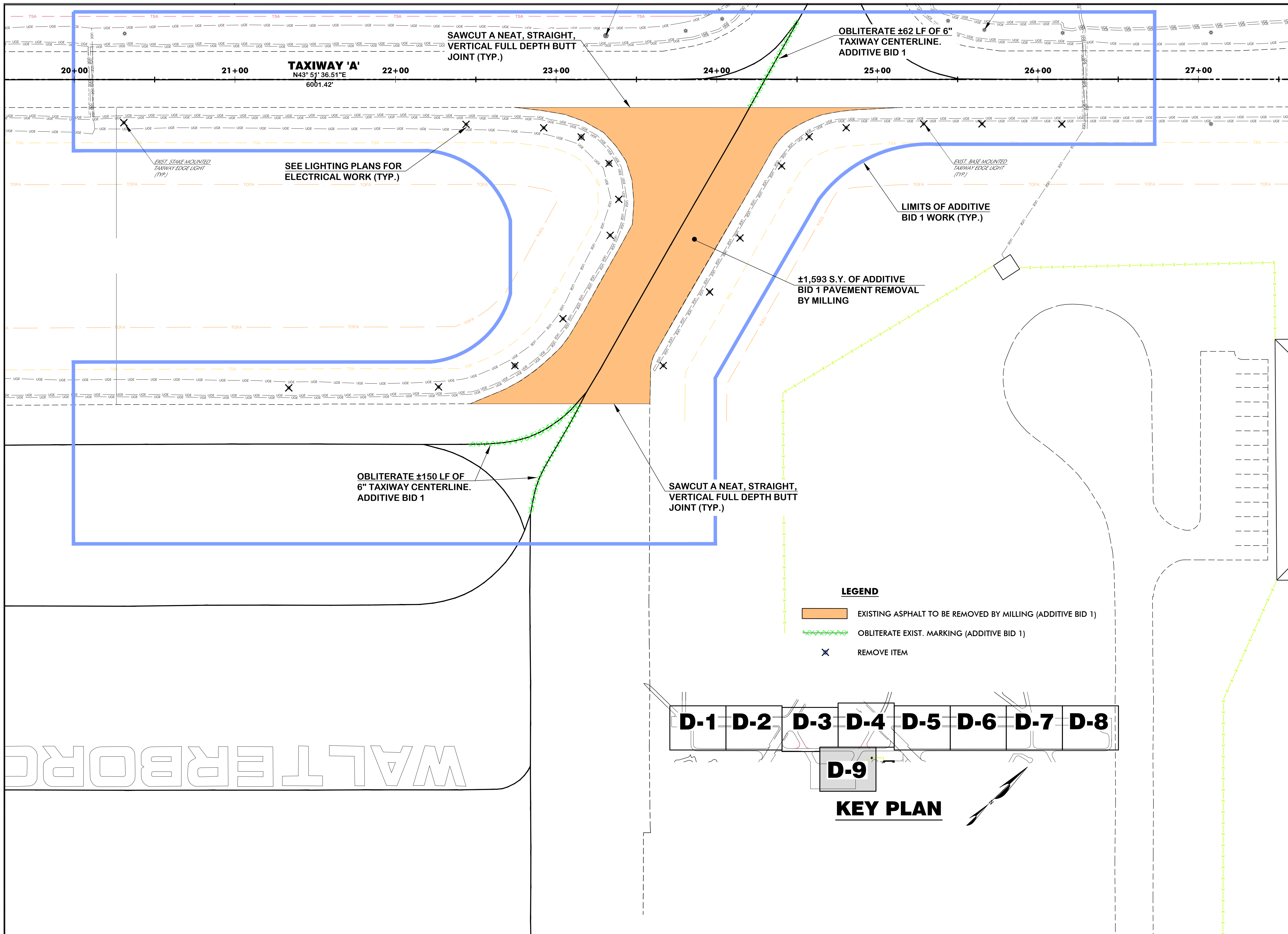
LEGEND

	EXISTING ASPHALT MILLING - DEPTH VARIES (SEE TS-1)
	EXISTING ASPHALT SHOULDER SURFACE MILLING
	EXISTING ASPHALT TO BE REMOVED BY MILLING
	REMOVE ITEM

SAWCUT A NEAT, STRAIGHT, VERTICAL FULL DEPTH BUTT JOINT (TYP.)

MATCHLINE SEE DWG. D-3

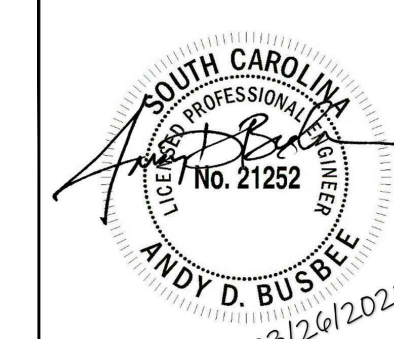
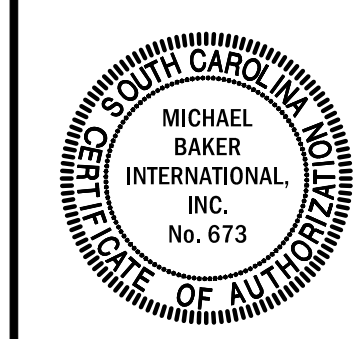
MATCHLINE SEE DWG. D-4



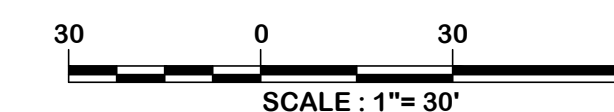
WALTERBORO, SOUTH CAROLINA

Michael Baker INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / LAW
Checked by: ADB
Technician: LAW
Project Number: 192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
DEMOLITION PLAN NO. 9

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number:
D-9

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Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

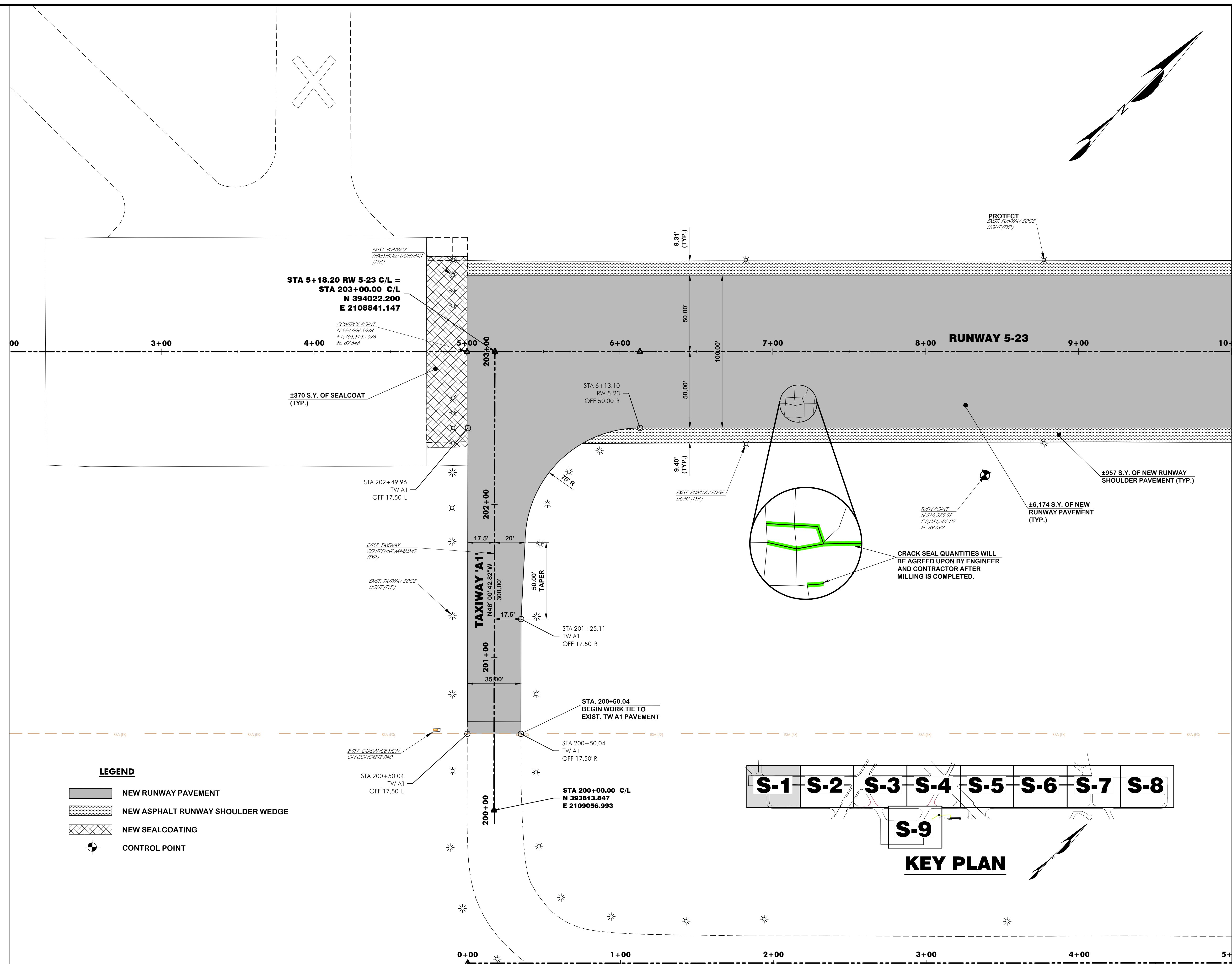
REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

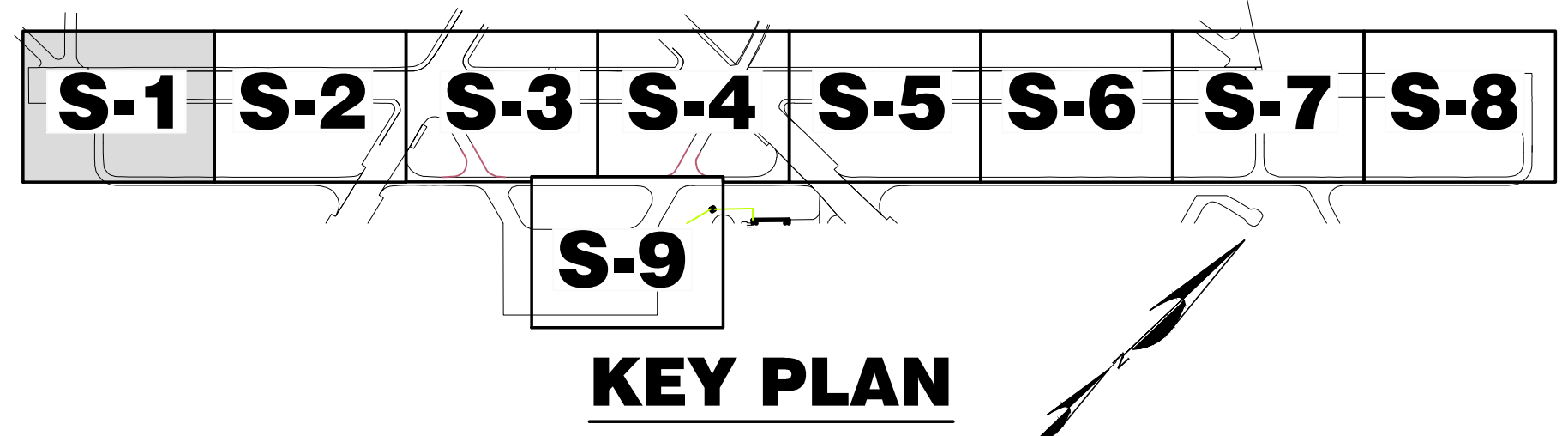
Drawing Name:
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FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: S-1
Drawing Number: S-1	

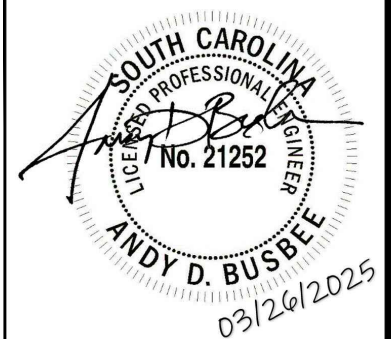
MATCHLINE STA. 10+00, SEE DWG. S-2



- LEGEND**
- NEW RUNWAY PAVEMENT
 - NEW ASPHALT RUNWAY SHOULDER WEDGE
 - NEW SEALCOATING
 - + CONTROL POINT



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Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

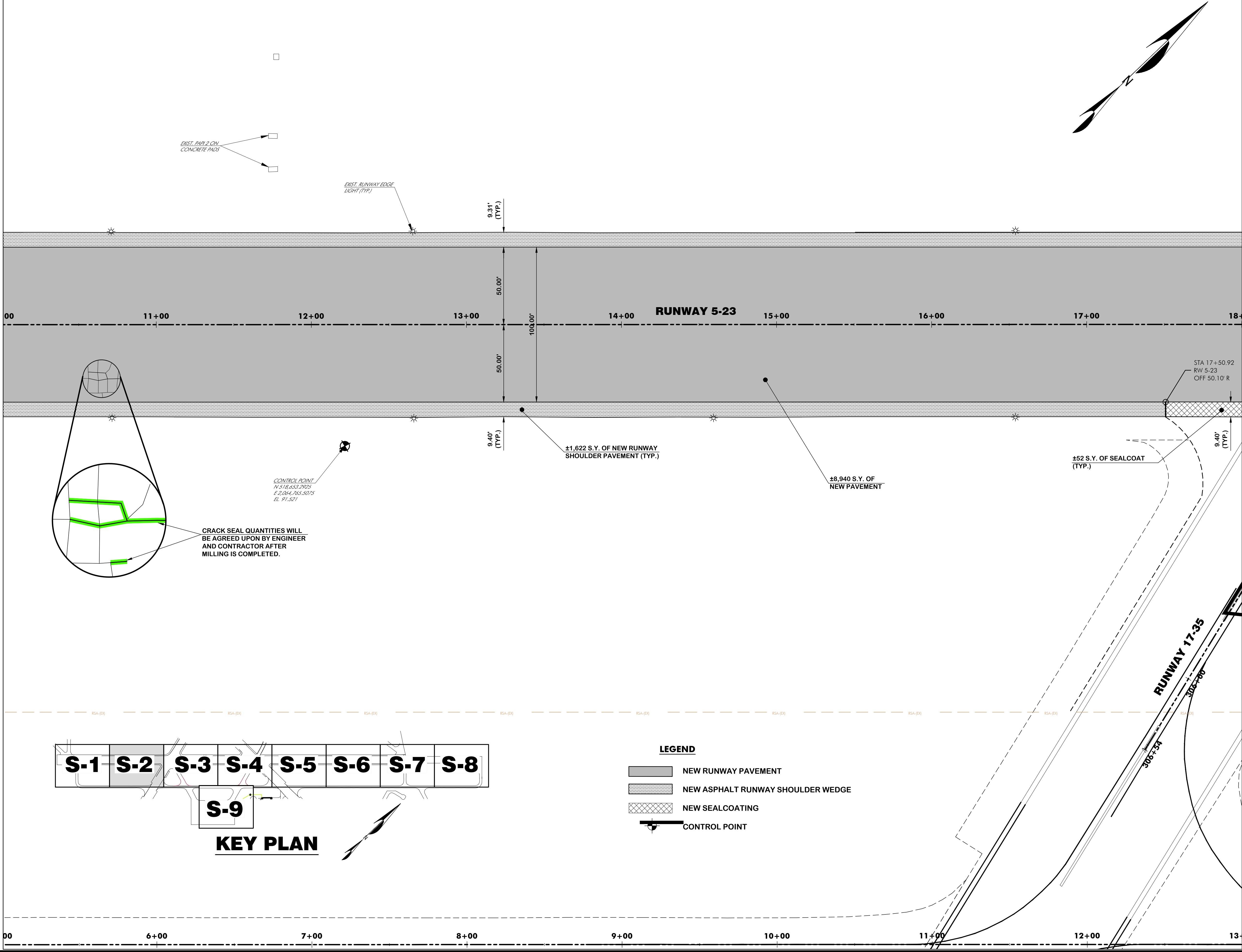
Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
STAKING PLAN NO. 2

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1" = 30'	Sheet Number: -
Drawing Number: S-2	

MATCHLINE STA. 10+00, SEE DWG. S-1

MATCHLINE STA. 18+00, SEE DWG. S-3



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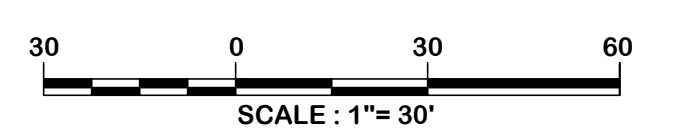
WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / NWM
Checked by: ADB
Technician: NWM
Project Number: 192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

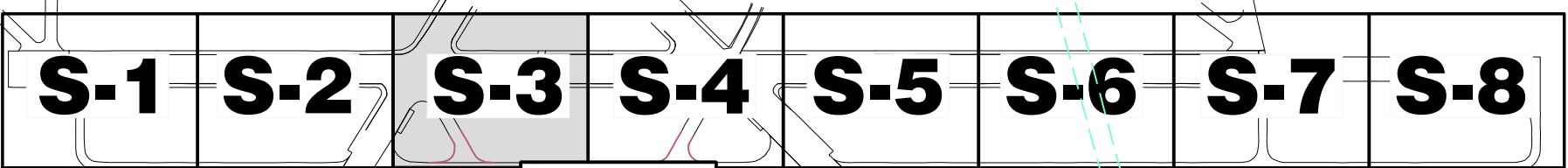
Drawing Name:
STAKING PLAN NO. 3

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number:
S-3

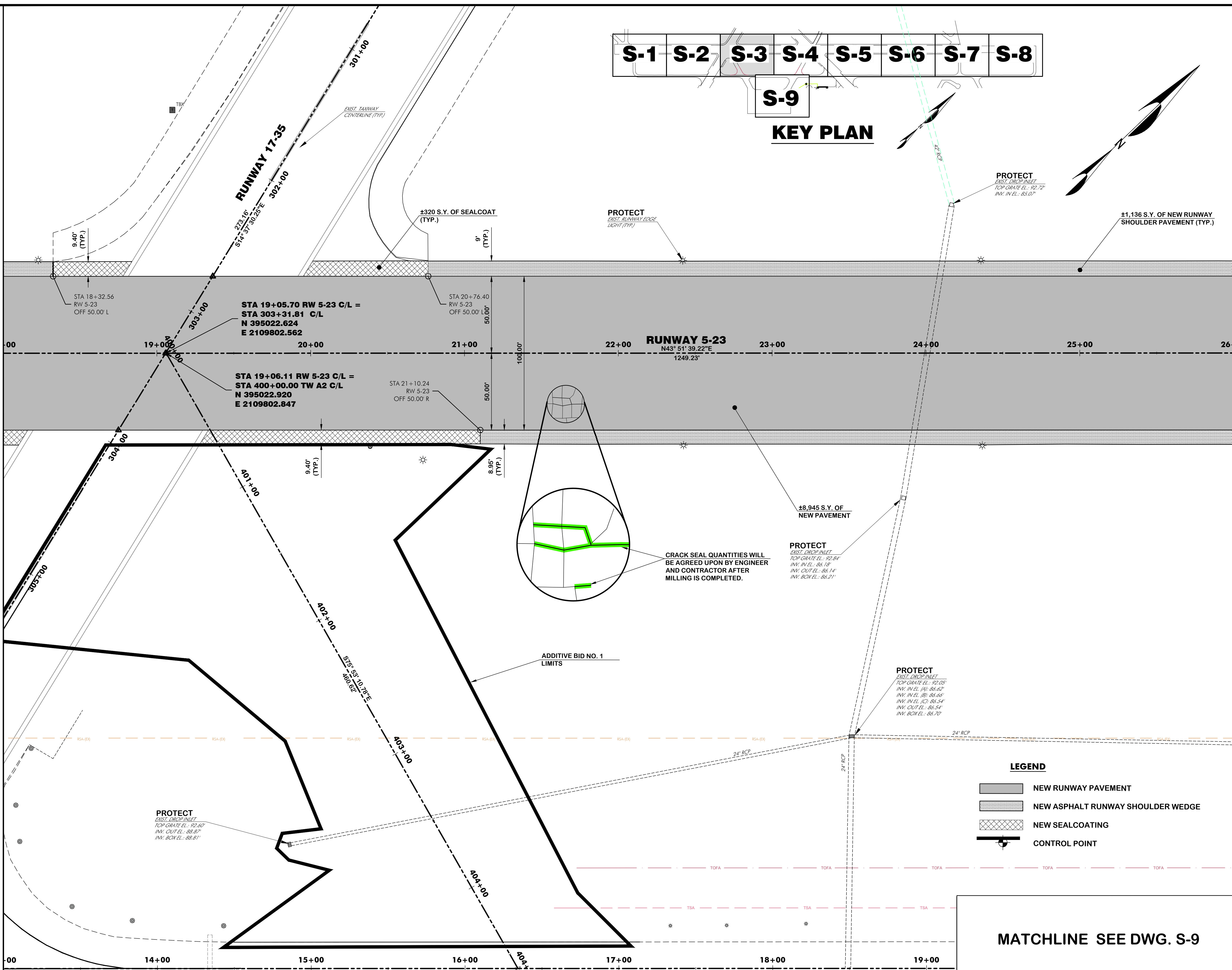


S-9

KEY PLAN

MATCHLINE STA. 18+00, SEE DWG. S-2

MATCHLINE STA. 26+00, SEE DWG. S-4



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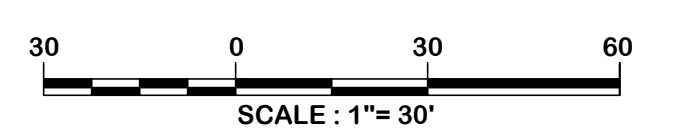
WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / NWM
Checked by: ADB
Technician: NWM
Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
STAKING PLAN NO. 4

FAA A.I.P. Project Number:
3-45-0057-025-2025
Date: **MARCH, 2025** Division: **AIRPORTS**
Scale: **1"= 30'** Sheet Number: **-**
Drawing Number: **S-4**

MATCHLINE STA. 28+00, SEE DWG. S-3

MATCHLINE STA. 34+00, SEE DWG. S-5

MATCHLINE SEE DWG. S-9

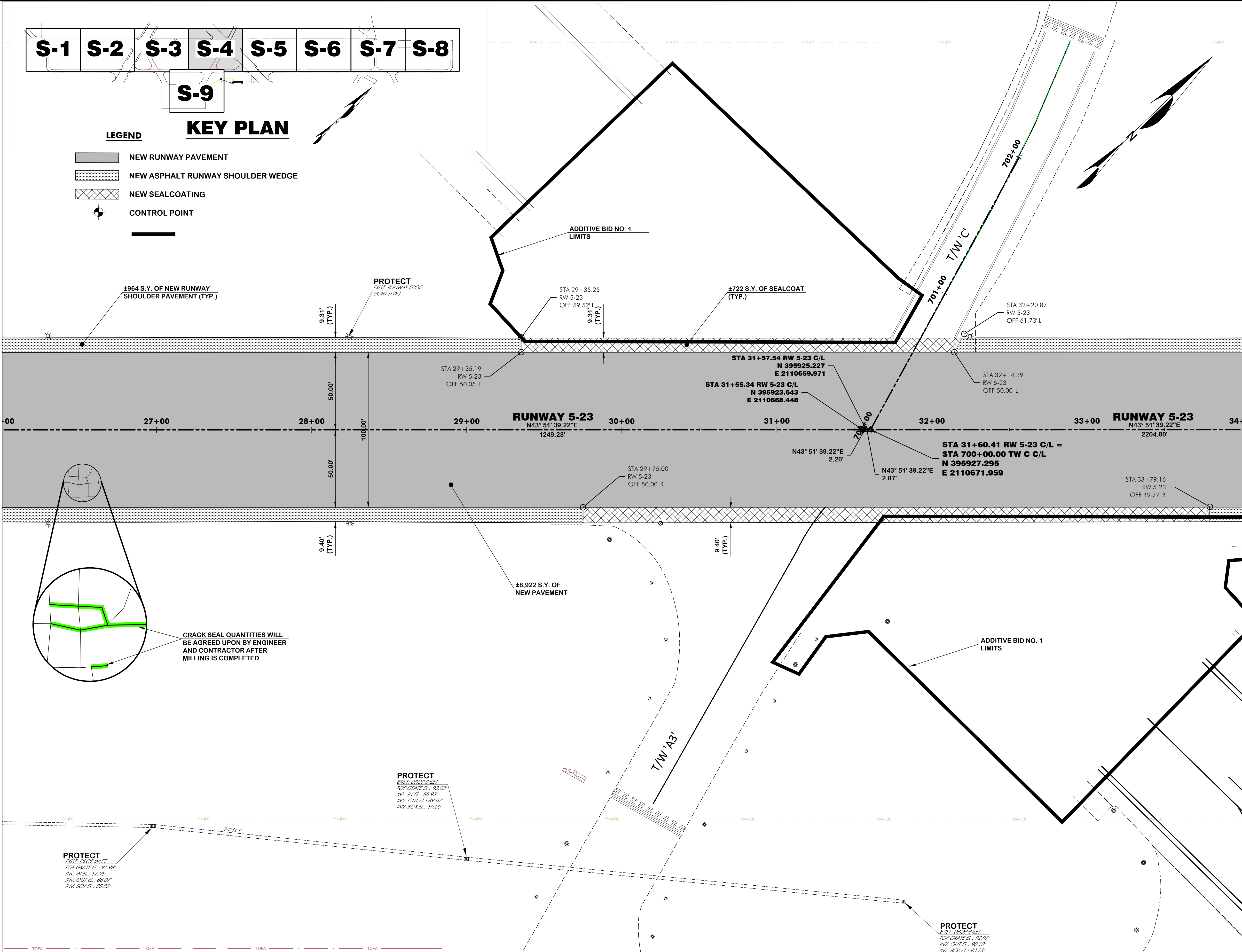
S-1 S-2 S-3 S-4 S-5 S-6 S-7 S-8

S-9

LEGEND

- NEW RUNWAY PAVEMENT
- NEW ASPHALT RUNWAY SHOULDER WEDGE
- NEW SEALCOATING
- CONTROL POINT

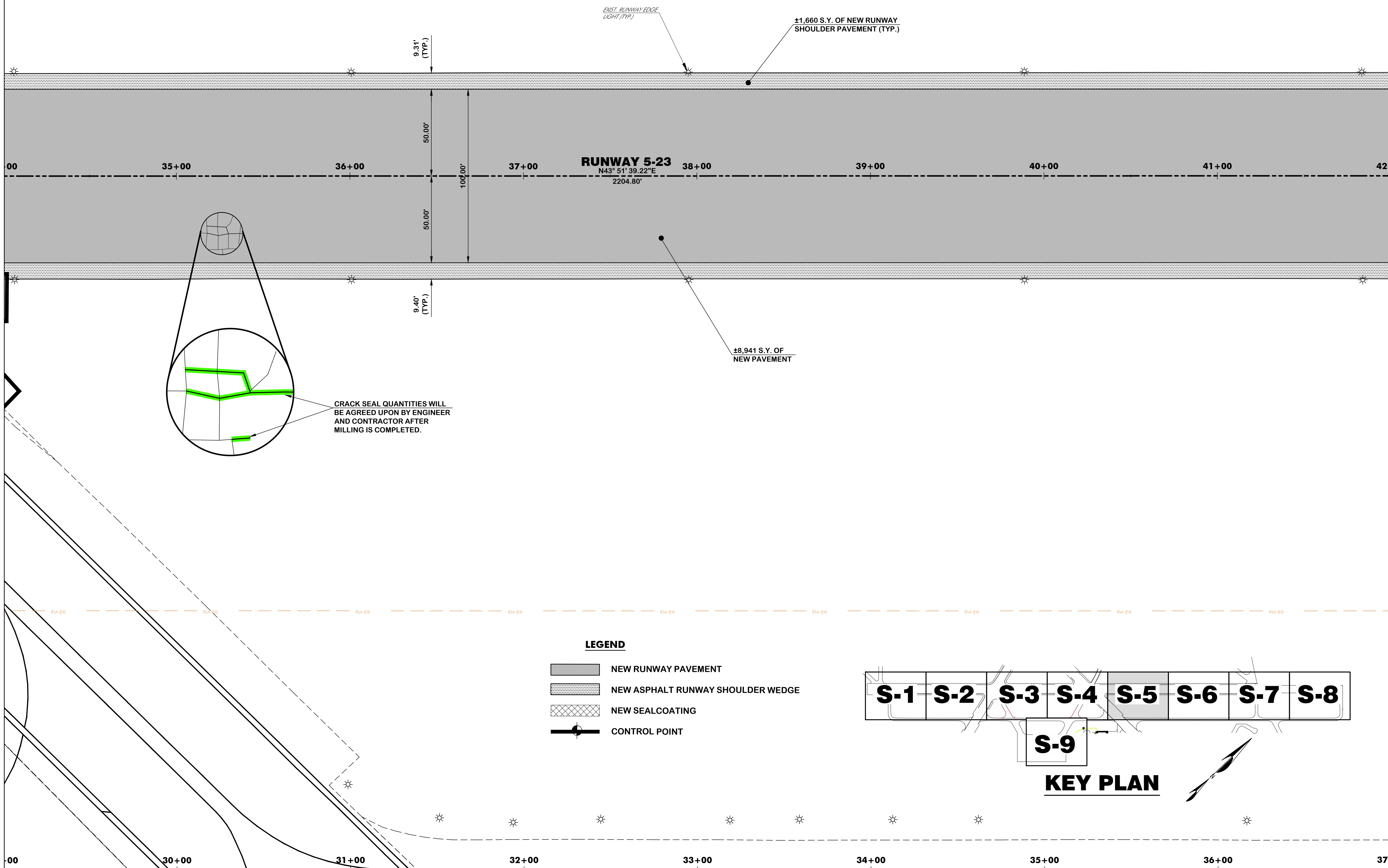
KEY PLAN



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MATCHLINE STA. 34+00, SEE DWG. S-4



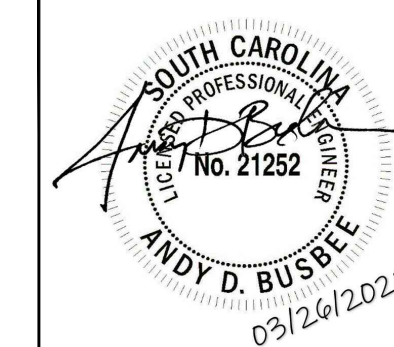
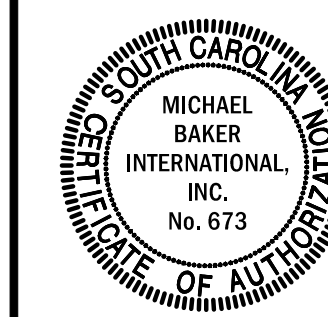
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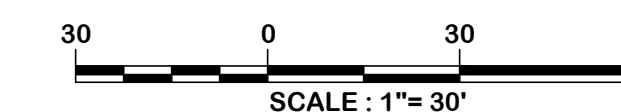
WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
STAKING PLAN NO. 5

FAA A.I.P. Project Number:
3-45-0057-025-2025

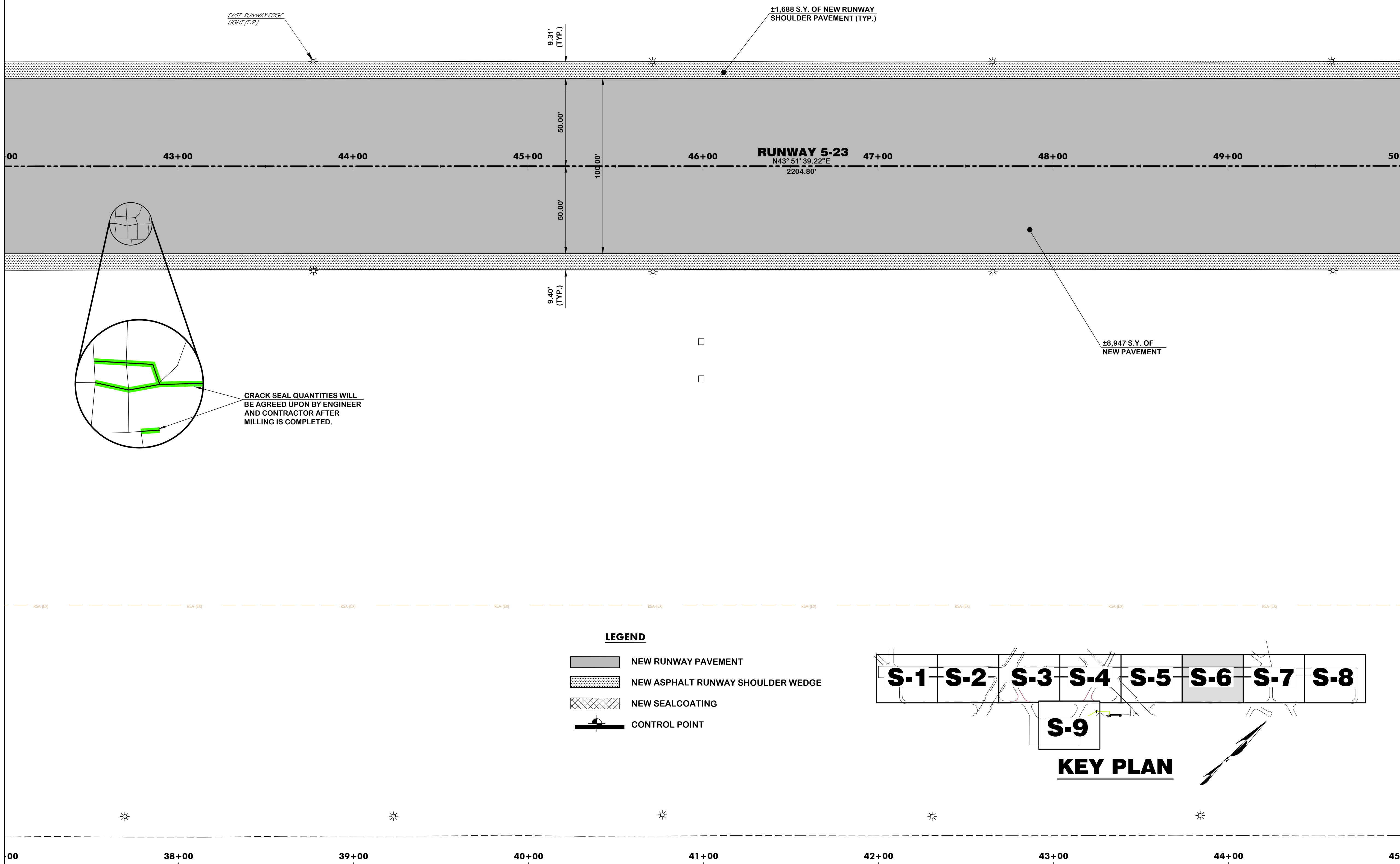
Date: MARCH, 2025	Division: AIRPORTS
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Scale: 1" = 30'	Sheet Number: -
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Drawing Number: S-5

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MATCHLINE STA. 42+00, SEE DWG. S-5



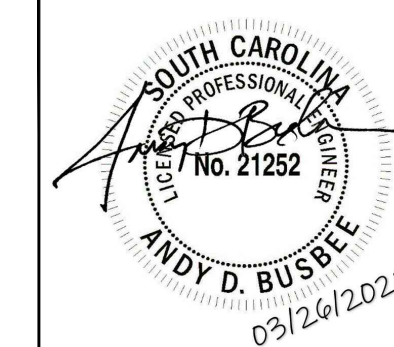
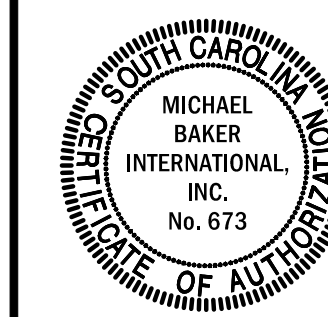
MATCHLINE STA. 50+00, SEE DWG. S-7



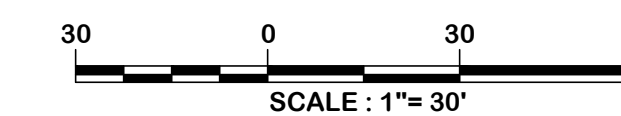
WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

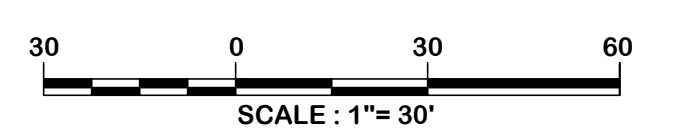
Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
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FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1" = 30'	Sheet Number: -
Drawing Number: S-6	



Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

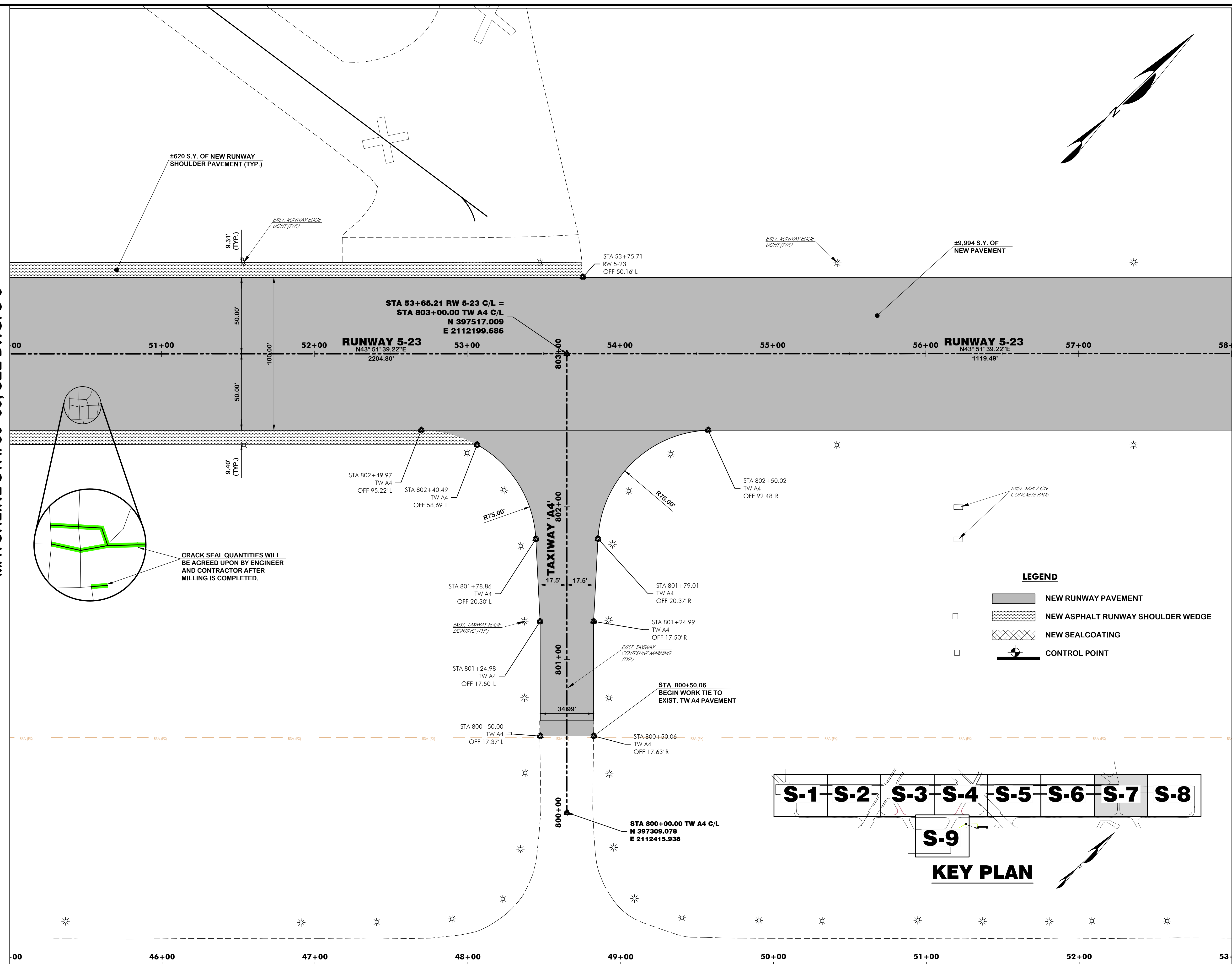
Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
STAKING PLAN NO. 7

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
	Drawing Number: S-7

MATCHLINE STA. 50+00, SEE DWG. S-6

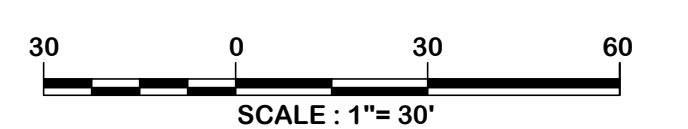
MATCHLINE STA. 58+00, SEE DWG. S-8



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Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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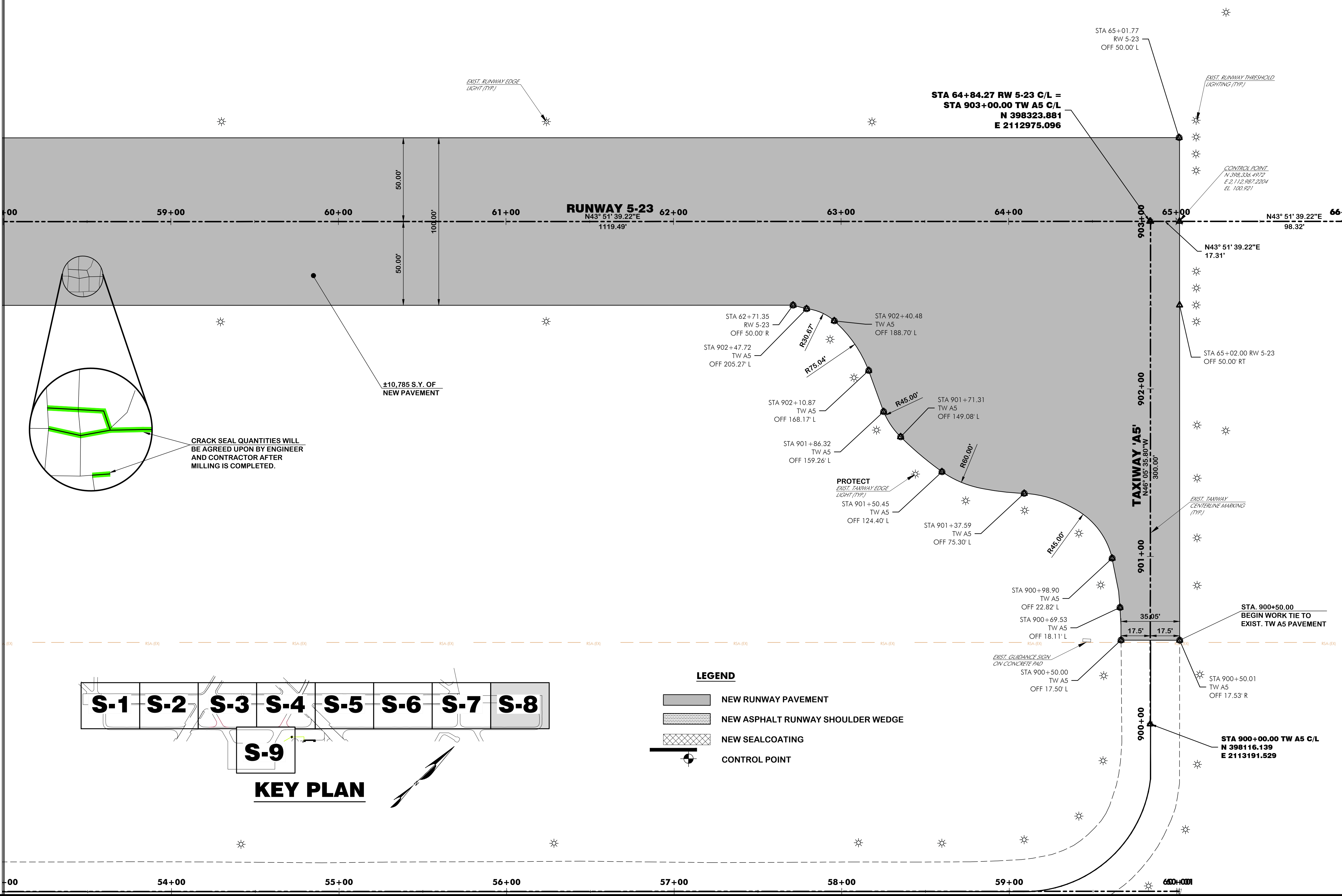
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
STAKING PLAN NO. 8

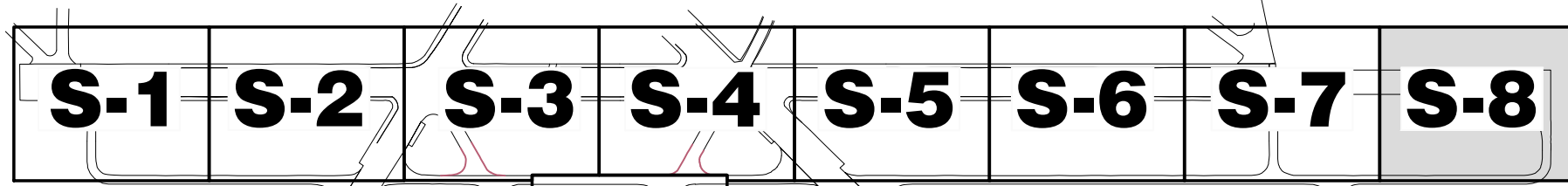
FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
Drawing Number: S-8	

MATCHLINE STA. 58+00, SEE DWG. S-7



LEGEND

- NEW RUNWAY PAVEMENT
- NEW ASPHALT RUNWAY SHOULDER WEDGE
- NEW SEALCOATING
- CONTROL POINT



KEY PLAN

Z:\Aviation\CLIENTS\walterboro lowcountry\gn\0205 rv 5-23 rehab\retail\02-drawings\RB\RW5-23-STAKE.dwg Modified: Mar 26, 2025 - 3:38pm Plotted: Mar 26, 2025 - 4:47pm Lauren Watson

MATCHLINE SEE DWG. S-3

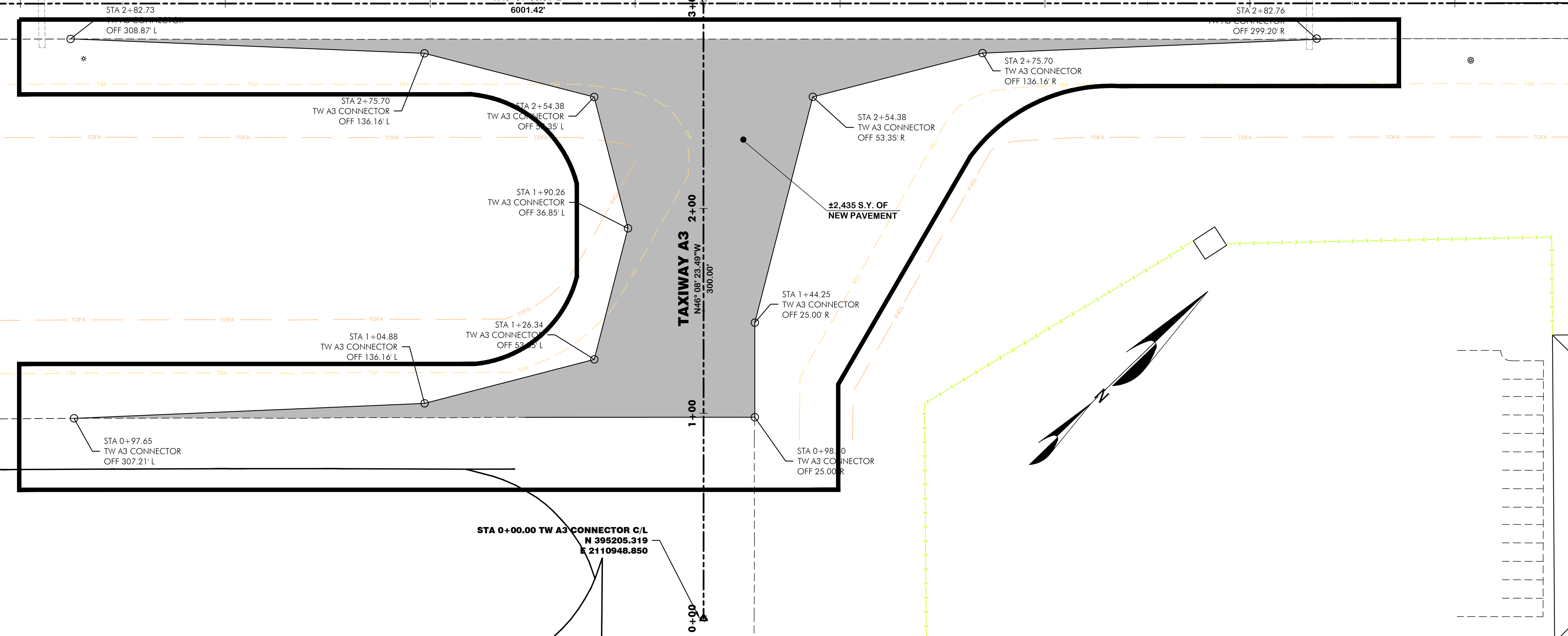
MATCHLINE SEE DWG. S-4

STA 23+33.34 PARALLEL TW A C/L =
STA 3+00.00 TW A3 CONNECTOR C/L
N 395414.595
E 2110733.891

TAXIWAY A
N43° 51' 36.51" E
6001.42'

TAXIWAY A3
N46° 09' 23.49" W
300.00'

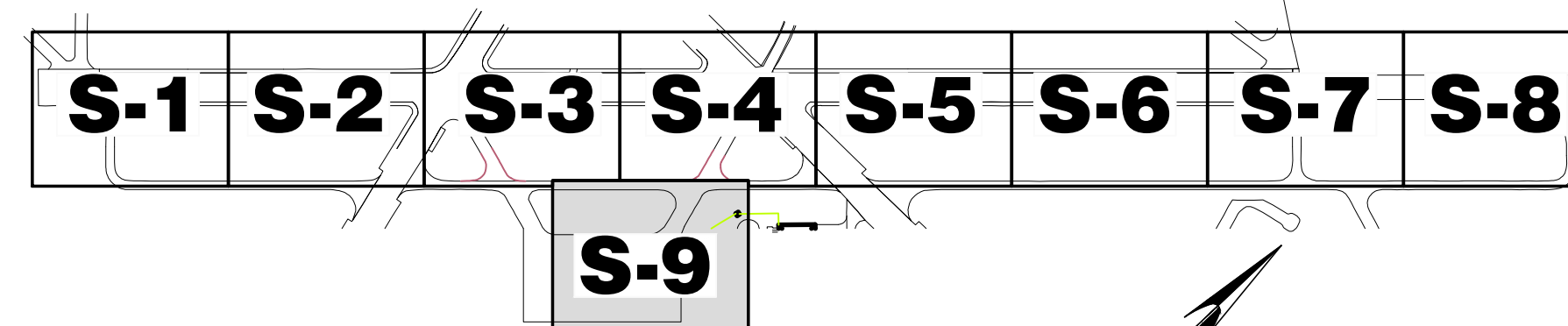
STA 0+00.00 TW A3 CONNECTOR C/L
N 395205.319
E 2110948.850



LEGEND

NEW TAXIWAY PAVEMENT

CONTROL POINT



KEY PLAN



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Columbia, South Carolina 29201

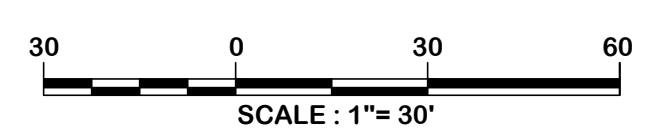


Designer:
BCM / NWM

Checked by:
ADB

Technician:
NWM

Project Number:
192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**STAKING PLAN NO. 9
MIDFIELD TAXIWAY A3**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number:
S-9

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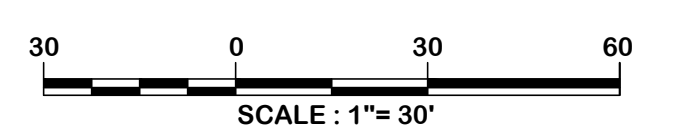


Designer:
BCM / NWM

Checked by:
ADB

Technician:
NWM

Project Number:
192930



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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**SEDIMENT AND EROSION
CONTROL PLAN NO. 1**

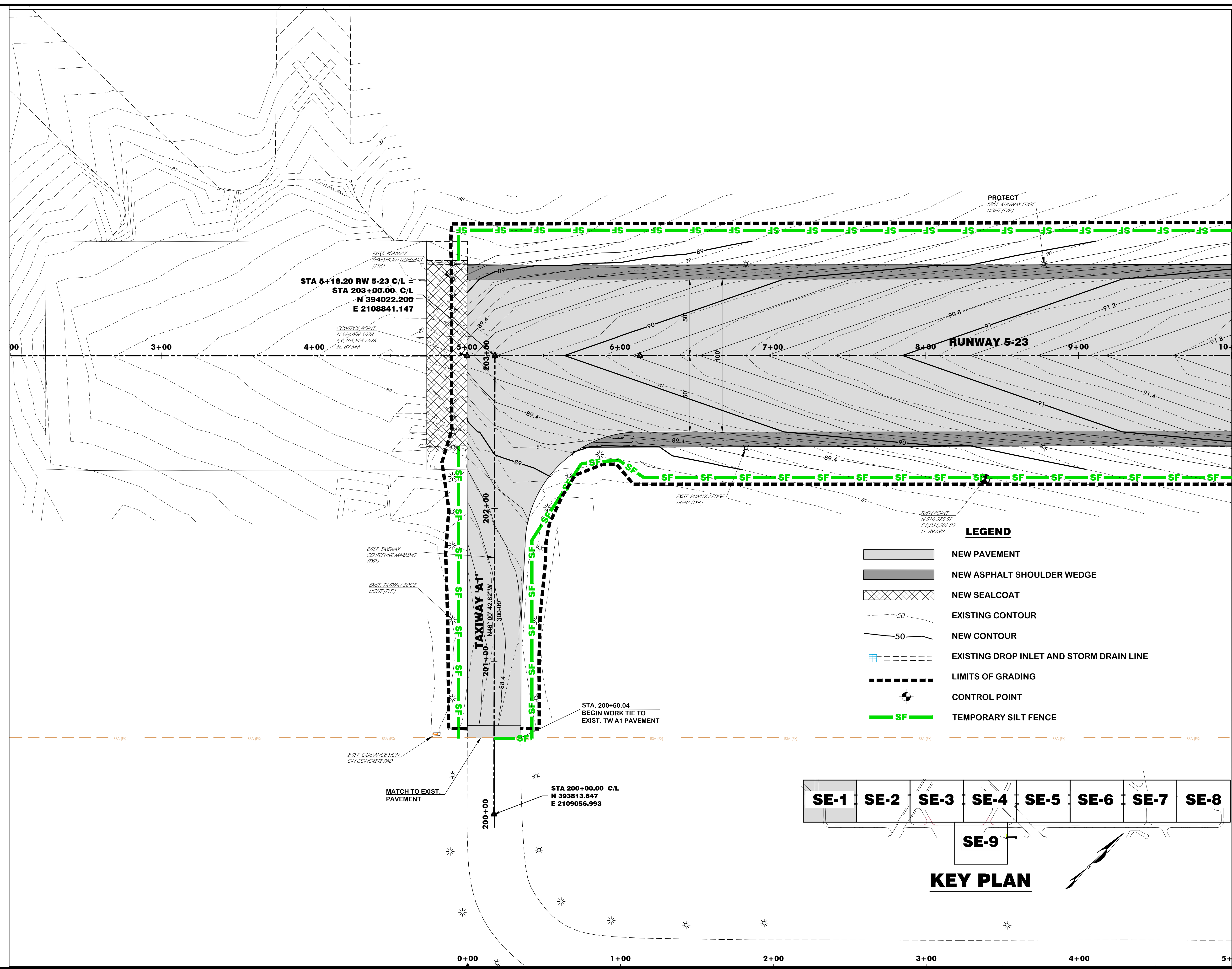
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3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number:

Drawing Number:
SE-1

MATCHLINE STA. 10+00, SEE DWG. SE-2



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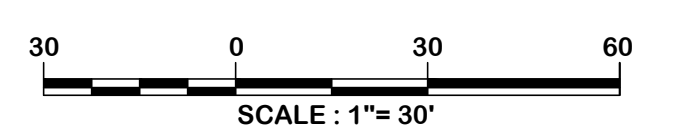


Designer:
BCM / NWM

Checked by:
ADB

Technician:
NWM

Project Number:
192930



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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**SEDIMENT AND EROSION
CONTROL PLAN NO. 2**

FAA A.I.P. Project Number:
3-45-0057-025-2025

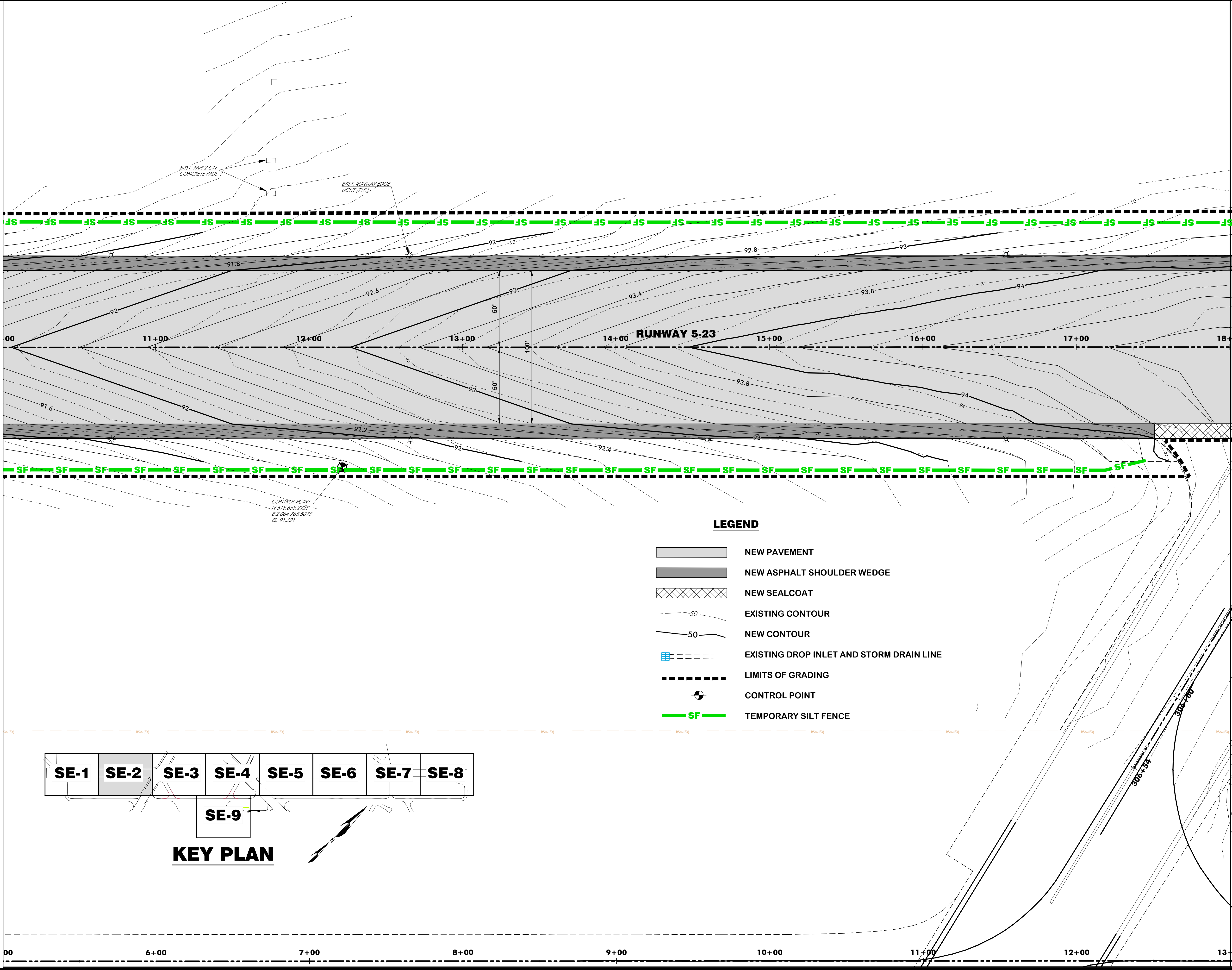
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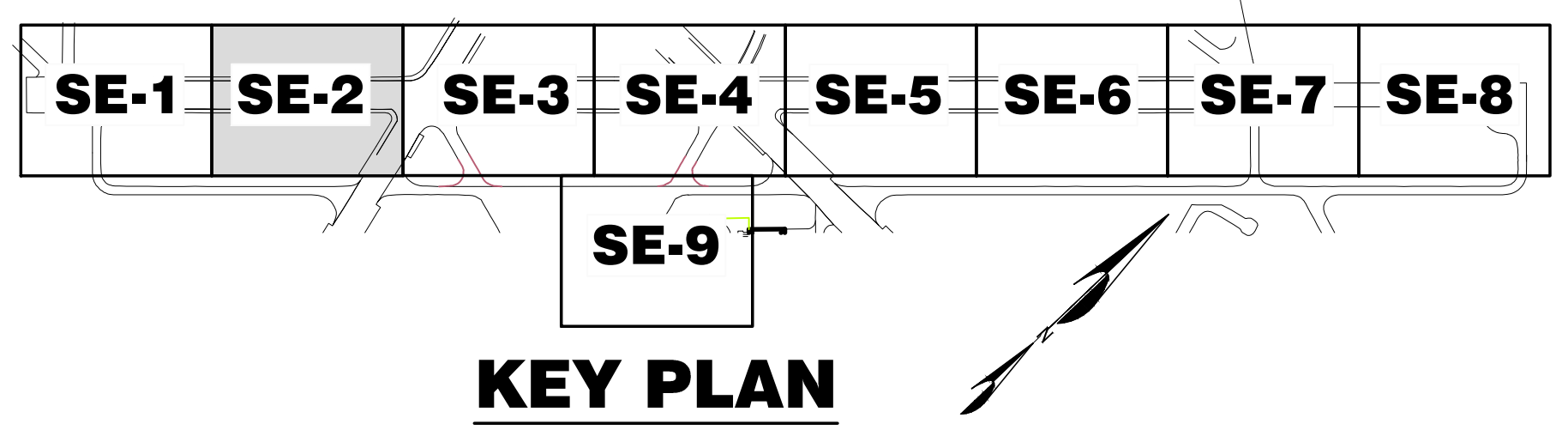
Drawing Number:
SE-2

MATCHLINE STA. 10+00, SEE DWG. SE-1

MATCHLINE STA. 18+00, SEE DWG. SE-3



- LEGEND**
- NEW PAVEMENT
 - NEW ASPHALT SHOULDER WEDGE
 - NEW SEALCOAT
 - EXISTING CONTOUR
 - NEW CONTOUR
 - EXISTING DROP INLET AND STORM DRAIN LINE
 - LIMITS OF GRADING
 - CONTROL POINT
 - TEMPORARY SILT FENCE



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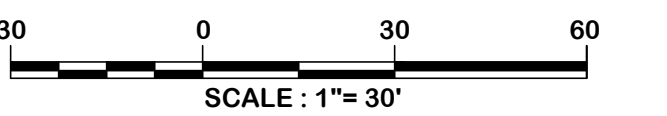


Designer:
BCM / NWM

Checked by:
ADB

Technician:
NWM

Project Number:
192930



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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**SEDIMENT AND EROSION
CONTROL PLAN NO. 3**

FAA A.I.P. Project Number:
3-45-0057-025-2025

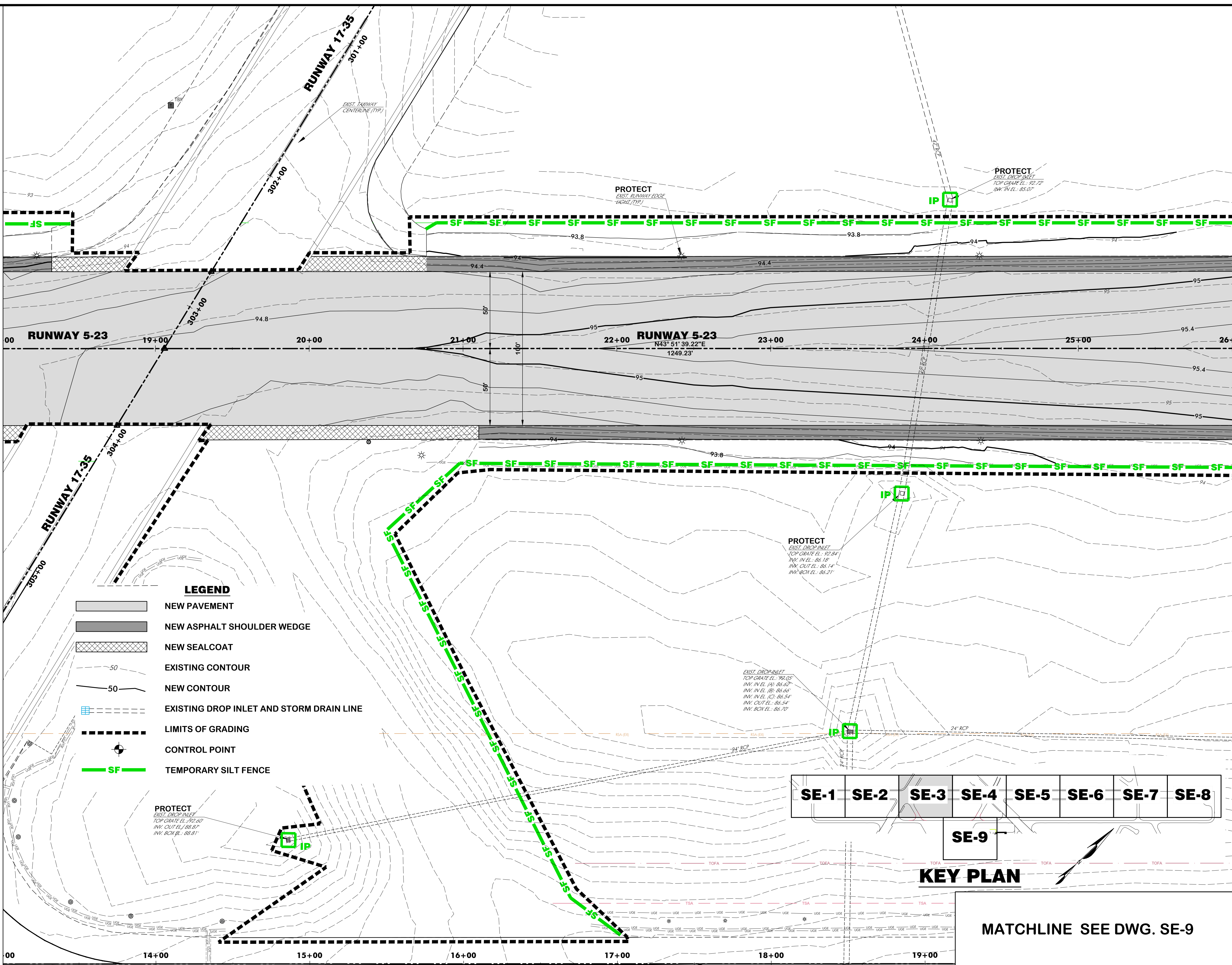
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Drawing Number:
SE-3

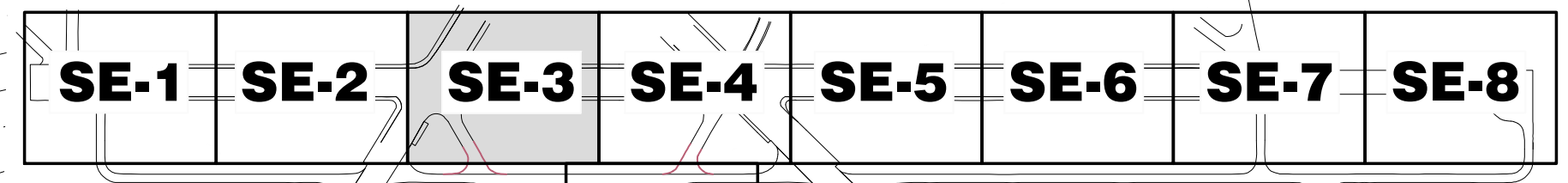
MATCHLINE STA. 18+00, SEE DWG. SE-2

MATCHLINE STA. 26+00, SEE DWG. SE-4



LEGEND

	NEW PAVEMENT
	NEW ASPHALT SHOULDER WEDGE
	NEW SEALCOAT
	EXISTING CONTOUR
	NEW CONTOUR
	EXISTING DROP INLET AND STORM DRAIN LINE
	LIMITS OF GRADING
	CONTROL POINT
	TEMPORARY SILT FENCE



KEY PLAN

MATCHLINE SEE DWG. SE-9

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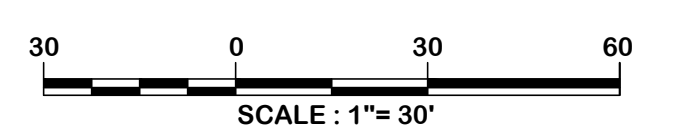


Designer:
BCM / NWM

Checked by:
ADB

Technician:
NWM

Project Number:
192930



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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**SEDIMENT AND EROSION
CONTROL PLAN NO. 4**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

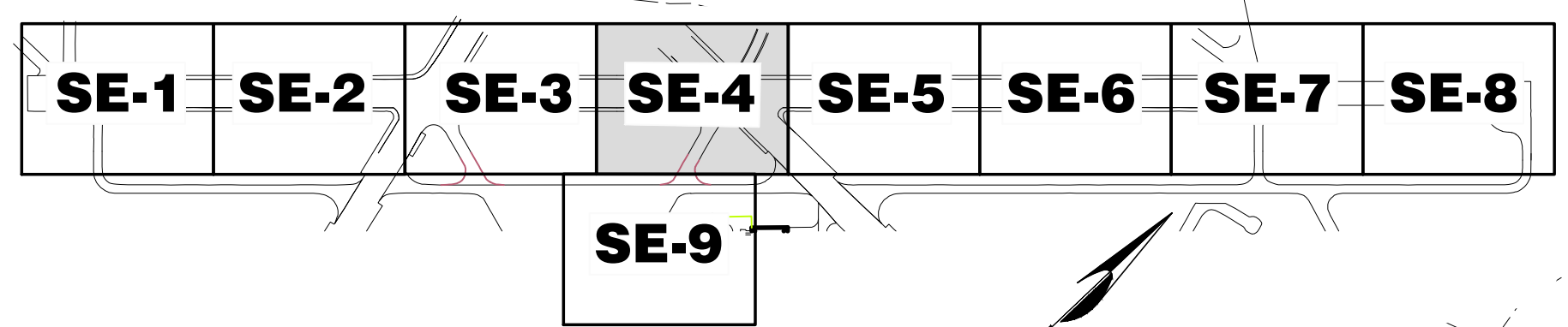
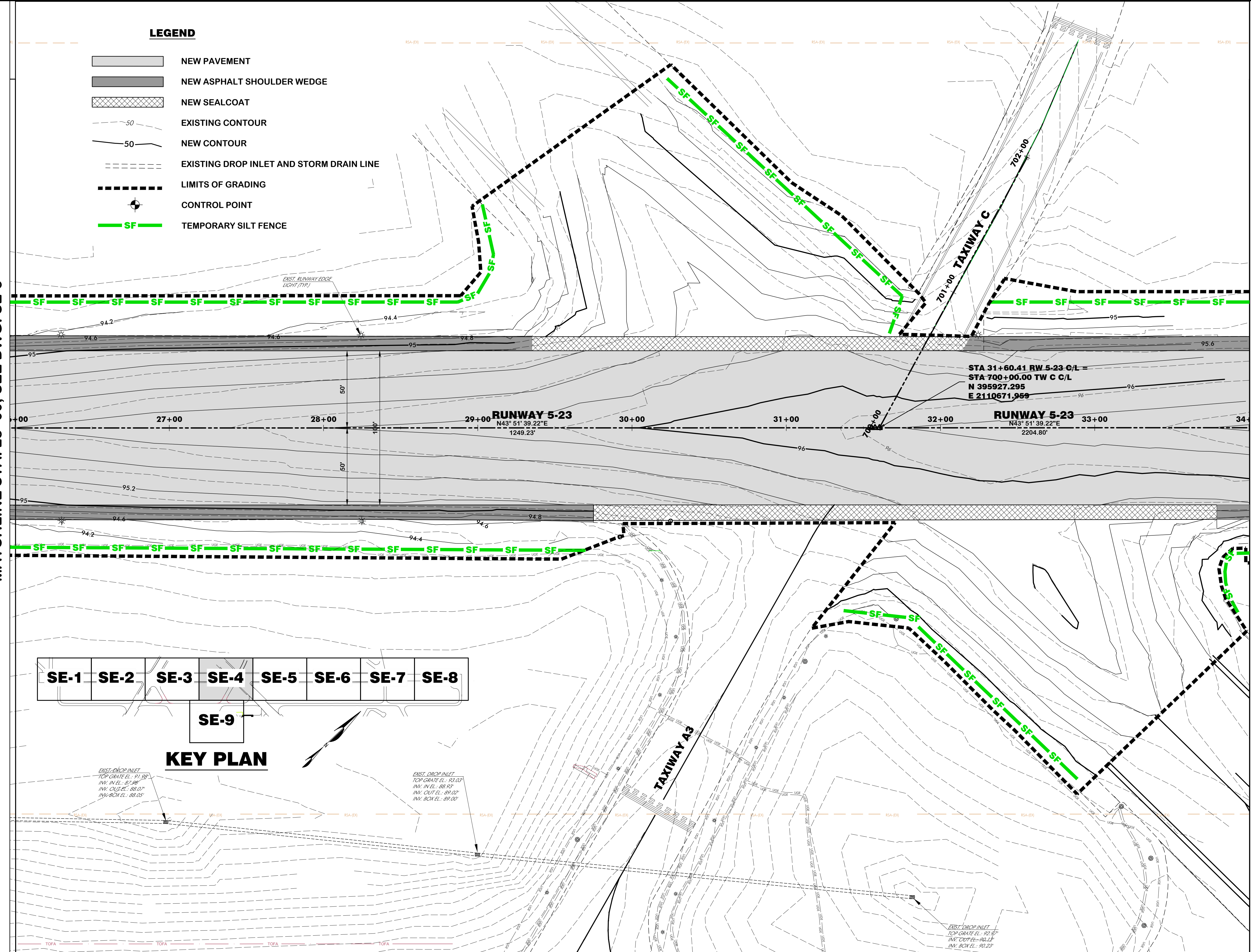
Drawing Number:
SE-4

LEGEND

- NEW PAVEMENT
- NEW ASPHALT SHOULDER WEDGE
- NEW SEALCOAT
- EXISTING CONTOUR
- NEW CONTOUR
- EXISTING DROP INLET AND STORM DRAIN LINE
- LIMITS OF GRADING
- CONTROL POINT
- TEMPORARY SILT FENCE

MATCHLINE STA. 28+00, SEE DWG. SE-3

MATCHLINE STA. 34+00, SEE DWG. SE-5



KEY PLAN

*EXIST. DROP INLET
TOP GRATE EL.: 91.98'
INV. IN. EL.: 87.98'
INV. OUT. EL.: 88.07'
INV. BCM EL.: 88.05'*

*EXIST. DROP INLET
TOP GRATE EL.: 93.03'
INV. IN. EL.: 88.53'
INV. OUT. EL.: 89.02'
INV. BCM EL.: 89.00'*

MATCHLINE SEE DWG. SE-9

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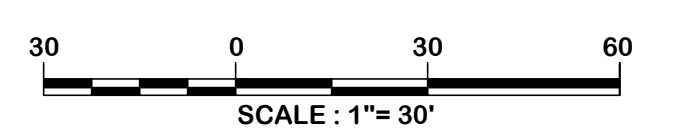
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Designer: BCM / NWM
Checked by: ADB
Technician: NWM
Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

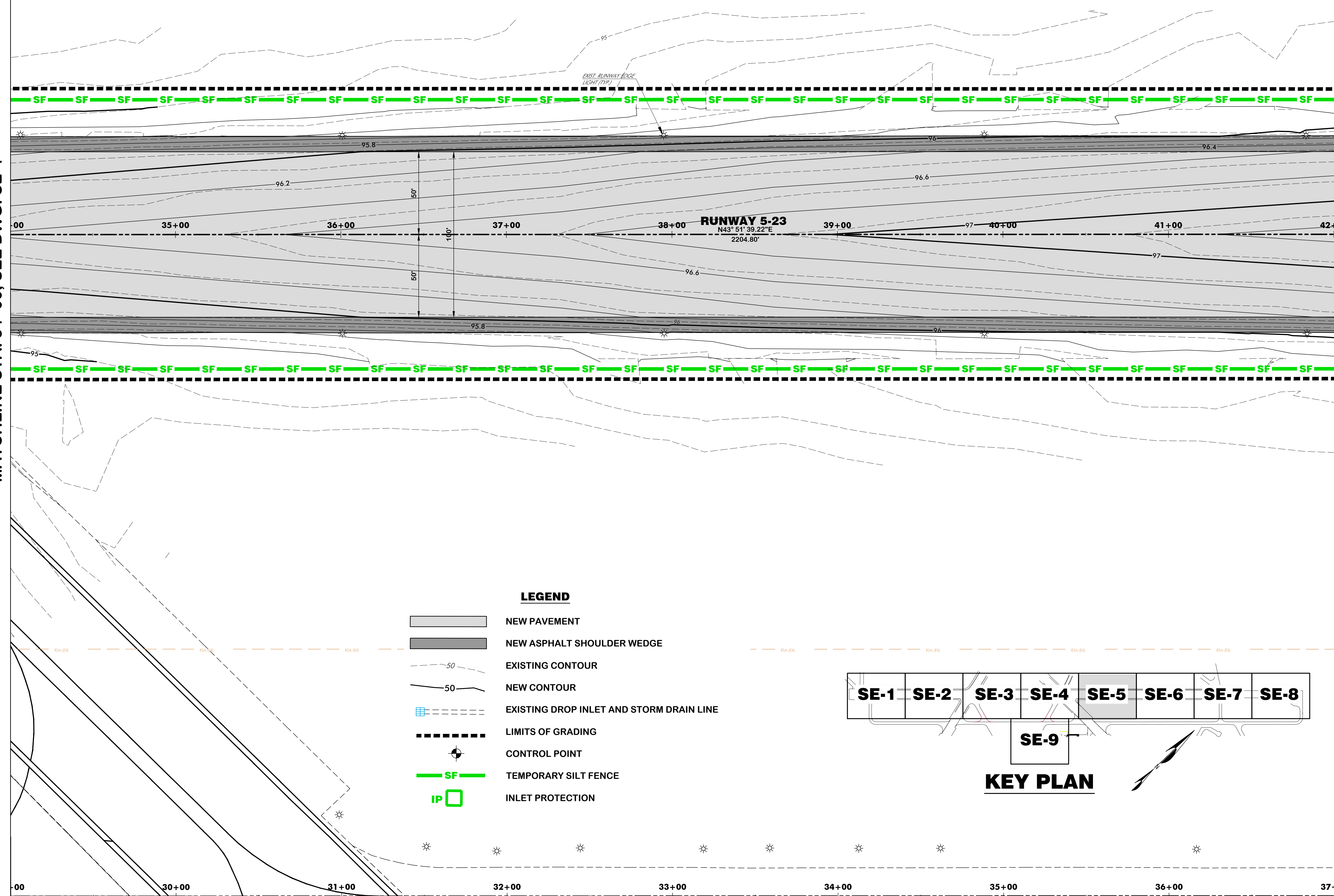
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**SEDIMENT AND EROSION
CONTROL PLAN NO. 5**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: AIRPORTS
Scale: 1" = 30'	Sheet Number: -
	Drawing Number: SE-5

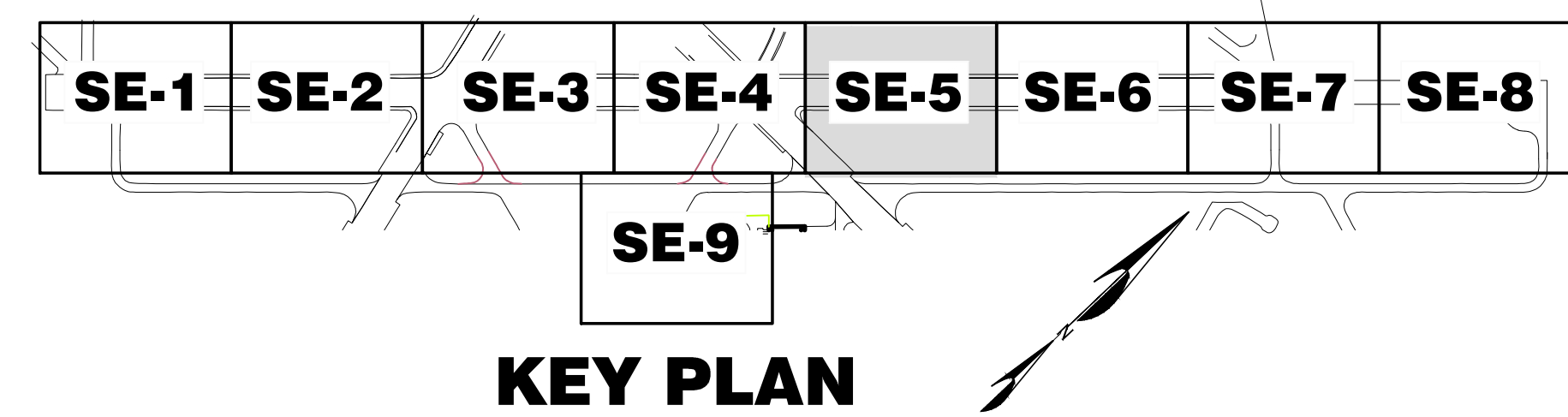
MATCHLINE STA. 34+00, SEE DWG. SE-4

MATCHLINE STA. 42+00, SEE DWG. SE-6



LEGEND

- NEW PAVEMENT
- NEW ASPHALT SHOULDER WEDGE
- EXISTING CONTOUR
- NEW CONTOUR
- EXISTING DROP INLET AND STORM DRAIN LINE
- LIMITS OF GRADING
- CONTROL POINT
- TEMPORARY SILT FENCE
- INLET PROTECTION



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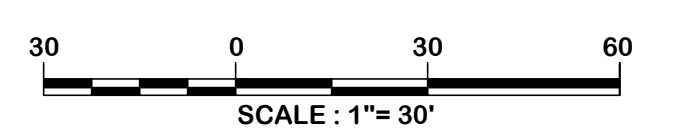
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Checked by: ADB
Technician: NWM
Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

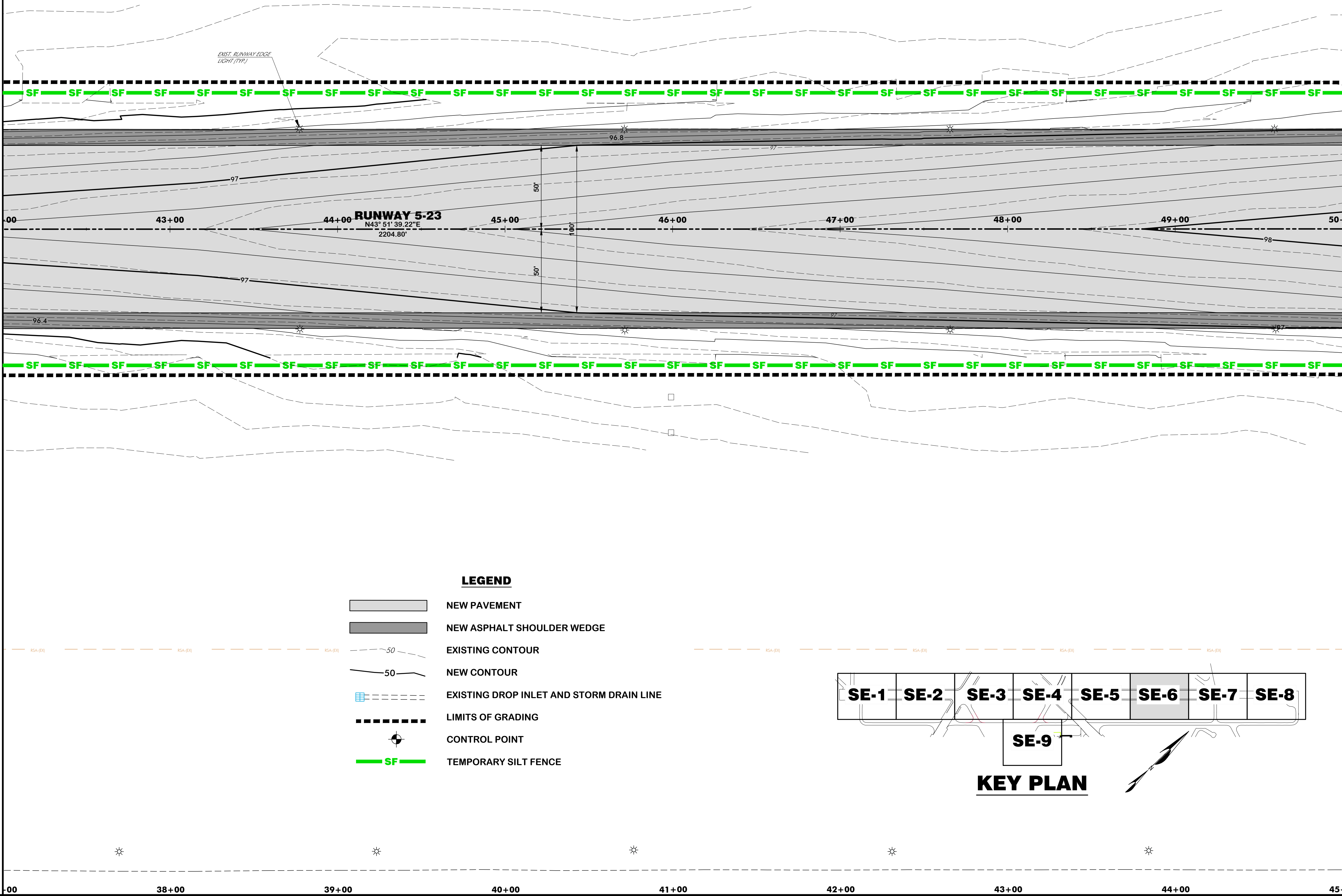
Drawing Name:
SEDIMENT AND EROSION CONTROL PLAN NO. 6

FAA A.I.P. Project Number:
3-45-0057-025-2025

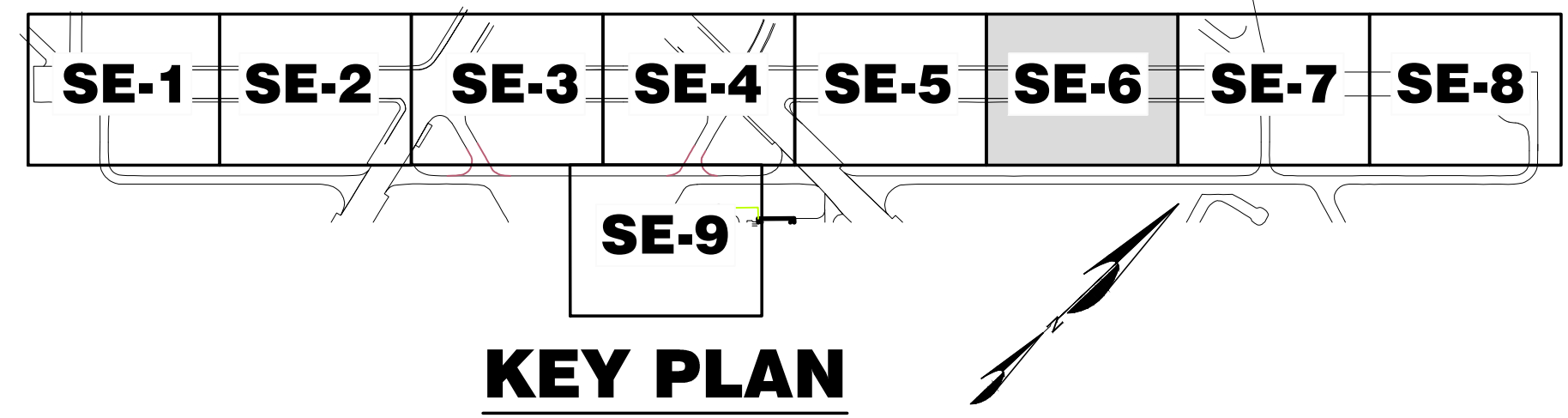
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
Drawing Number: SE-6	

MATCHLINE STA. 42+00, SEE DWG. SE-5

MATCHLINE STA. 50+00, SEE DWG. SE-7



- LEGEND**
- NEW PAVEMENT
 - NEW ASPHALT SHOULDER WEDGE
 - EXISTING CONTOUR
 - NEW CONTOUR
 - EXISTING DROP INLET AND STORM DRAIN LINE
 - LIMITS OF GRADING
 - CONTROL POINT
 - TEMPORARY SILT FENCE



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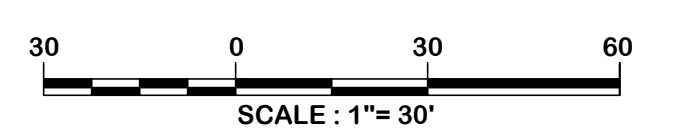
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Checked by: ADB
Technician: NWM
Project Number: 192930



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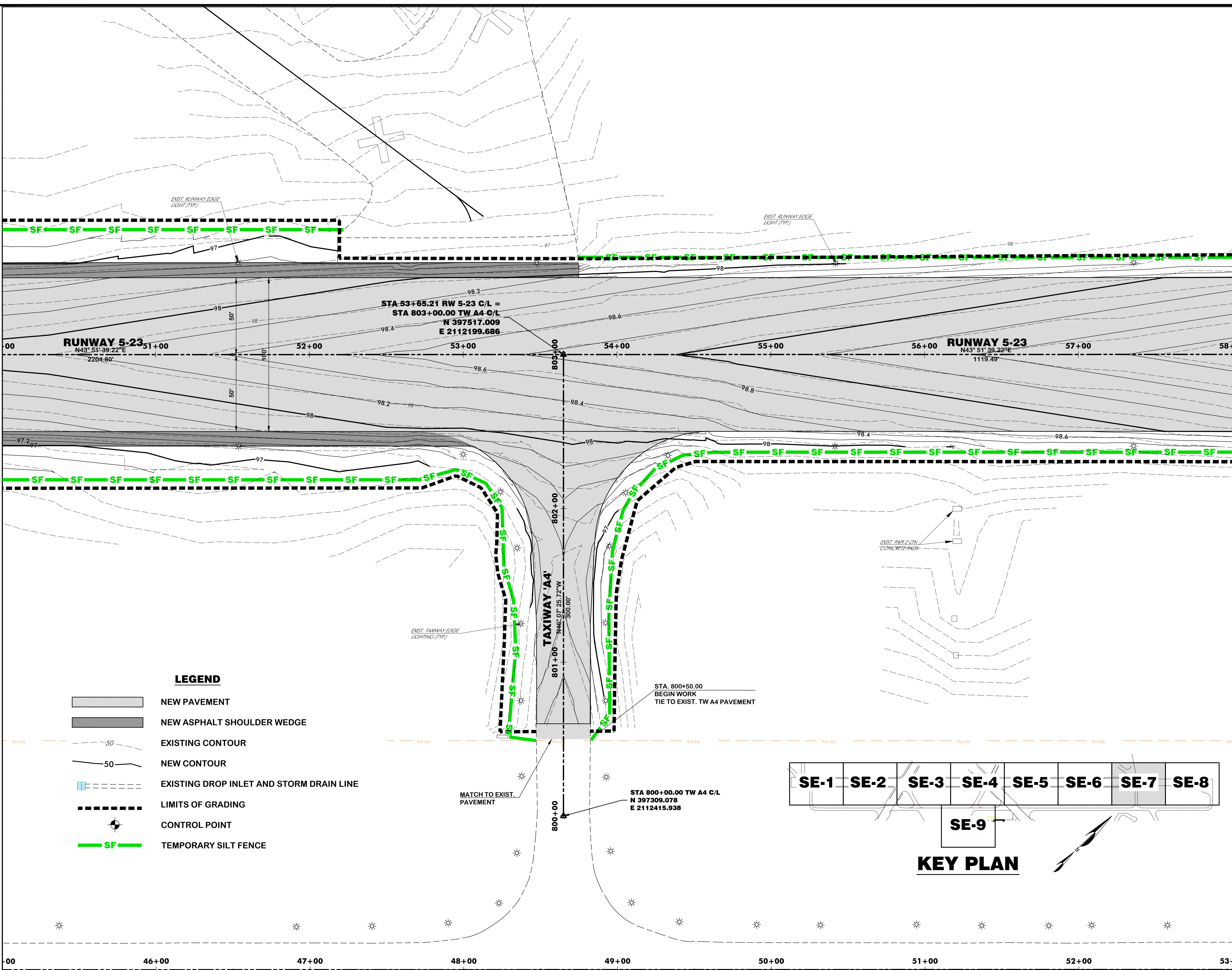
Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**SEDIMENT AND EROSION
CONTROL PLAN NO. 7**

FAA A.I.P. Project Number:
3-45-0057-025-2025
Date: **MARCH, 2025**
Scale: **1"= 30'**
Division: **AIRPORTS**
Sheet Number: **-**
Drawing Number: **SE-7**

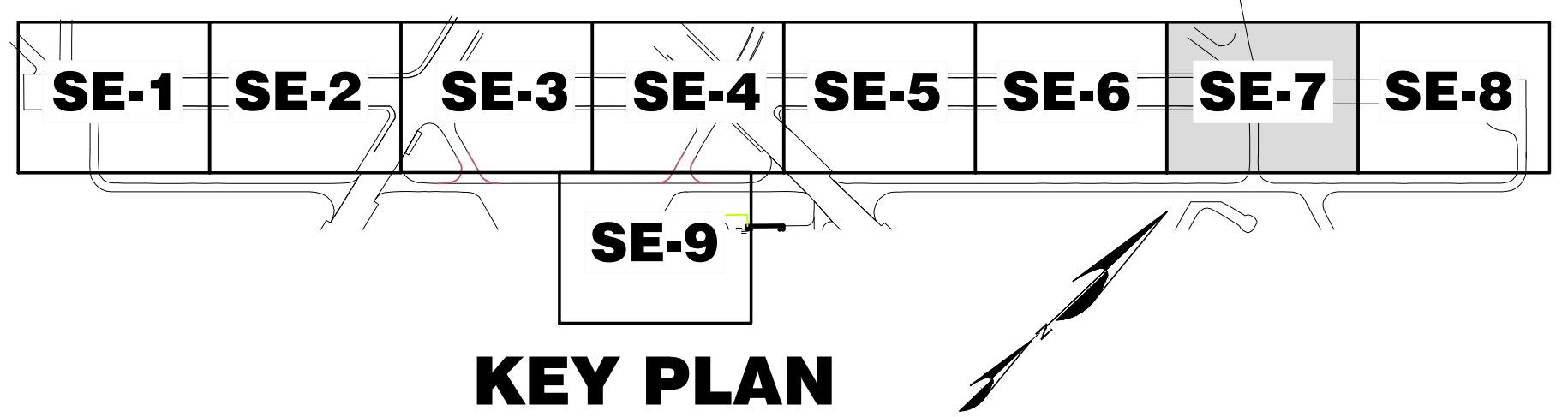
MATCHLINE STA. 50+00, SEE DWG. SE-6

MATCHLINE STA. 58+00, SEE DWG. SE-8



LEGEND

- NEW PAVEMENT
- NEW ASPHALT SHOULDER WEDGE
- EXISTING CONTOUR
- NEW CONTOUR
- EXISTING DROP INLET AND STORM DRAIN LINE
- LIMITS OF GRADING
- CONTROL POINT
- TEMPORARY SILT FENCE



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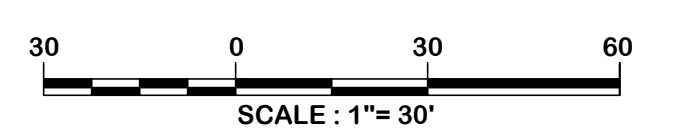
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Designer: BCM / NWM
Checked by: ADB
Technician: NWM
Project Number: 192930



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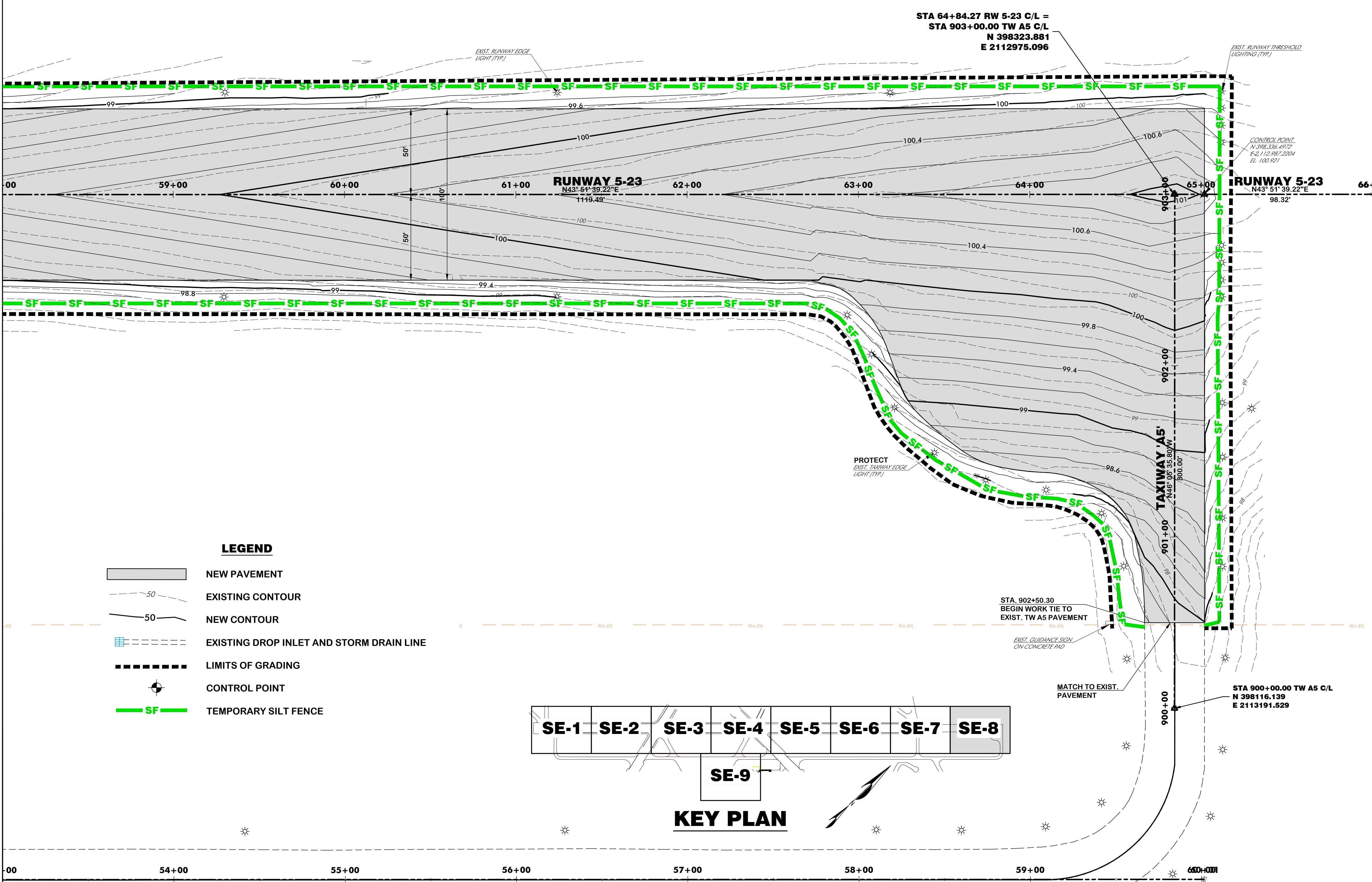
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

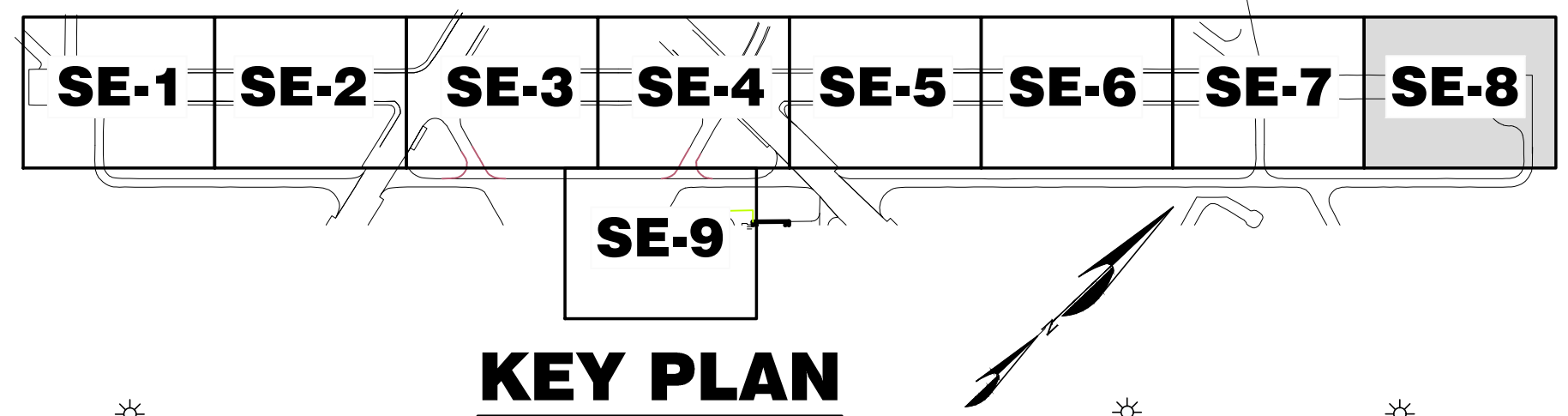
Drawing Name:
SEDIMENT AND EROSION CONTROL PLAN NO. 8

FAA A.I.P. Project Number:
3-45-0057-025-2025
Date: MARCH, 2025
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Division: AIRPORTS
Sheet Number: -
Drawing Number: SE-8

MATCHLINE STA. 58+00, SEE DWG. SE-7



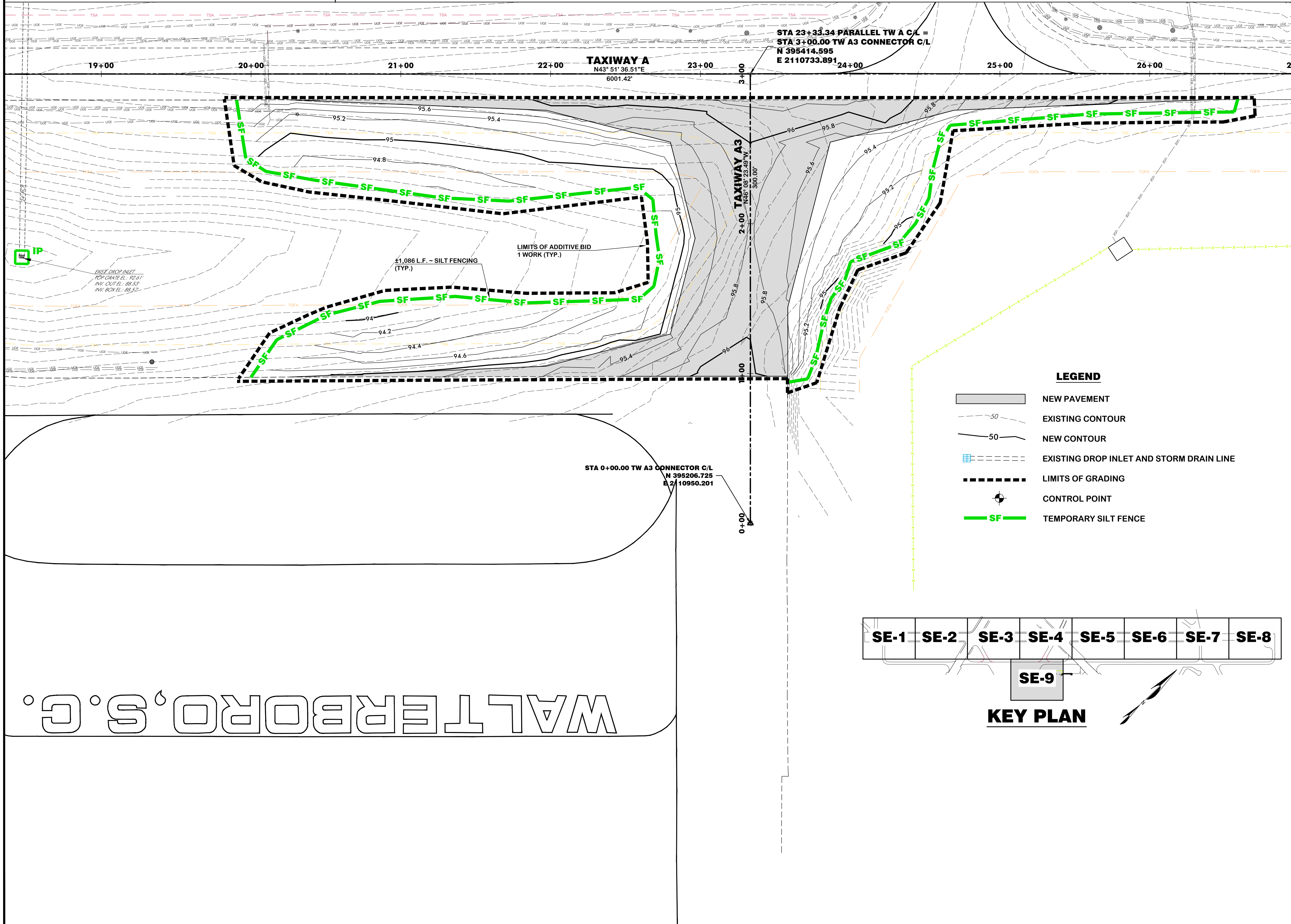
- LEGEND**
- NEW PAVEMENT
 - EXISTING CONTOUR
 - NEW CONTOUR
 - EXISTING DROP INLET AND STORM DRAIN LINE
 - LIMITS OF GRADING
 - CONTROL POINT
 - TEMPORARY SILT FENCE



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MATCHLINE, SEE DWG. SE-3

MATCHLINE, SEE DWG. SE-4



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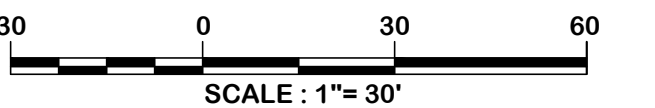


Designer:
BCM / NWM

Checked by:
ADB

Technician:
NWM

Project Number:
192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
SEDIMENT AND EROSION CONTROL PLAN NO. 9

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number:
SE-9

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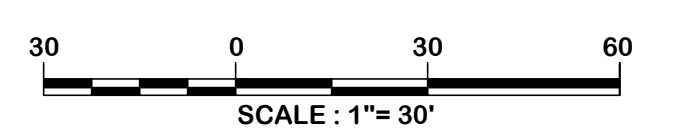
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Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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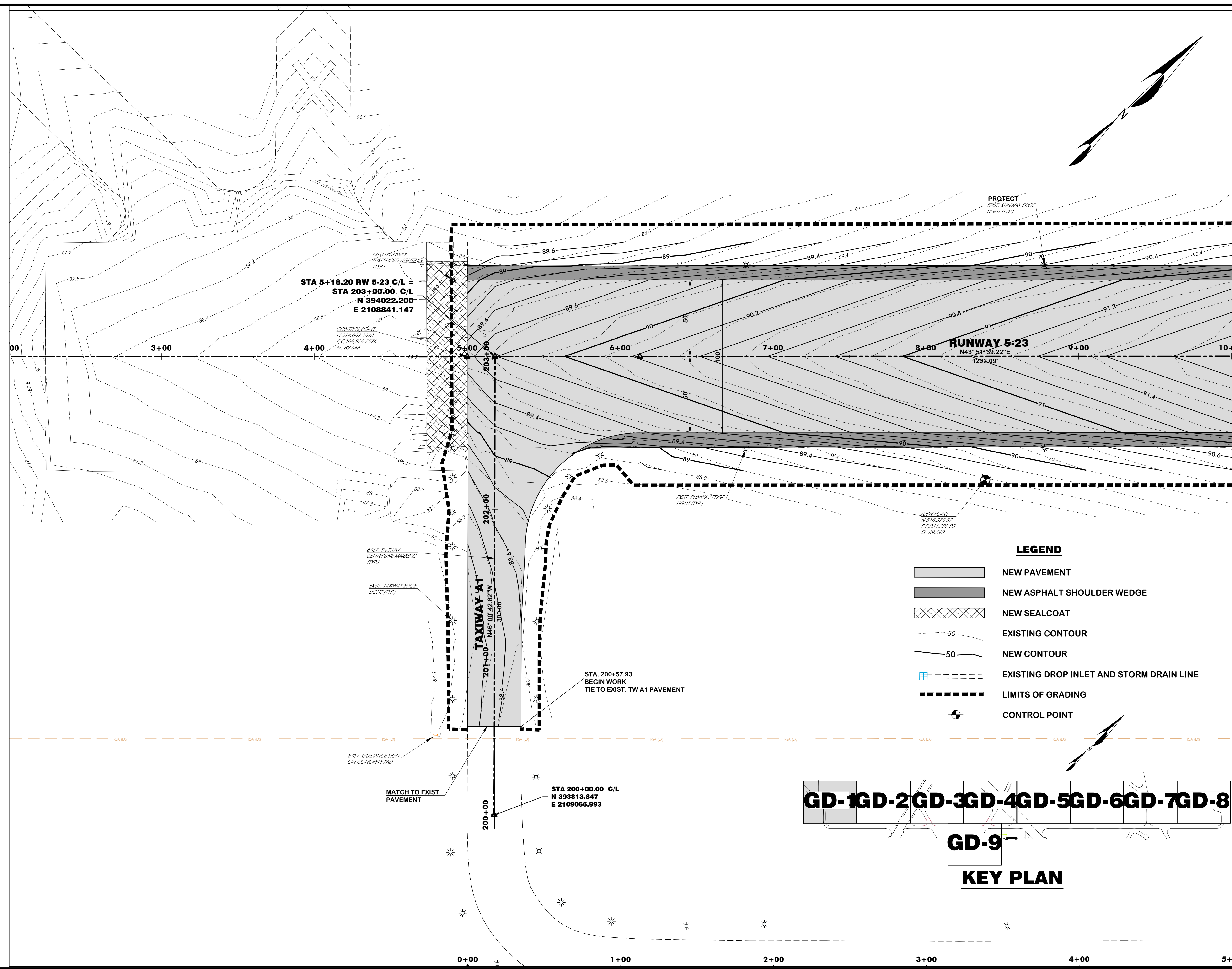
REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**GRADING AND DRAINAGE
PLAN NO. 1**

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: GD-1
Drawing Number: GD-1	

MATCHLINE STA. 10+00, SEE DWG. GD-2



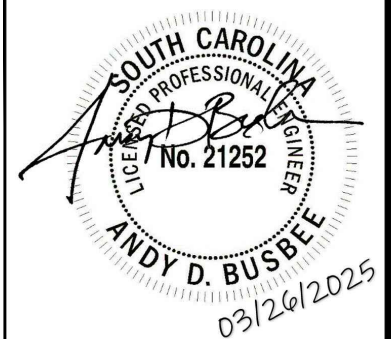
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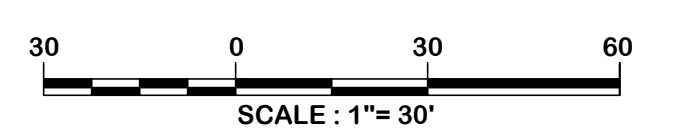
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Designer: BCM / NWM
Checked by: ADB
Technician: NWM
Project Number: 192930



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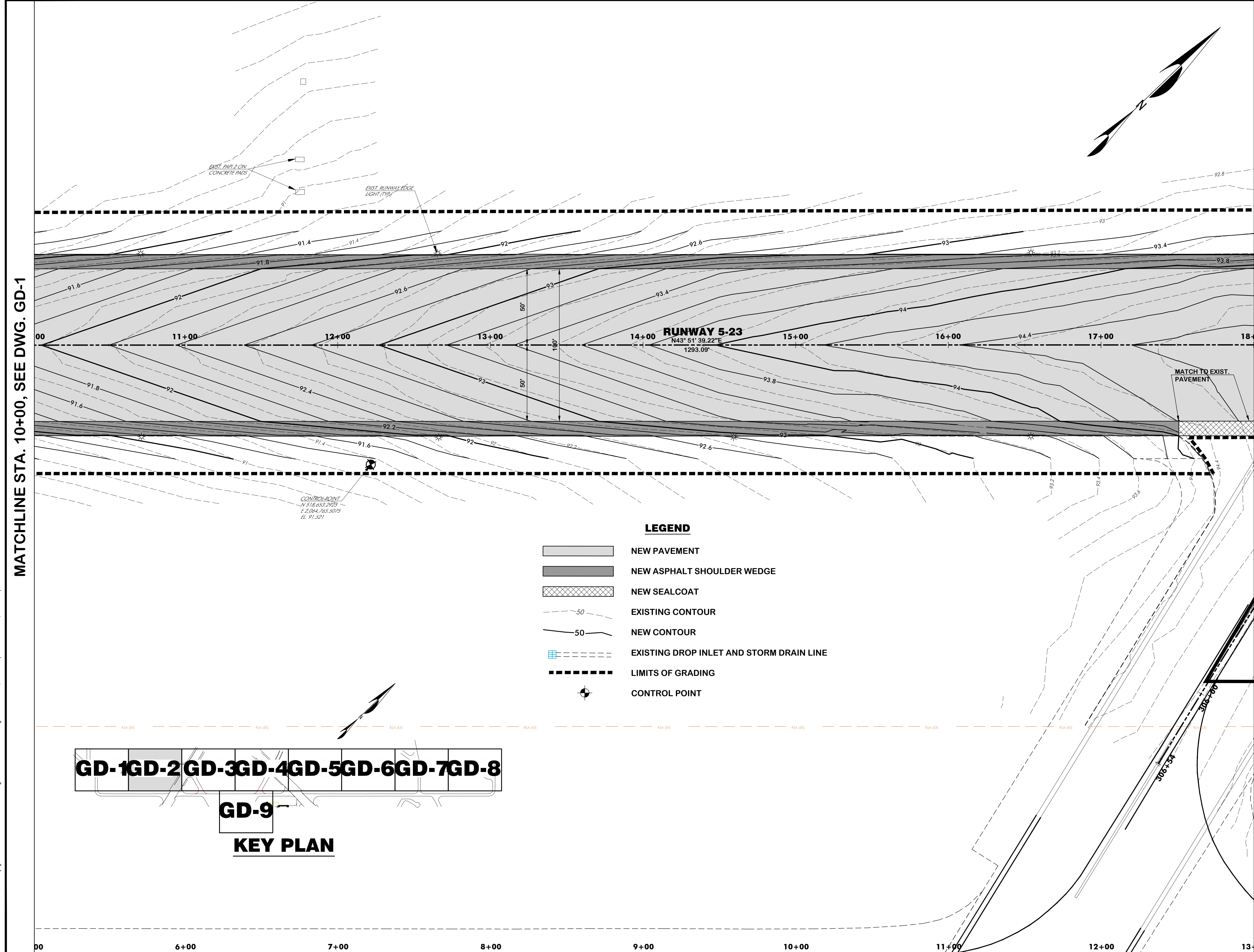
REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**GRADING AND DRAINAGE
PLAN NO. 2**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: AIRPORTS
Scale: 1" = 30'	Sheet Number: -
Drawing Number: GD-2	



MATCHLINE STA. 10+00, SEE DWG. GD-1

MATCHLINE STA. 18+00, SEE DWG. GD-3

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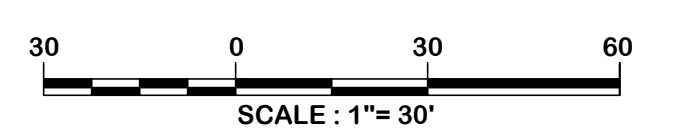
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Designer: BCM / NWM
Checked by: ADB
Technician: NWM
Project Number: 192930



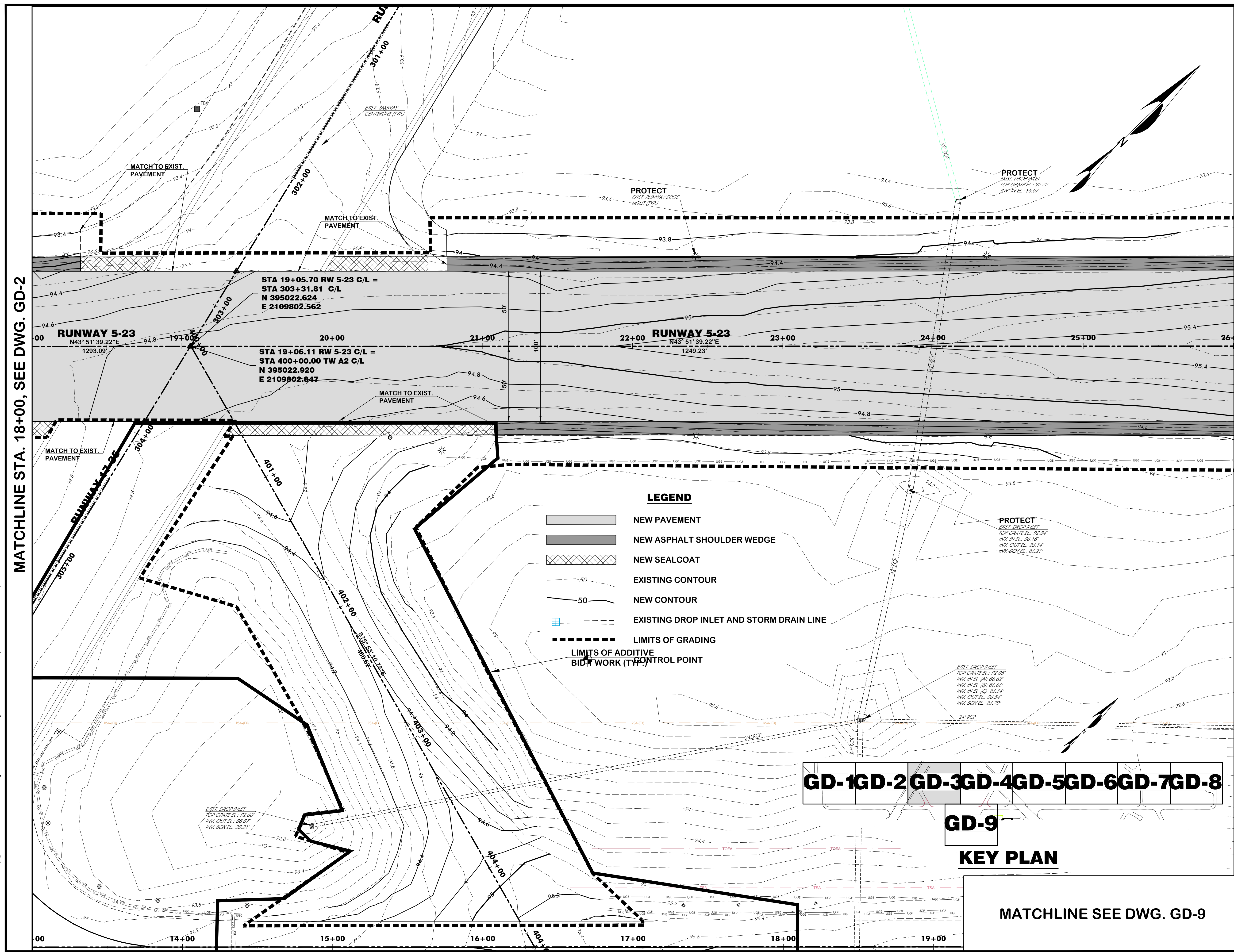
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**GRADING AND DRAINAGE
PLAN NO. 3**

FAA A.I.P. Project Number:
3-45-0057-025-2025
Date: **MARCH, 2025**
Division: **AIRPORTS**
Scale: **1"= 30'**
Sheet Number: **-**
Drawing Number: **GD-3**



GD-1GD-2GD-3GD-4GD-5GD-6GD-7GD-8

GD-9

KEY PLAN

MATCHLINE SEE DWG. GD-9

MATCHLINE STA. 18+00, SEE DWG. GD-2

MATCHLINE STA. 26+00, SEE DWG. GD-4

Z:\Aviation\CLIENTS\Walterboro lowcountry\192930\192930-05-23-rehab\192930-05-23-GRADING.dwg Modified: Mar 26, 2025 - 4:15pm Plotter: Mar 26, 2025 - 4:53pm Lauren Watson

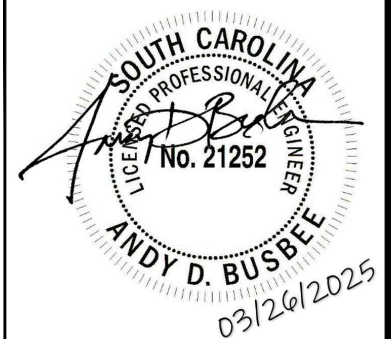


WALTERBORO, SOUTH CAROLINA

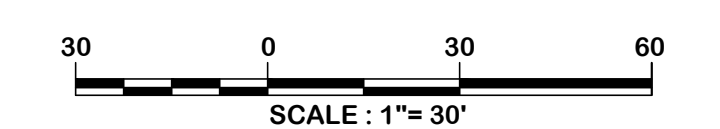
Michael Baker

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Columbia, South Carolina 29201



Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

**RUNWAY 5-23
REHABILITATION**

**GRADING AND DRAINAGE
PLAN NO. 4**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number:
GD-4

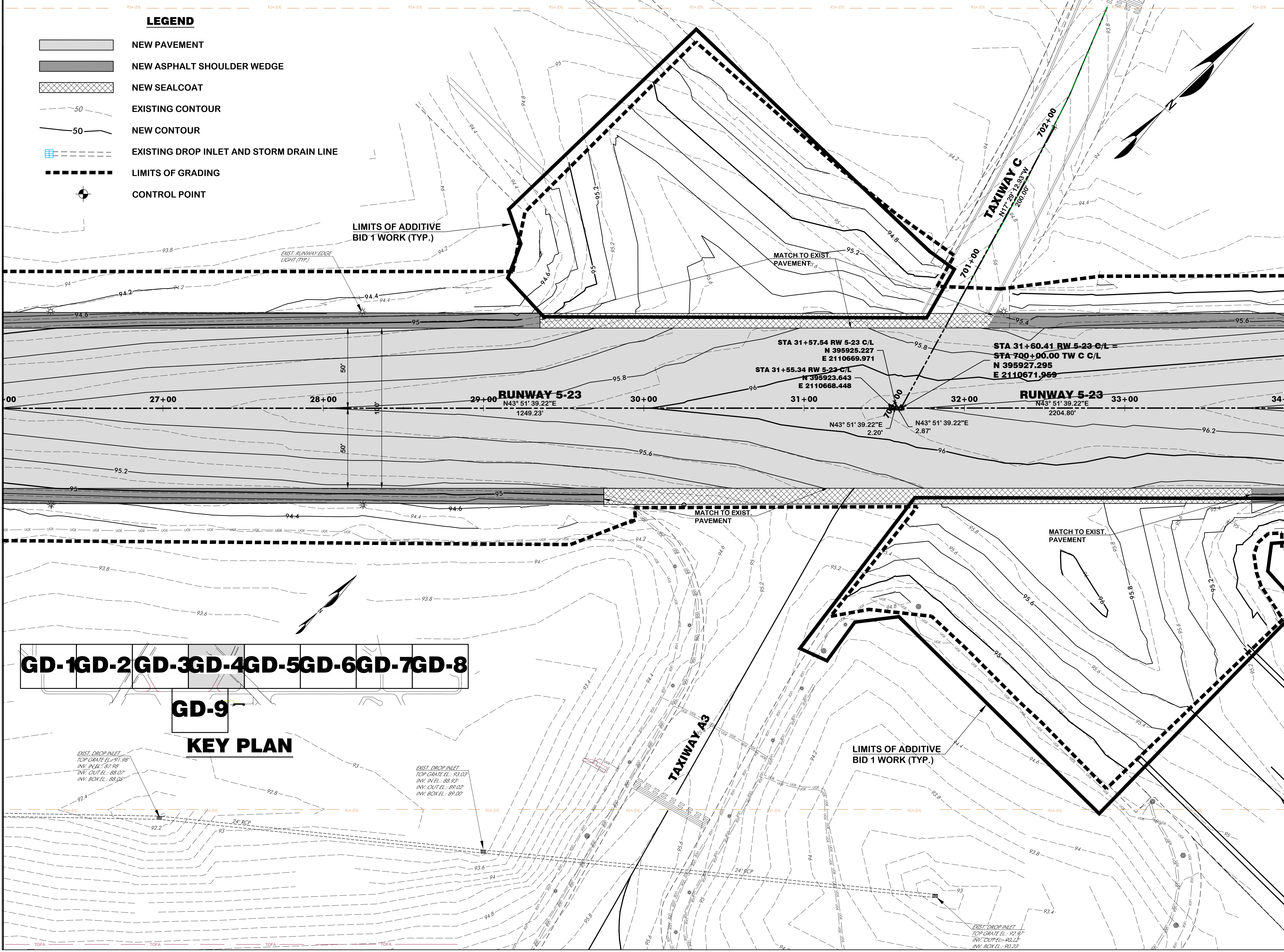
MATCHLINE STA. 28+00, SEE DWG. GD-3

MATCHLINE STA. 34+00, SEE DWG. GD-5

MATCHLINE SEE DWG. GD-9

LEGEND

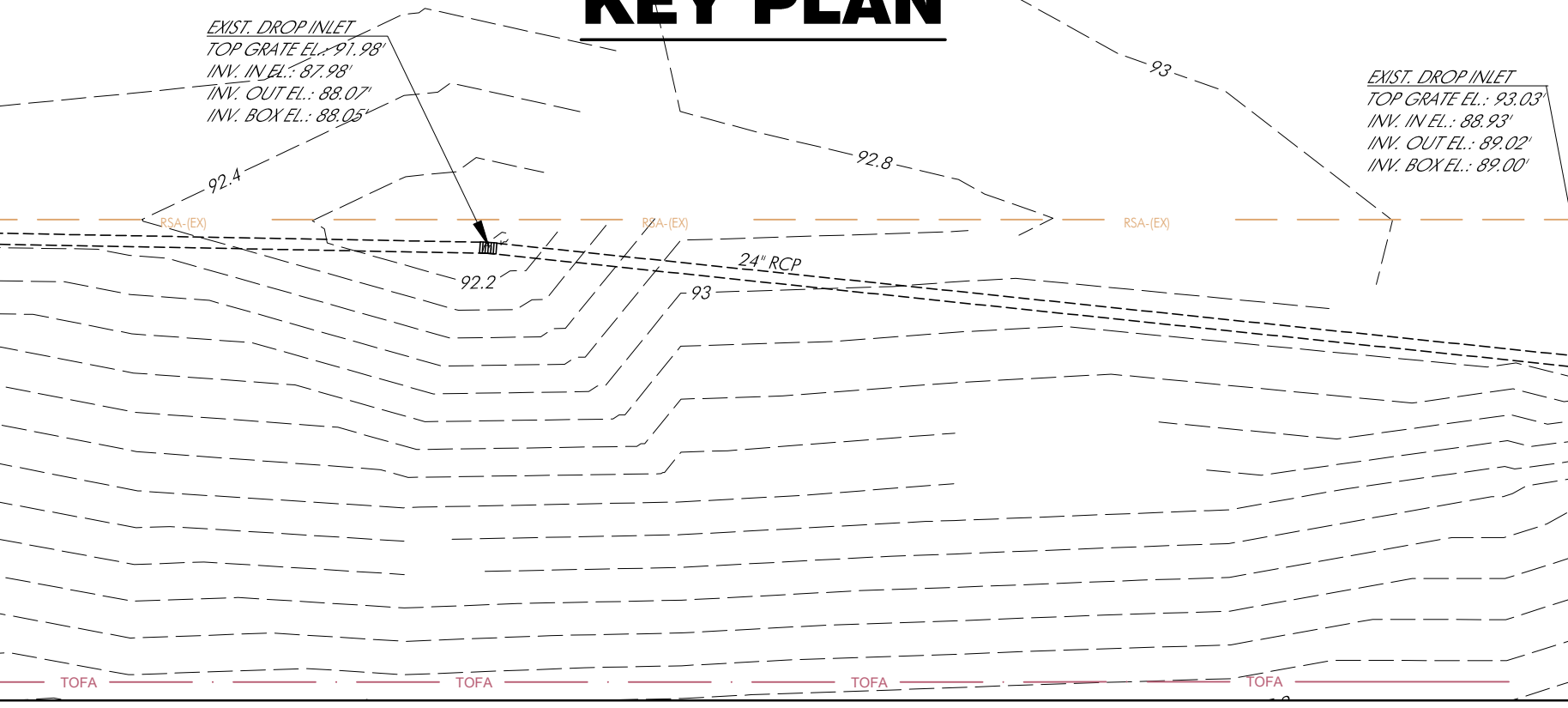
- NEW PAVEMENT
- NEW ASPHALT SHOULDER WEDGE
- NEW SEALCOAT
- EXISTING CONTOUR
- NEW CONTOUR
- EXISTING DROP INLET AND STORM DRAIN LINE
- LIMITS OF GRADING
- CONTROL POINT



GD-1 GD-2 GD-3 GD-4 GD-5 GD-6 GD-7 GD-8

GD-9

KEY PLAN



Z:\Aviation\CLIENTS\walterboro lowcountry\gn\2025\rv 5-23 rehab\retail\02-drawings\RB\RV\RV5-23-GRD4E.dwg Modified: Mar 26, 2025 - 4:15pm Plotter: Mar 26, 2025 - 4:50pm Lauren Wilson



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Columbia, South Carolina 29201



Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



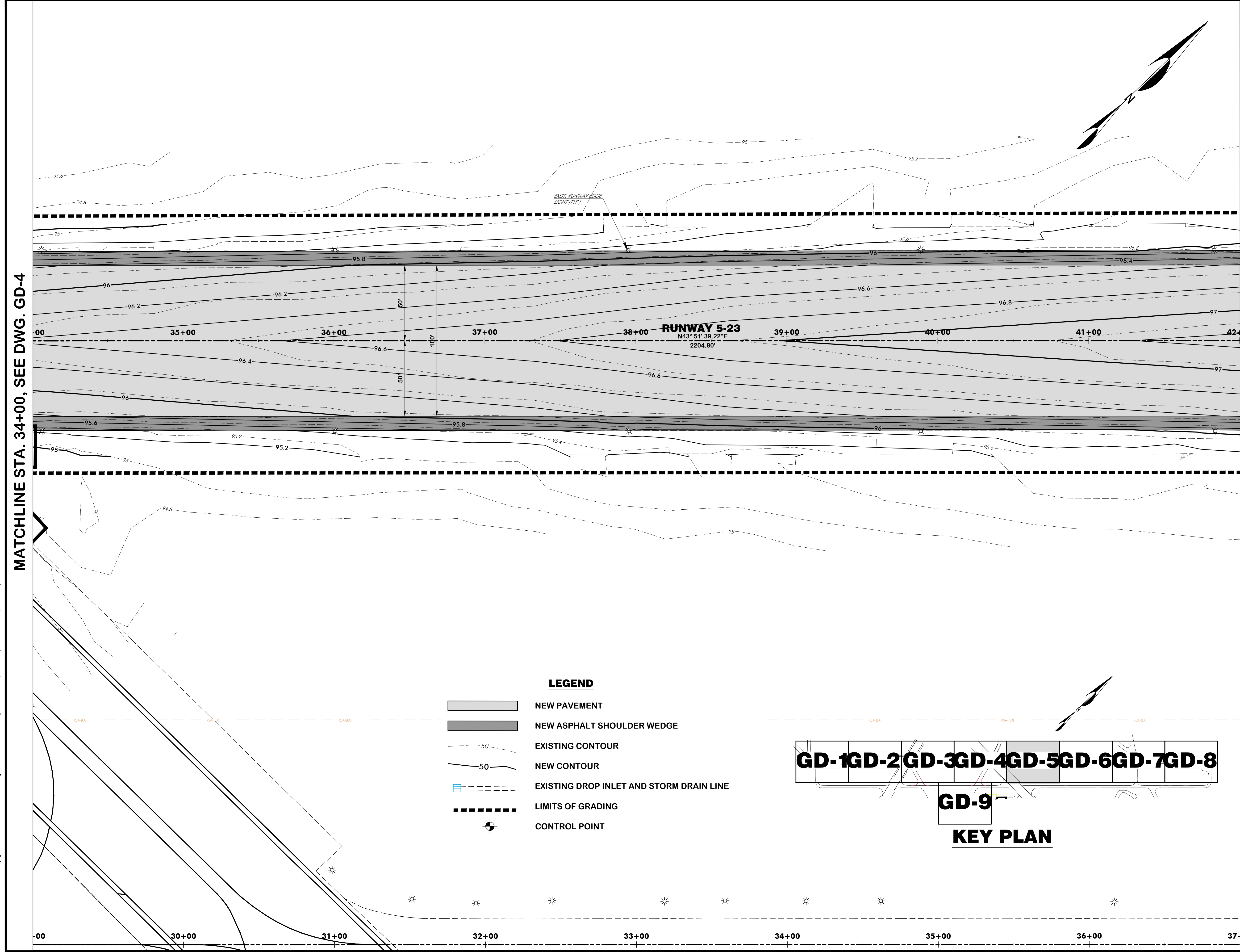
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

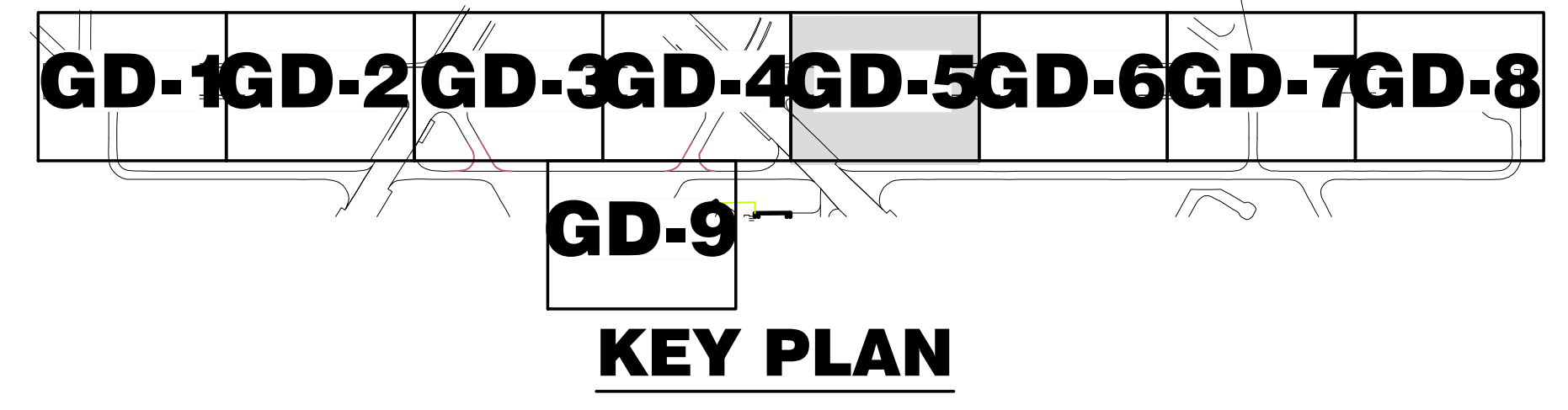
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**GRADING AND DRAINAGE
PLAN NO. 5**

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1" = 30'	Sheet Number: -
Drawing Number: GD-5	



LEGEND

- NEW PAVEMENT
- NEW ASPHALT SHOULDER WEDGE
- EXISTING CONTOUR
- NEW CONTOUR
- EXISTING DROP INLET AND STORM DRAIN LINE
- LIMITS OF GRADING
- CONTROL POINT



MATCHLINE STA. 34+00, SEE DWG. GD-4

MATCHLINE STA. 42+00, SEE DWG. GD-6

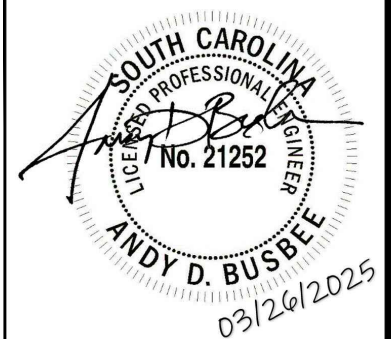
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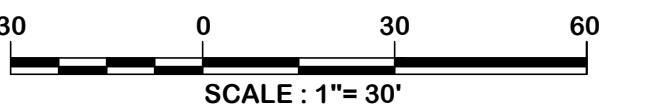
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Columbia, South Carolina 29201



Designer: BCM / NWM
Checked by: ADB
Technician: NWM
Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

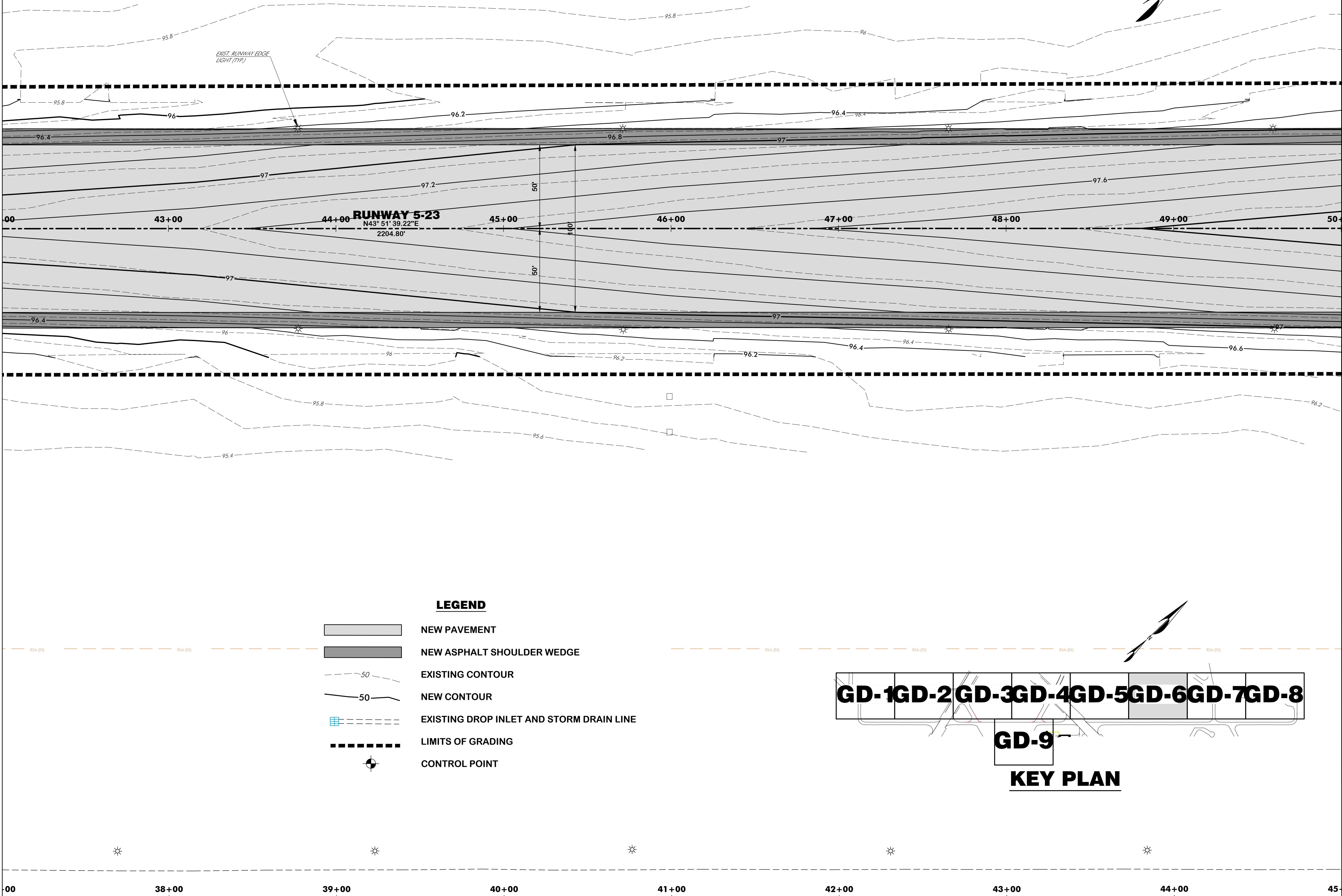
Drawing Name:
**GRADING AND DRAINAGE
PLAN NO. 6**

FAA A.I.P. Project Number:
3-45-0057-025-2025

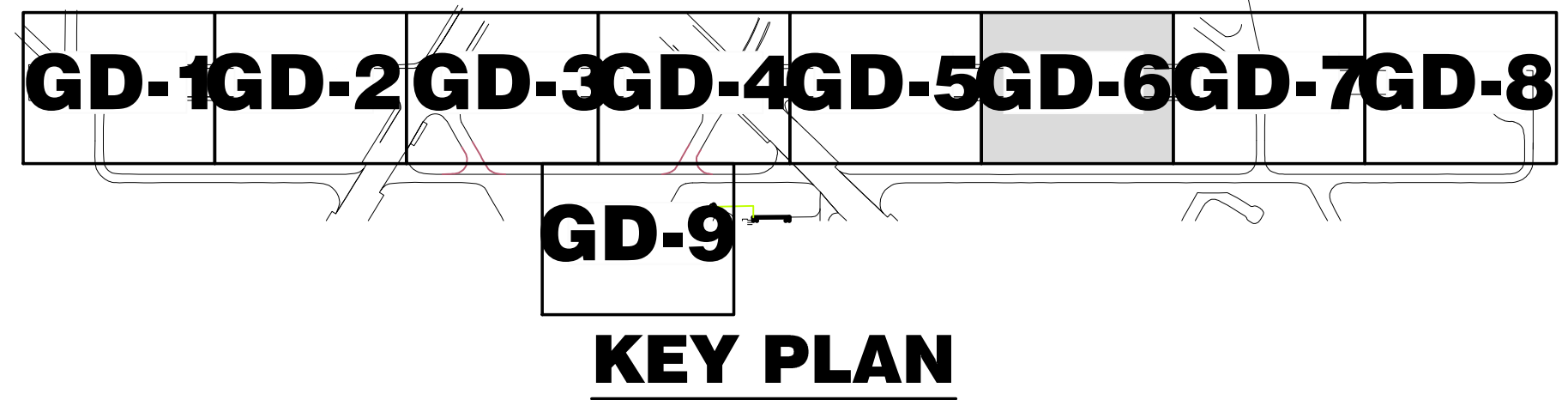
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
Drawing Number: GD-6	

MATCHLINE STA. 42+00, SEE DWG. GD-5

MATCHLINE STA. 50+00, SEE DWG. GD-7



- LEGEND**
- NEW PAVEMENT
 - NEW ASPHALT SHOULDER WEDGE
 - EXISTING CONTOUR
 - NEW CONTOUR
 - EXISTING DROP INLET AND STORM DRAIN LINE
 - LIMITS OF GRADING
 - CONTROL POINT



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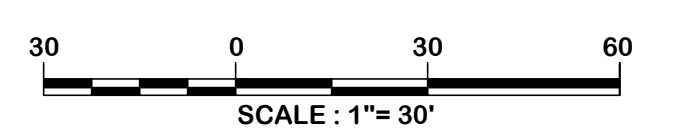
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700 Huger Street
Columbia, South Carolina 29201



Designer: BCM / NWM
Checked by: ADB
Technician: NWM
Project Number: 192930



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REVISIONS			
No.	Description	Date	By

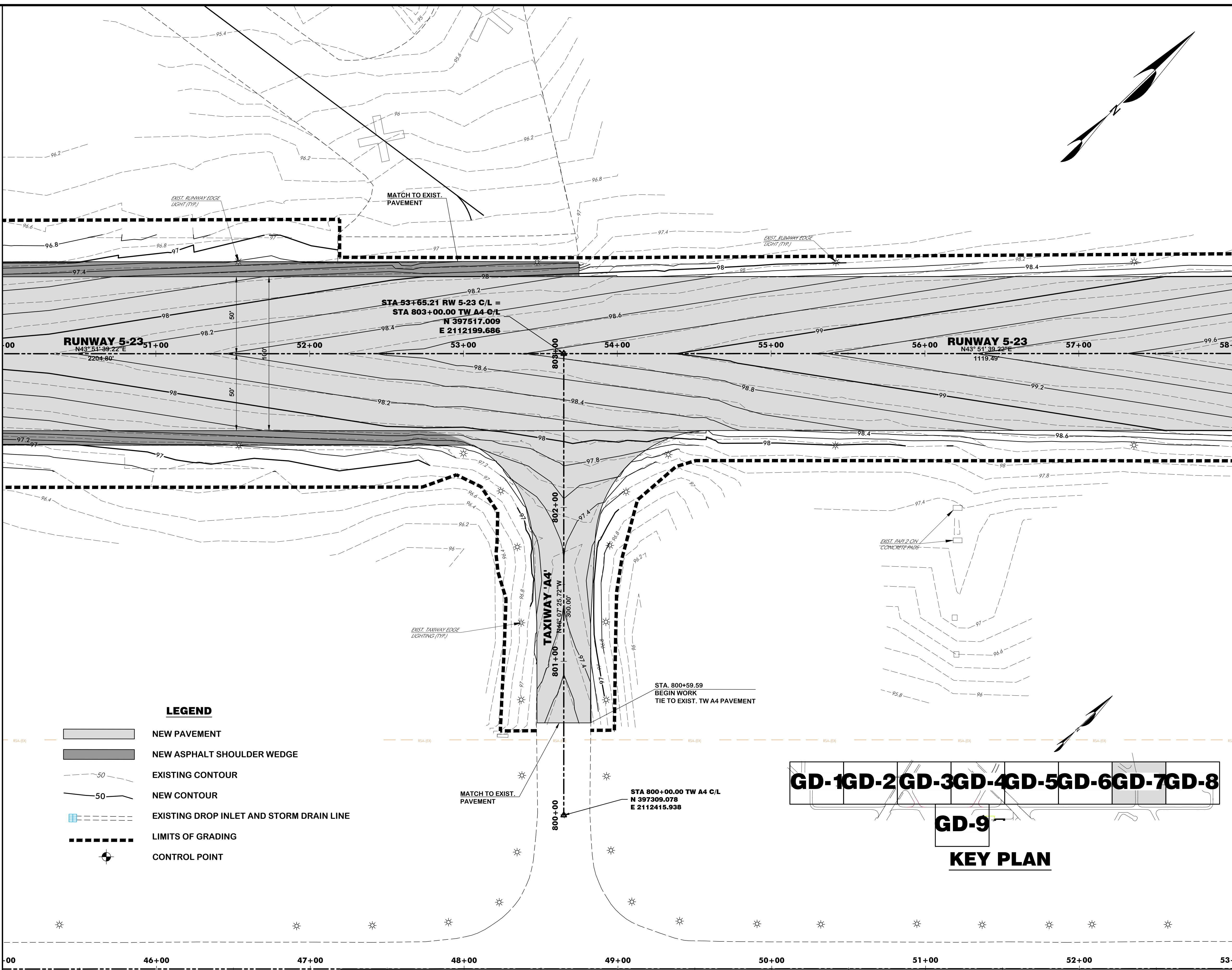
Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**GRADING AND DRAINAGE
PLAN NO. 7**

FAA A.I.P. Project Number:
3-45-0057-025-2025
Date: **MARCH, 2025**
Scale: **1"= 30'**
Division: **AIRPORTS**
Sheet Number: **-**
Drawing Number: **GD-7**

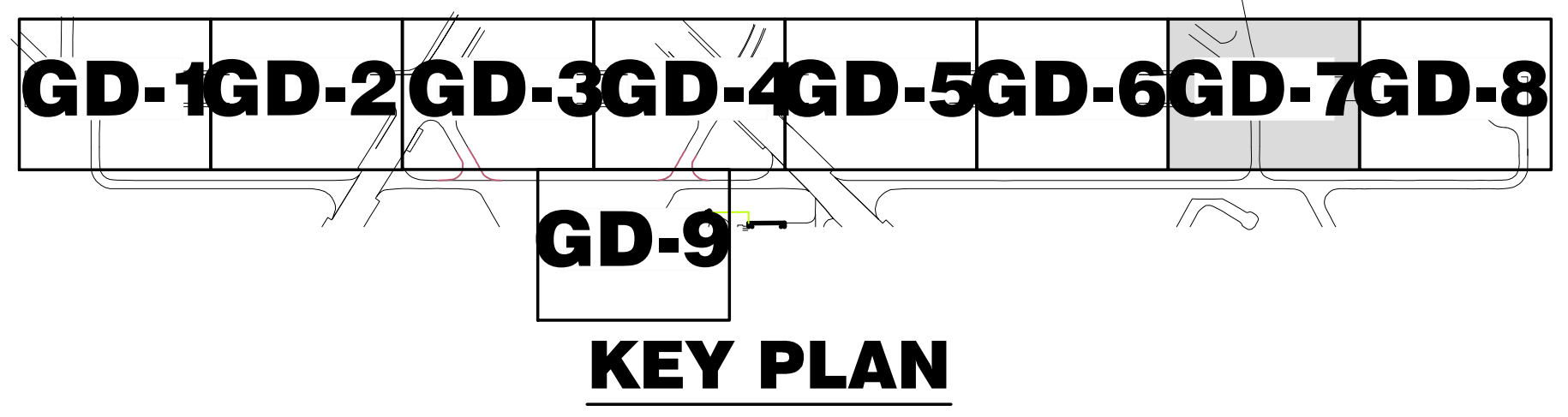
MATCHLINE STA. 50+00, SEE DWG. GD-6

MATCHLINE STA. 58+00, SEE DWG. GD-8



LEGEND

- NEW PAVEMENT
- NEW ASPHALT SHOULDER WEDGE
- EXISTING CONTOUR
- NEW CONTOUR
- EXISTING DROP INLET AND STORM DRAIN LINE
- LIMITS OF GRADING
- CONTROL POINT



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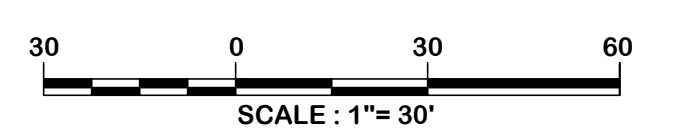
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INTERNATIONAL

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Columbia, South Carolina 29201



Designer: BCM / NWM
Checked by: ADB
Technician: NWM
Project Number: 192930



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REVISIONS			
No.	Description	Date	By

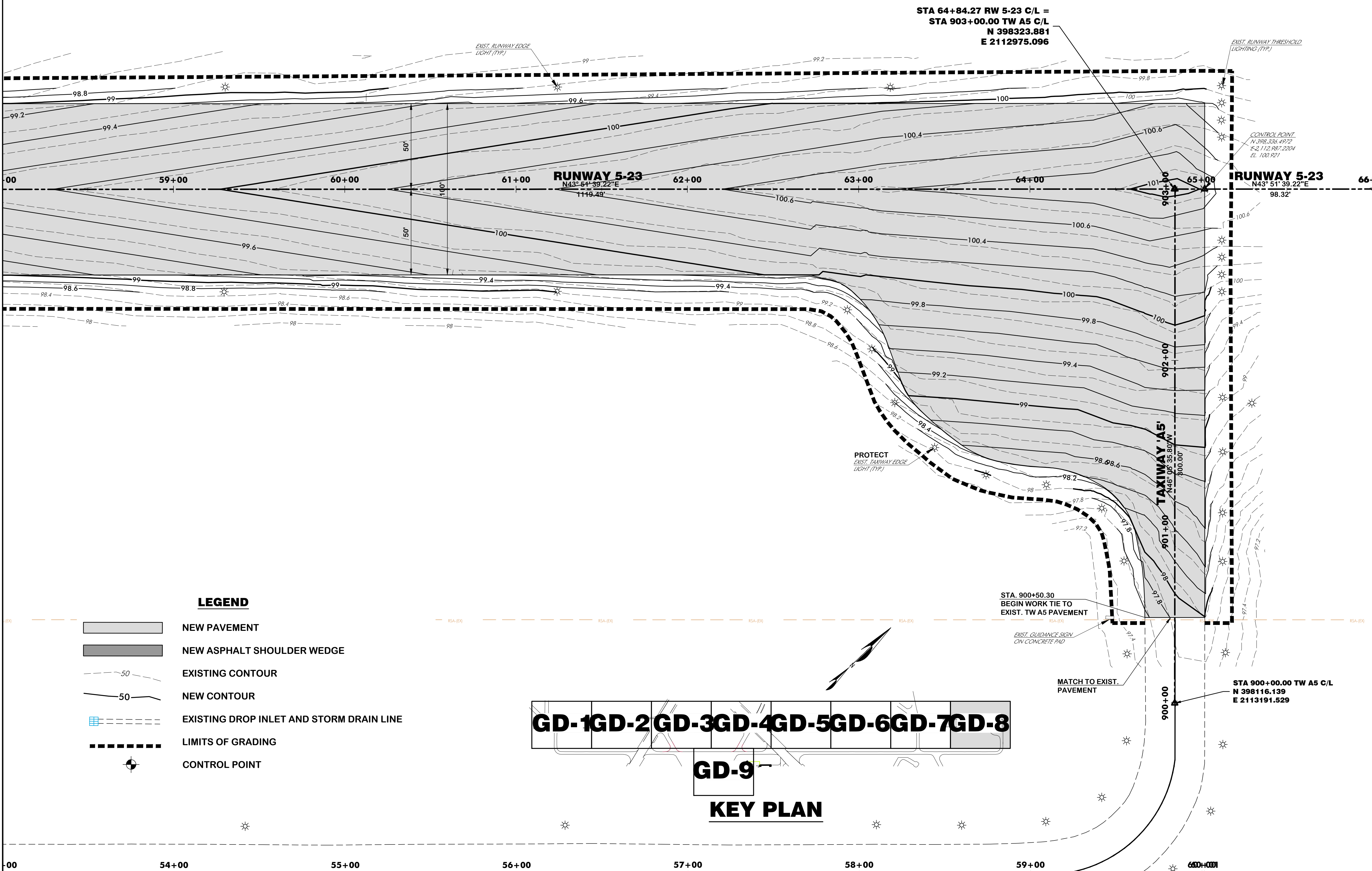
Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**GRADING AND DRAINAGE
PLAN NO. 8**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
Drawing Number: GD-8	

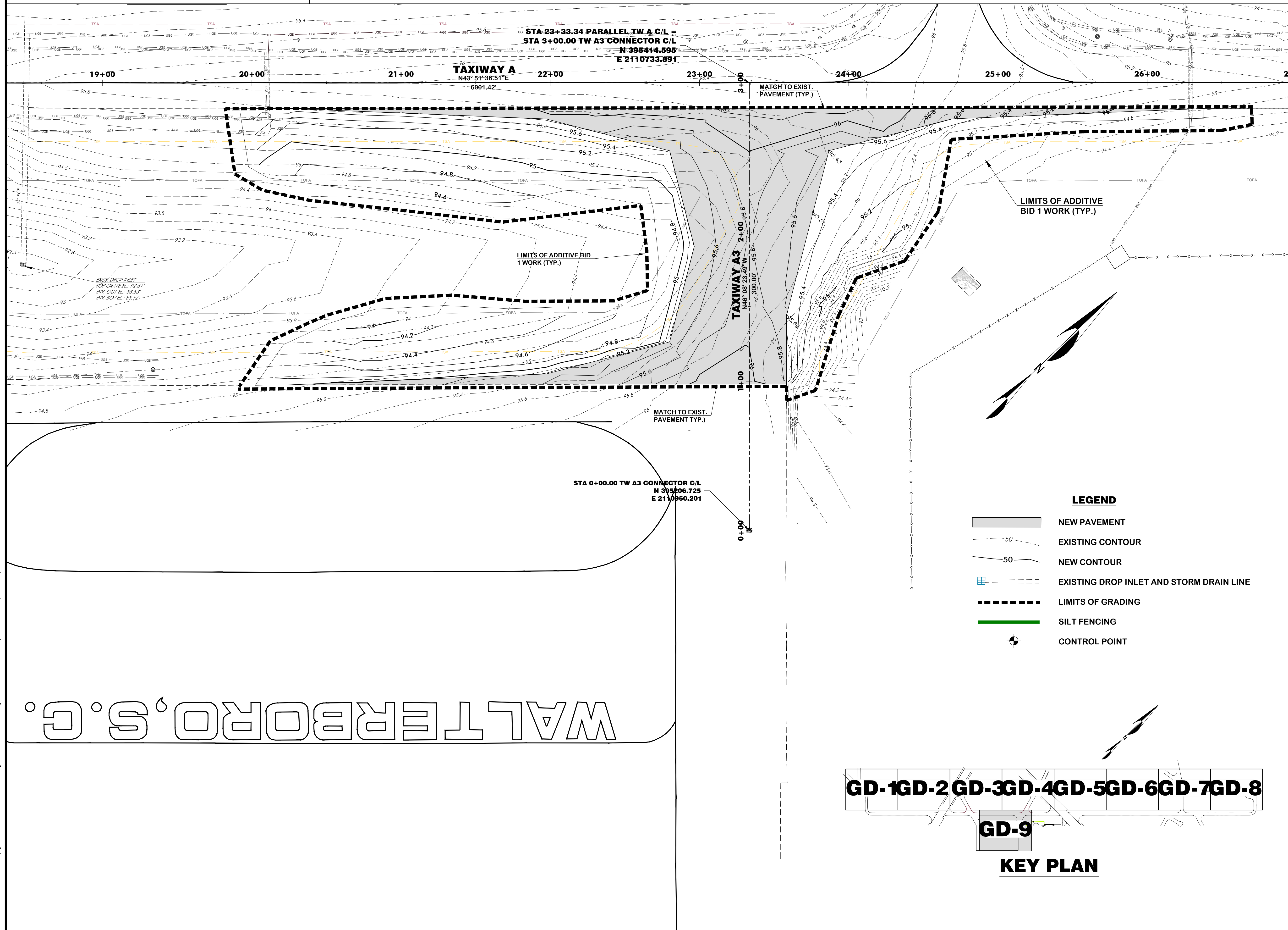
MATCHLINE STA. 58+00, SEE DWG. GD-7



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MATCHLINE , SEE DWG. GD-3

MATCHLINE , SEE DWG. GD-4



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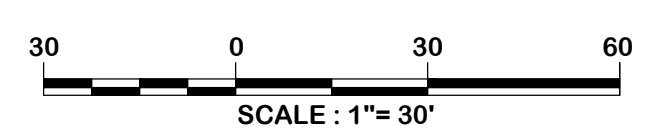


Designer:
BCM / NWM

Checked by:
ADB

Technician:
NWM

Project Number:
192930



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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**GRADING AND DRAINAGE
PLAN NO. 9**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number:
GD-9

- LEGEND**
- NEW PAVEMENT
 - EXISTING CONTOUR
 - NEW CONTOUR
 - EXISTING DROP INLET AND STORM DRAIN LINE
 - LIMITS OF GRADING
 - SILT FENCING
 - CONTROL POINT

GD-1GD-2GD-3GD-4GD-5GD-6GD-7GD-8

GD-9

KEY PLAN

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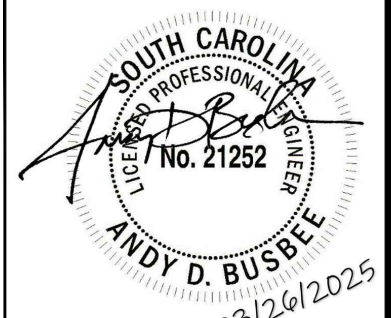


WALTERBORO, SOUTH CAROLINA

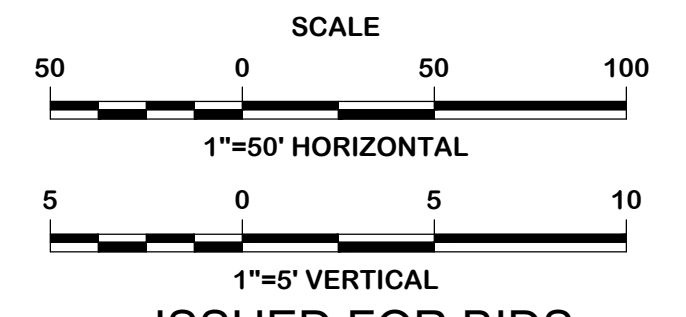
Michael Baker

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Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



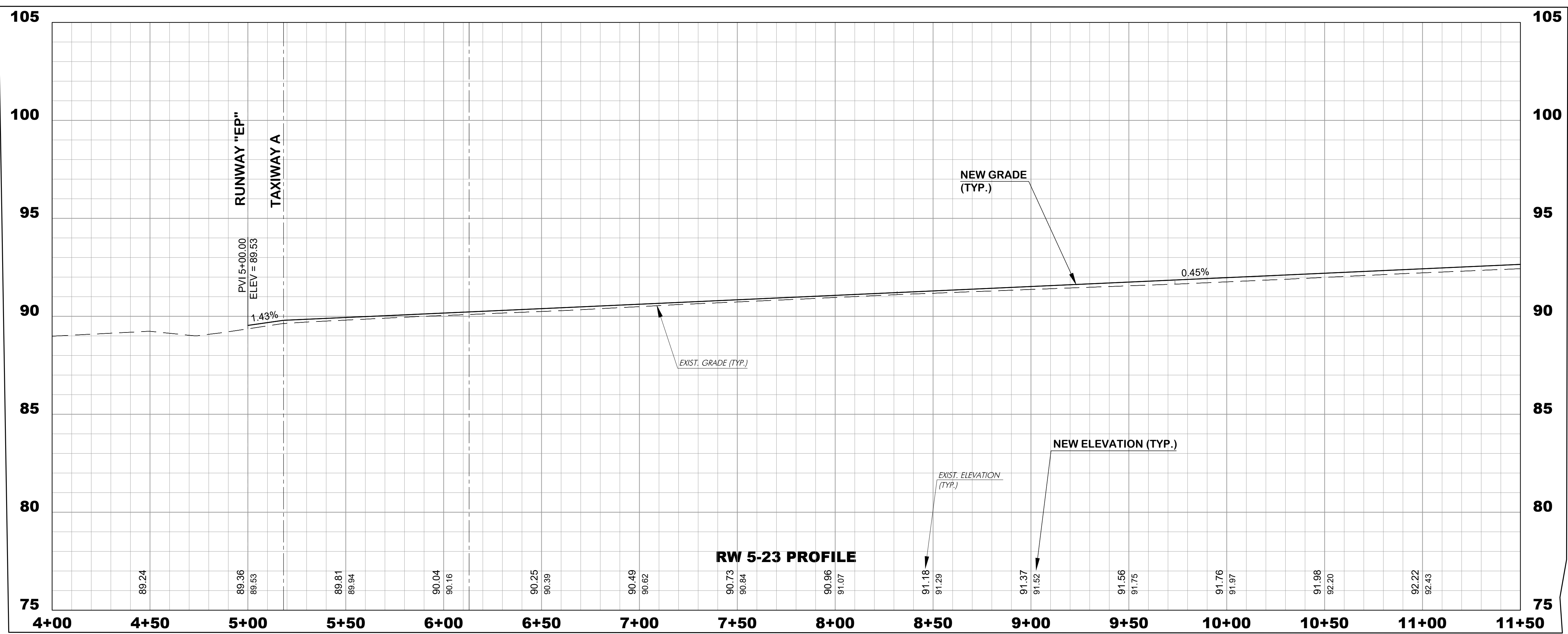
ISSUED FOR BIDS

No.	Description	Date	By

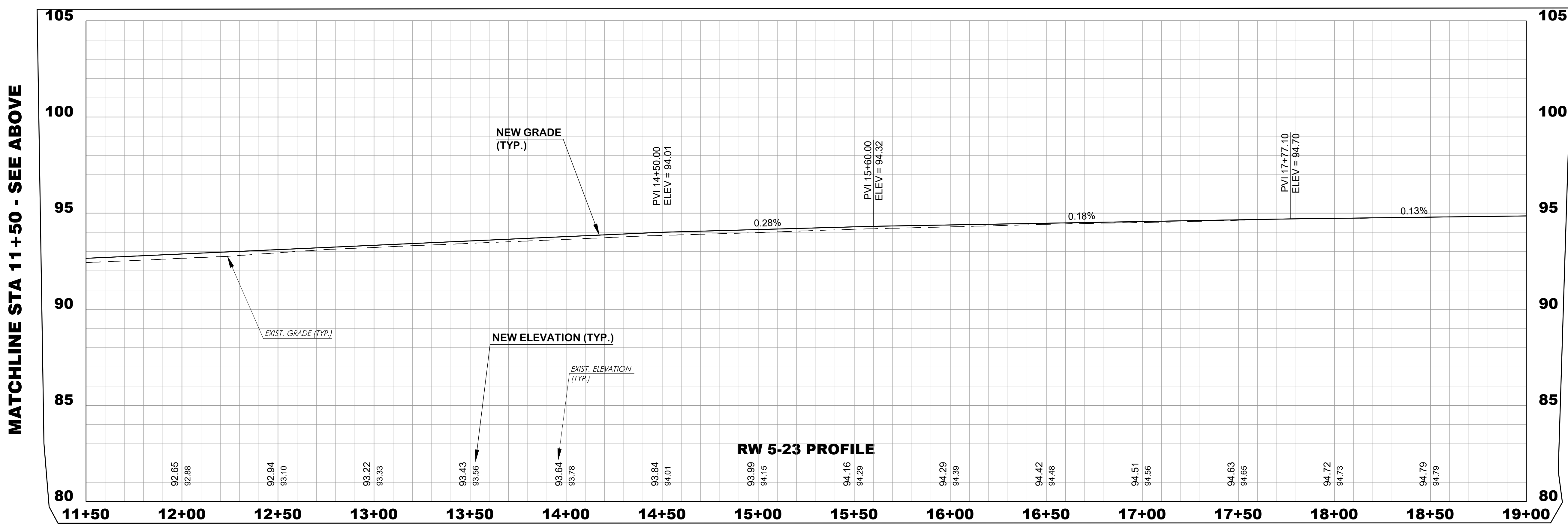
Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 CENTERLINE PROFILES STA. 4+00 TO 19+00

FAA A.I.P. Project Number: 3-45-0057-025-2025	Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ. 1"=5' VERT.	Sheet Number:	Drawing Number: P-1



MATCHLINE STA 11+50 - SEE BELOW



MATCHLINE STA 11+50 - SEE ABOVE

MATCHLINE STA. 19+00, SEE DWG P-2

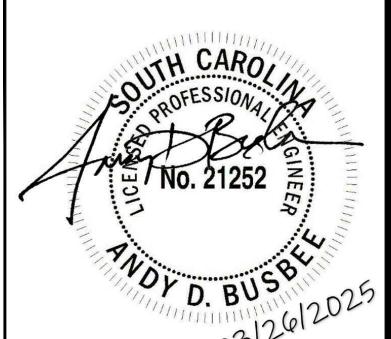


WALTERBORO, SOUTH CAROLINA

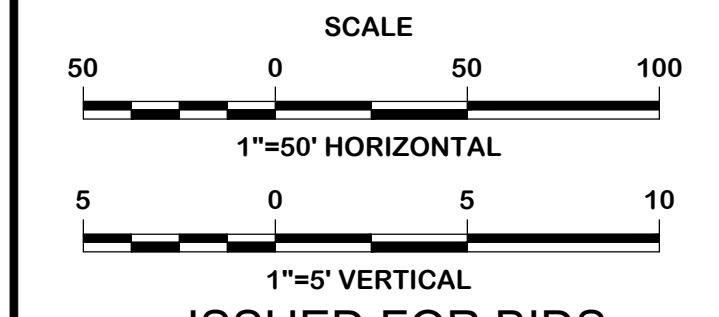
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Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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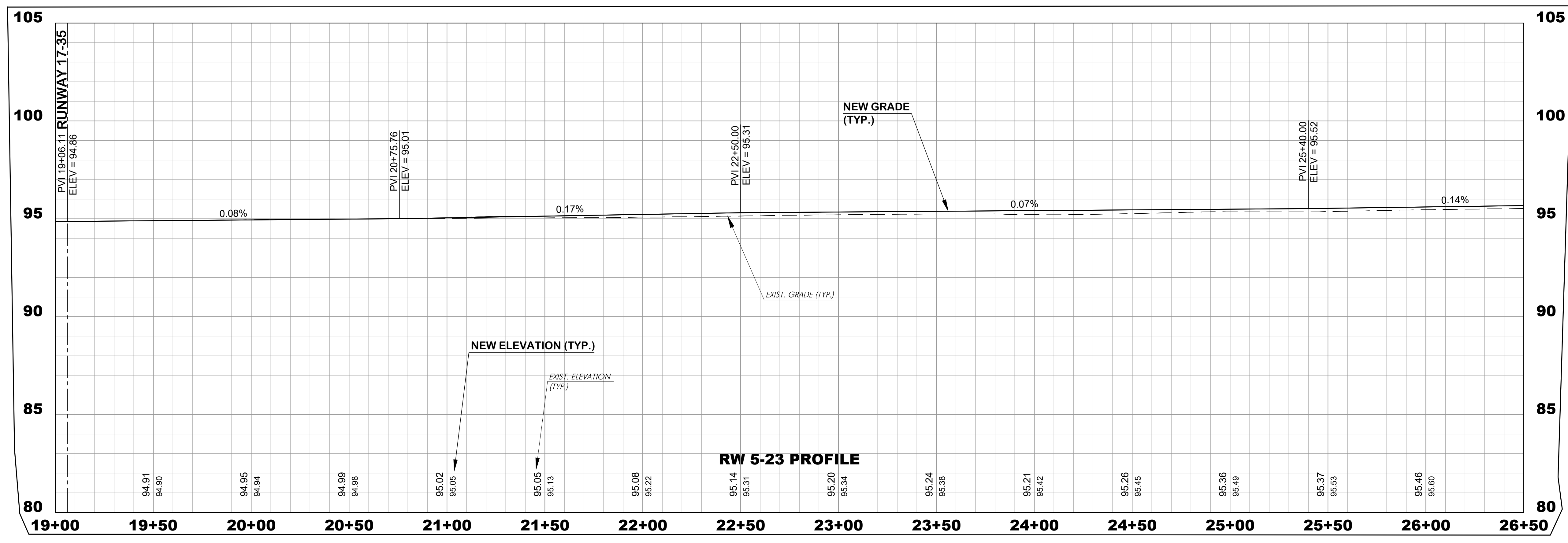
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 CENTERLINE PROFILES STA. 19+00 TO 34+00

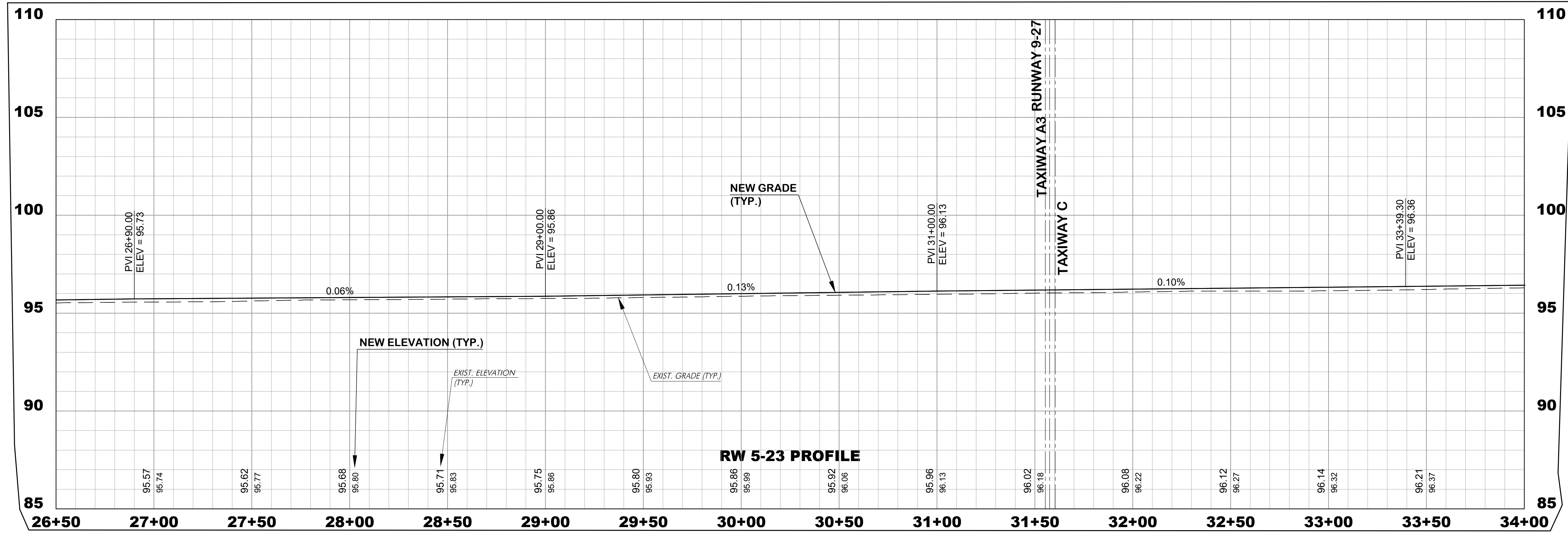
FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ. 1"=5' VERT.	Sheet Number: Drawing Number: P-2

MATCHLINE STA. 19+00, SEE DWG P-1



MATCHLINE STA. 26+50 - SEE BELOW

MATCHLINE STA. 26+50 - SEE ABOVE



MATCHLINE STA. 34+00, SEE DWG P-3

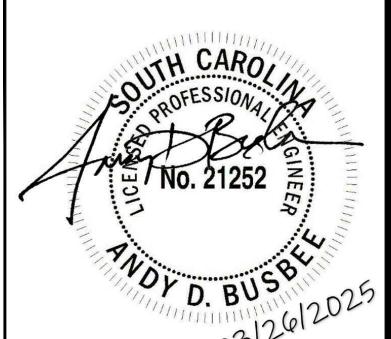


WALTERBORO, SOUTH CAROLINA

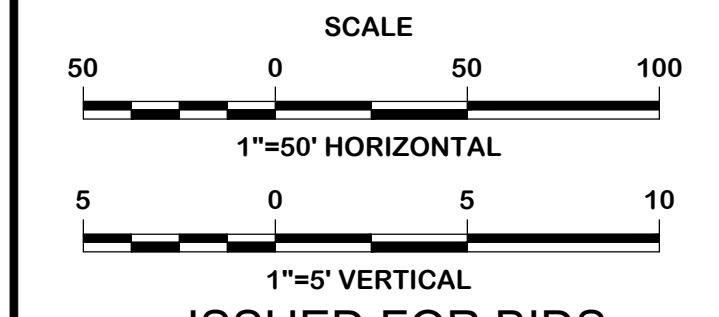
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Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 CENTERLINE PROFILES STA. 34+00 TO 49+00

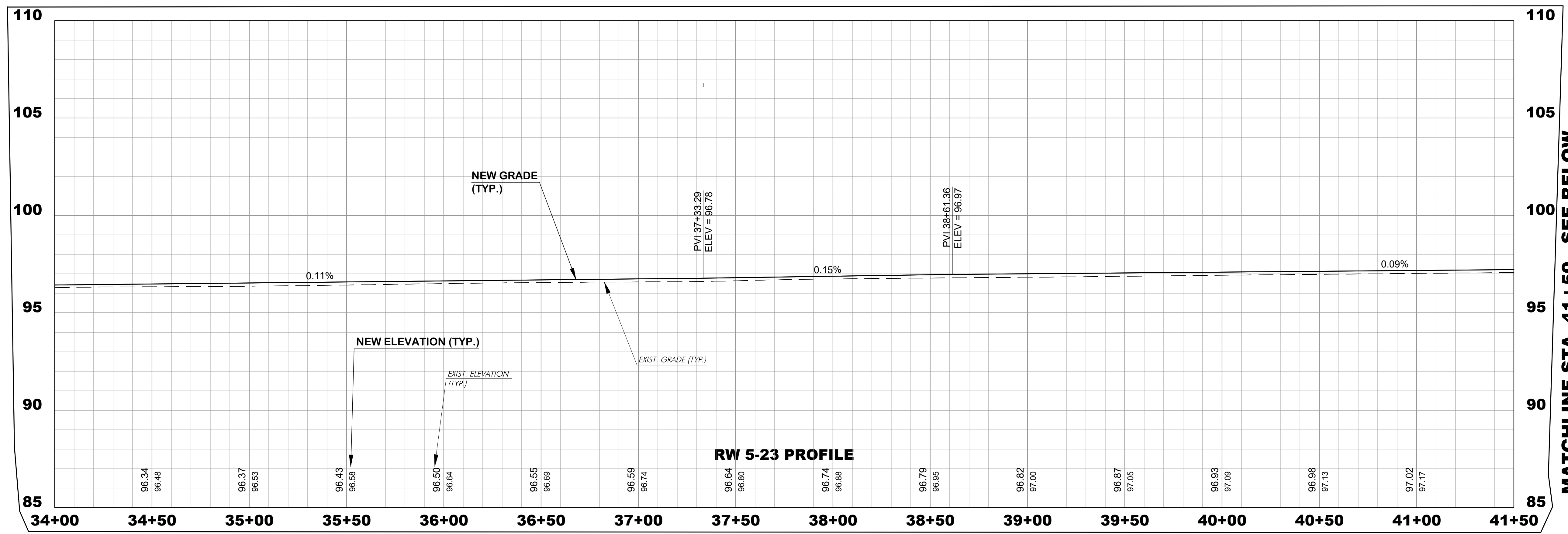
FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"=50 HORIZ.** Sheet Number:

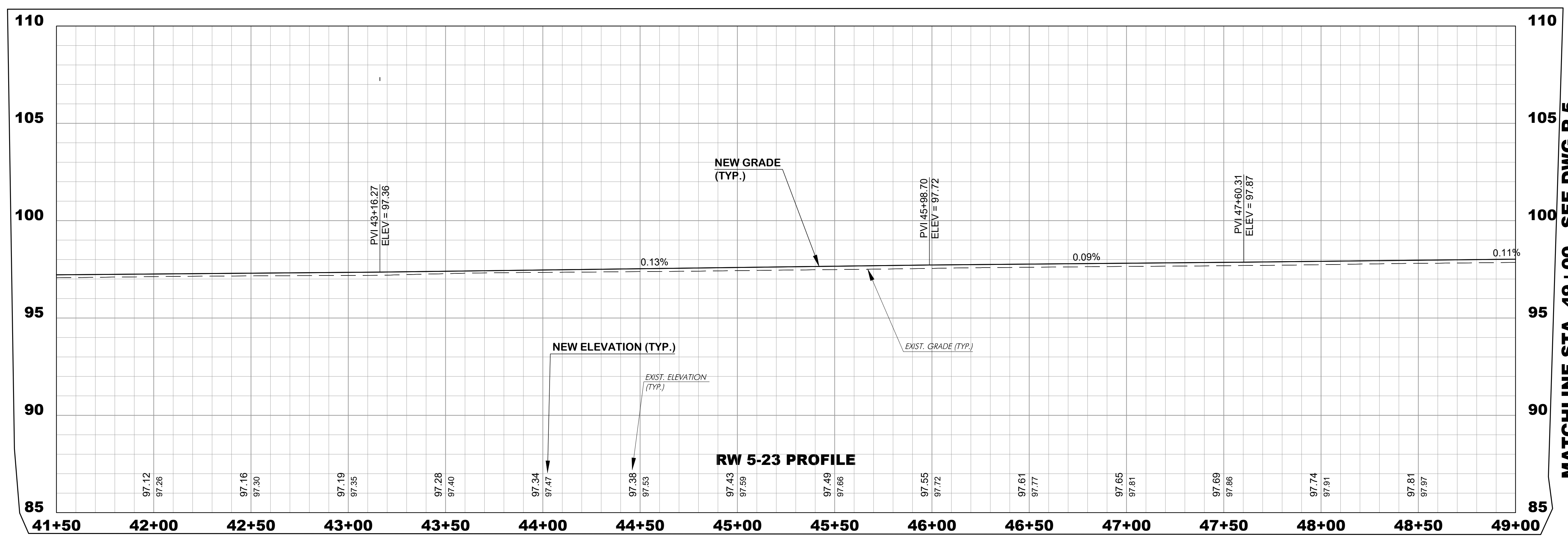
1"=5' VERT. Drawing Number:
P-3

MATCHLINE STA. 53+00, SEE DWG P-2



MATCHLINE STA. 41+50 - SEE BELOW

MATCHLINE STA. 41+50, SEE ABOVE



MATCHLINE STA. 49+00 - SEE DWG P-5

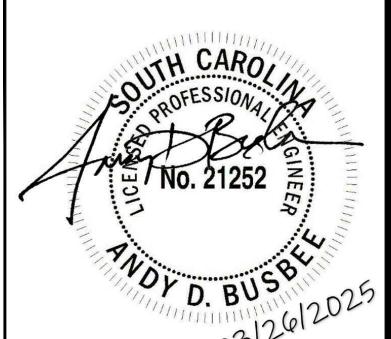


WALTERBORO, SOUTH CAROLINA

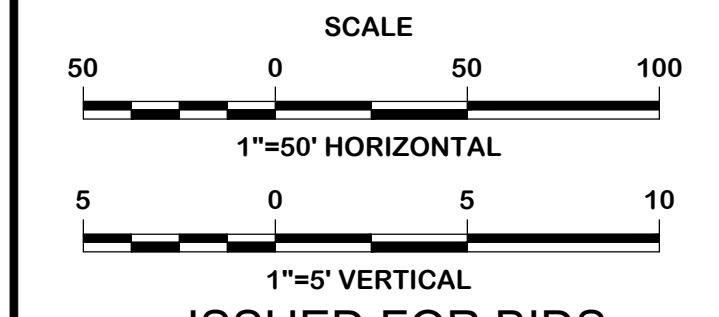
Michael Baker

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700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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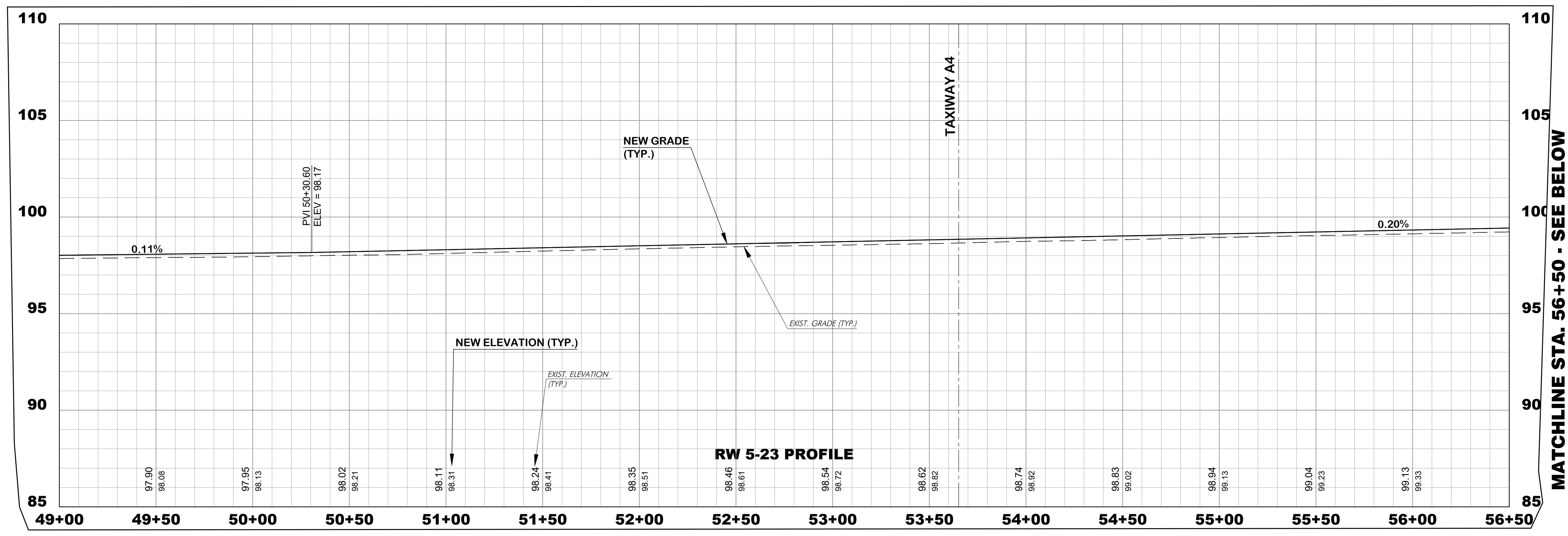
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 CENTERLINE PROFILES STA. 49+00 TO 64+00

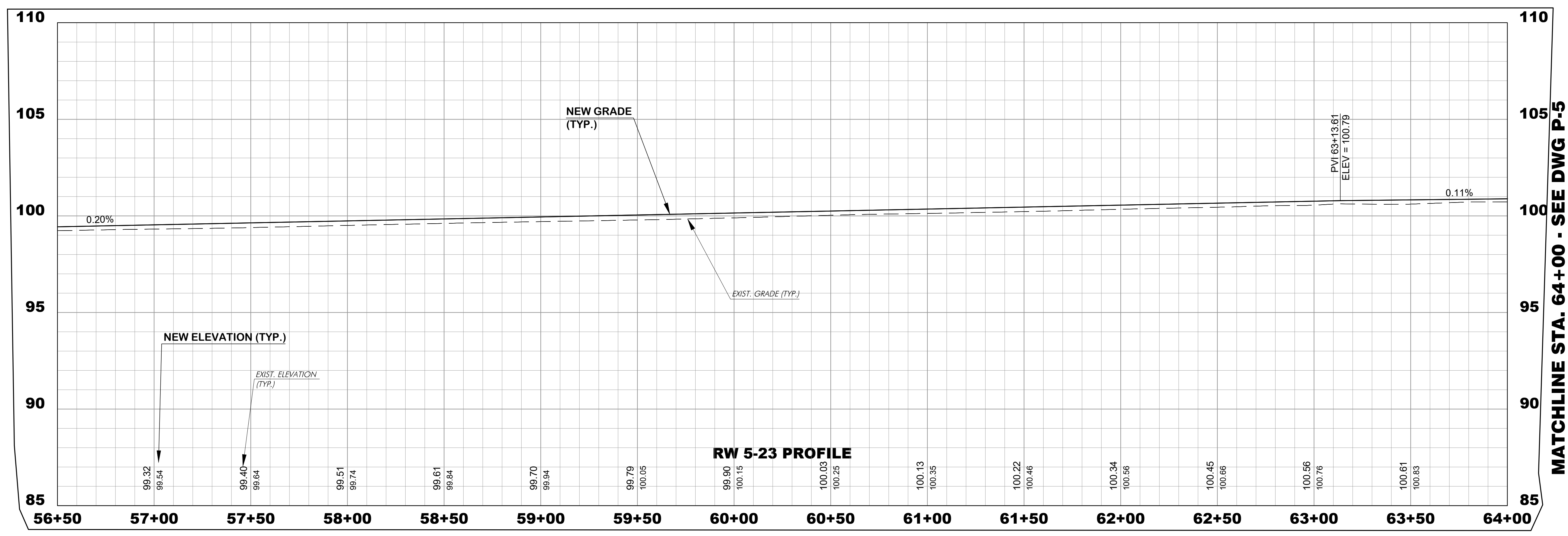
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Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ.	Sheet Number:
1"=5' VERT.	Drawing Number: P-4

MATCHLINE STA. 49+00, SEE DWG P-3



MATCHLINE STA. 56+50 - SEE BELOW

MATCHLINE STA. 56+50, SEE ABOVE



MATCHLINE STA. 64+00 - SEE DWG P-5

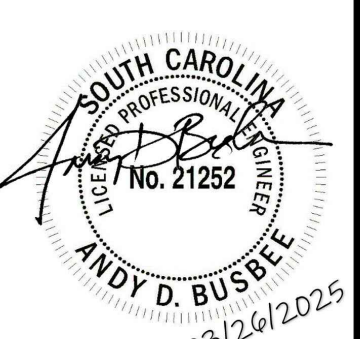


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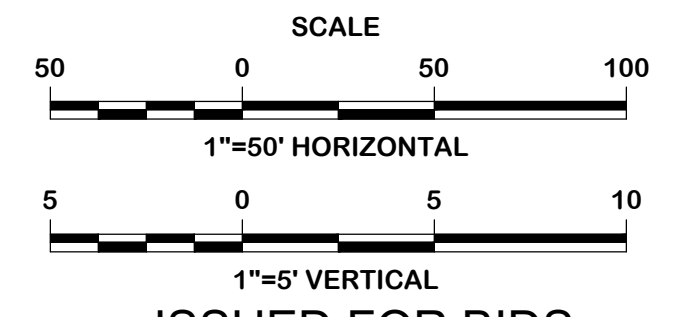
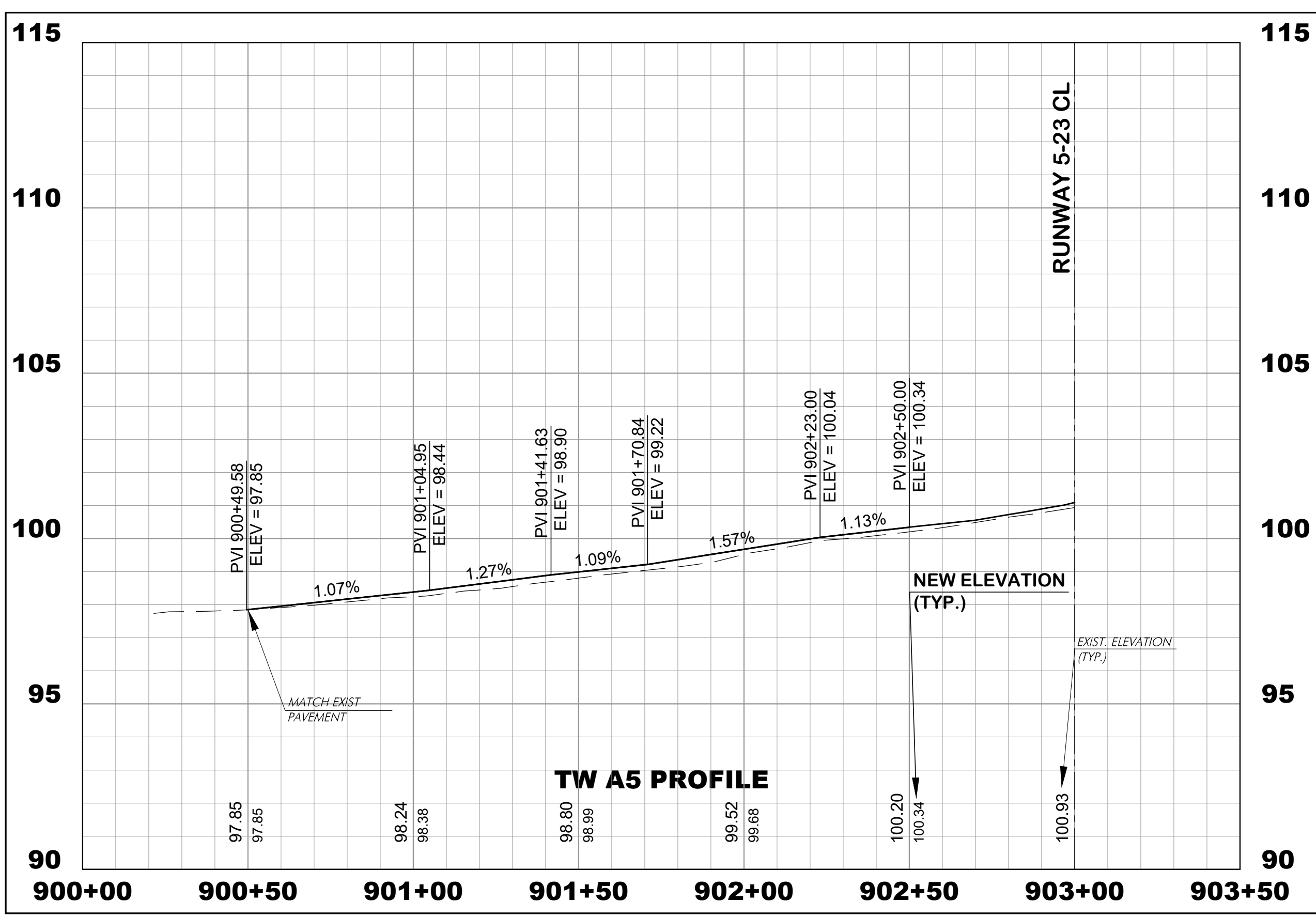
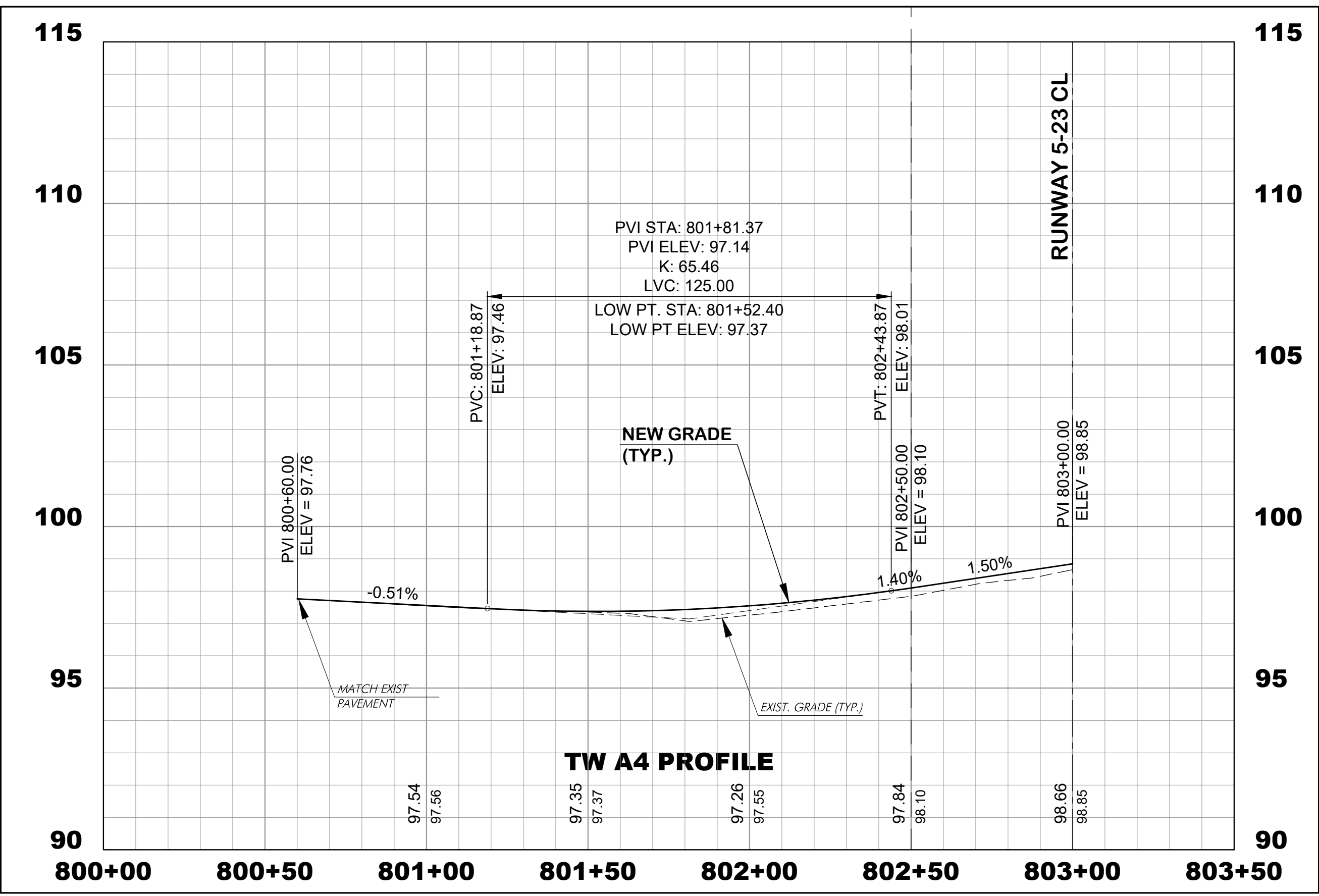
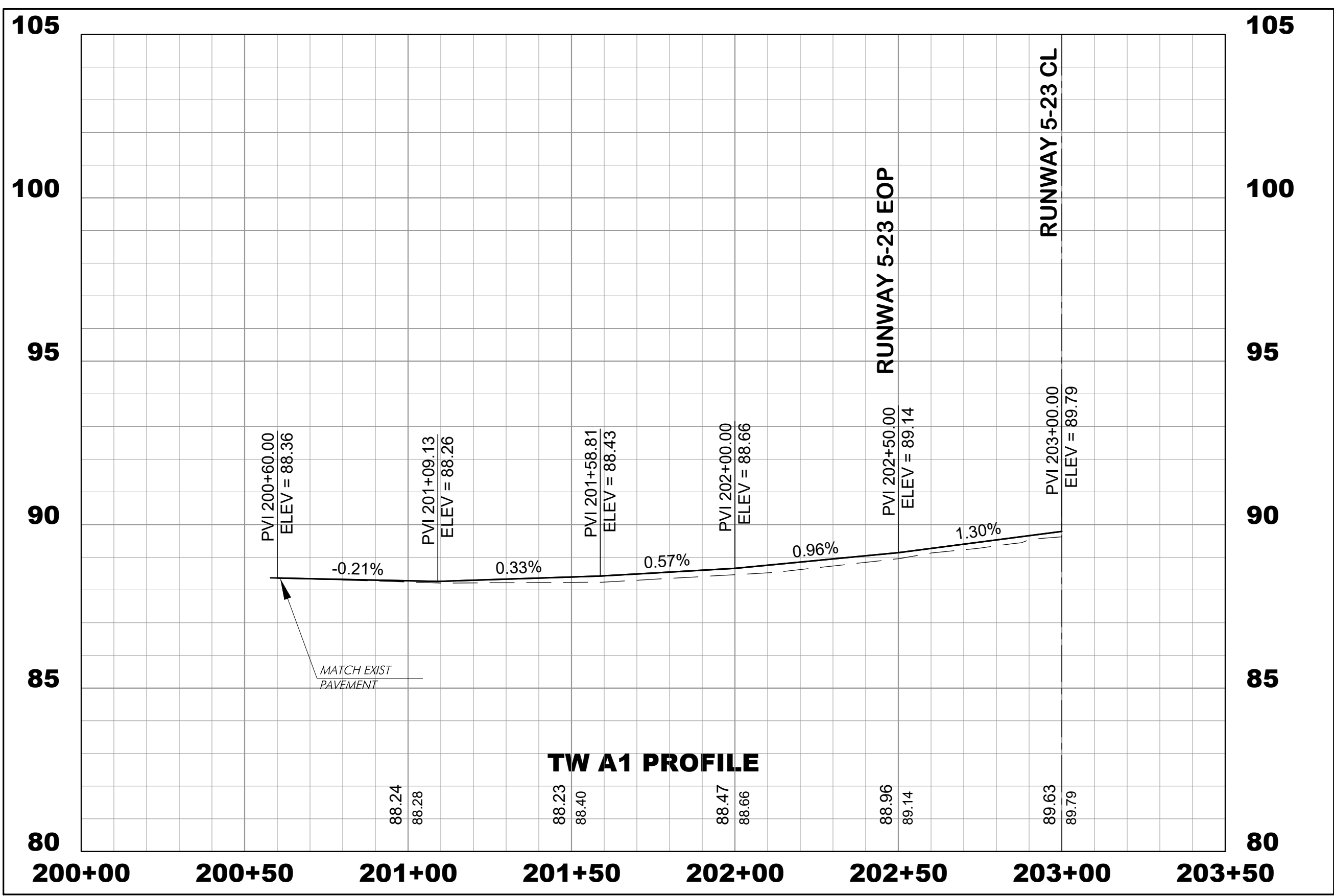
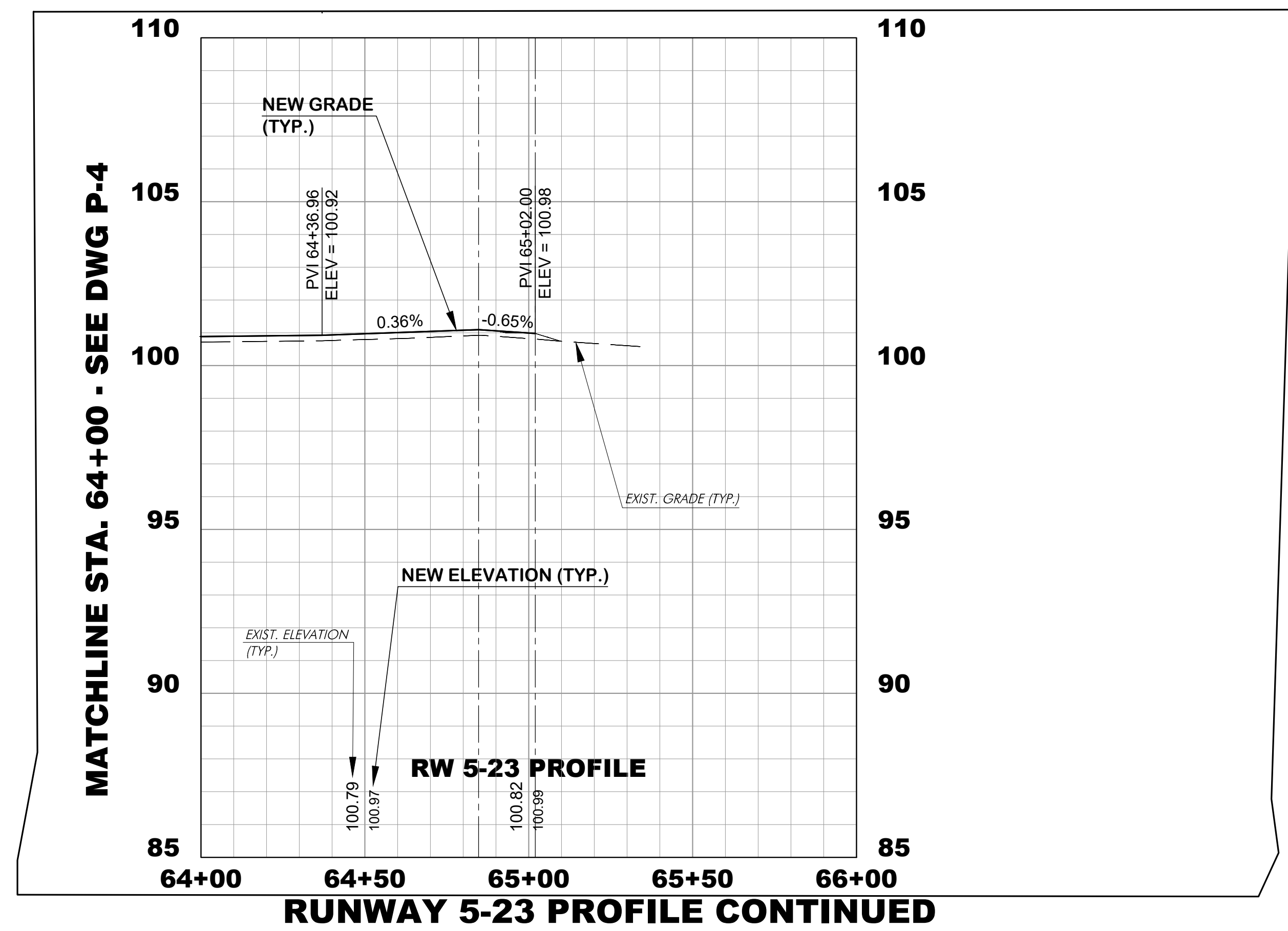
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Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 STA. 64+00 TO 66+00 PROFILE TAXIWAYS A1, A4 & A5 CENTERLINE PROFILES

FAA A.I.P. Project Number:
3-45-0057-025-2025

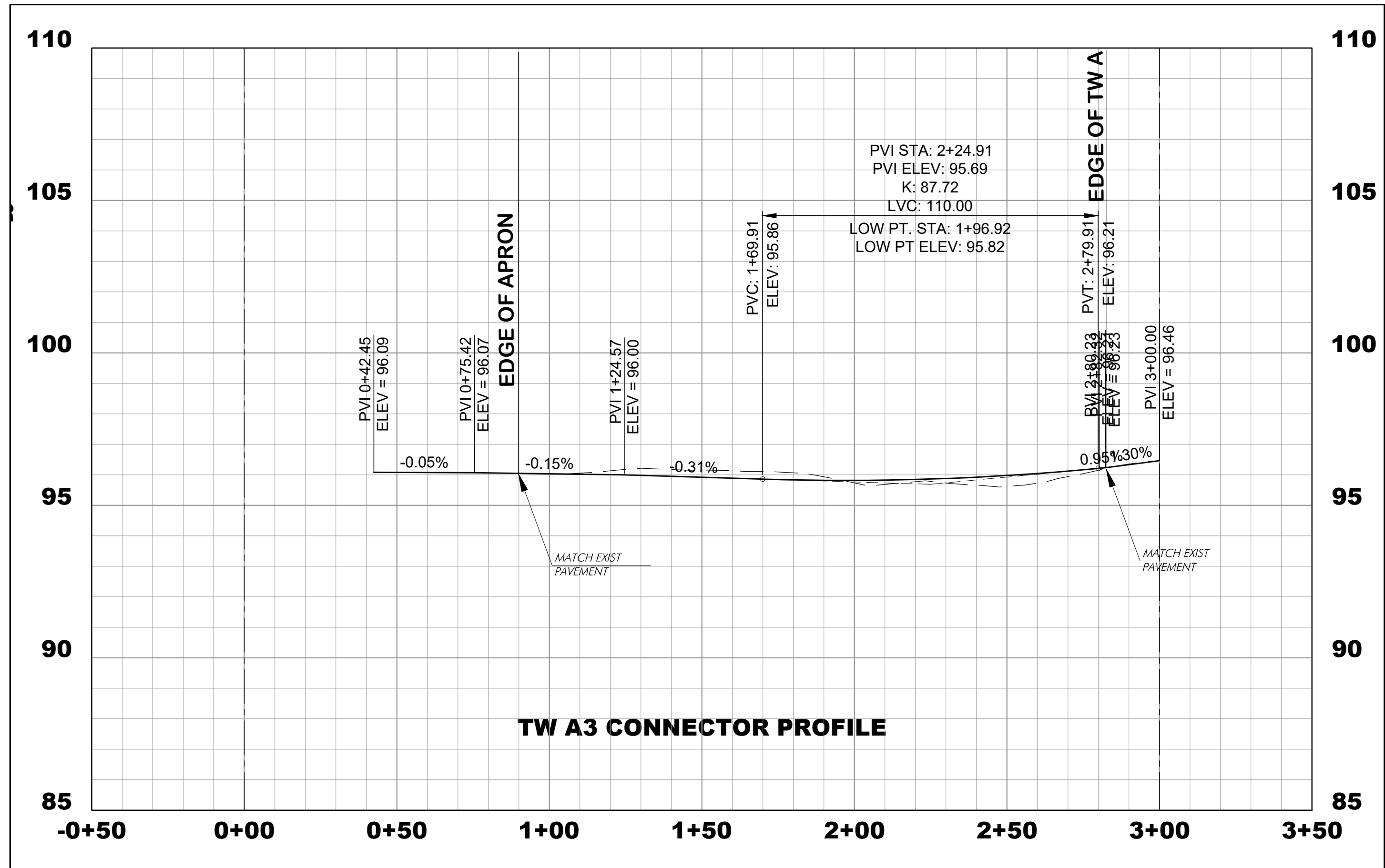
Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"=50 HORIZ.** Sheet Number:

1"=5' VERT. Drawing Number:
P-5

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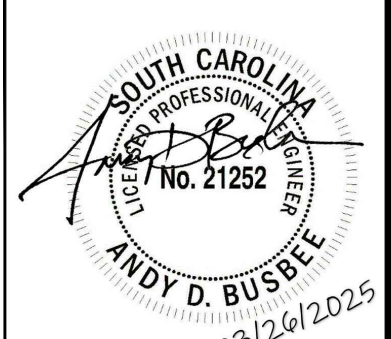


WALTERBORO, SOUTH CAROLINA

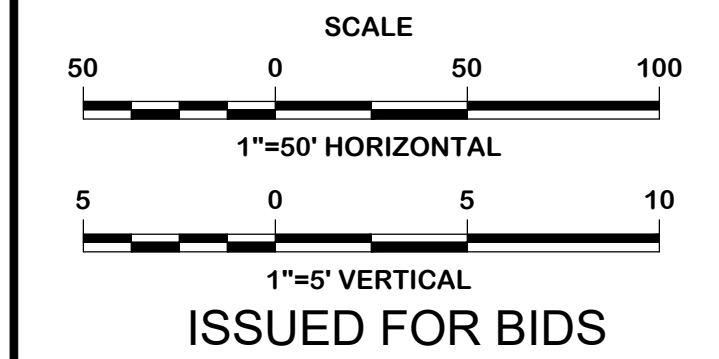
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700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

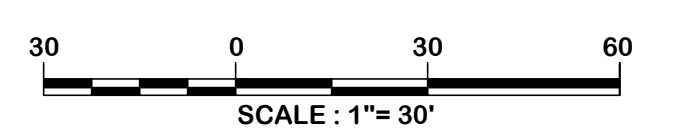
Drawing Name:
RUNWAY 5-23 MIDFIELD TAXIWAY A3 CENTERLINE PROFILE

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ.	Sheet Number:
1"=5' VERT.	Drawing Number: P-6



Designer: ADB	Checked by: ADB
Technician: LAW	Project Number: 192930



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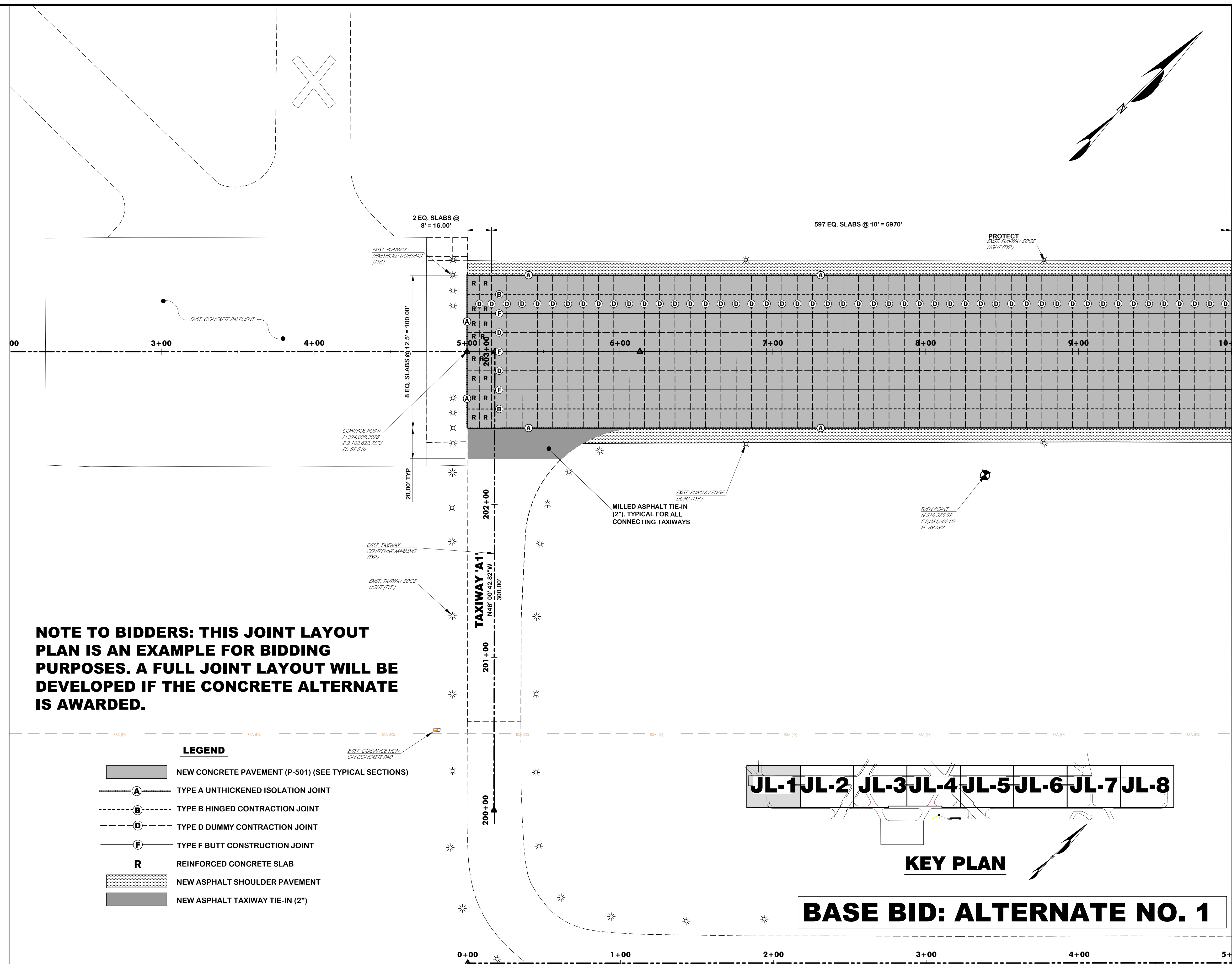
REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
JOINT LAYOUT PLAN NO. 1

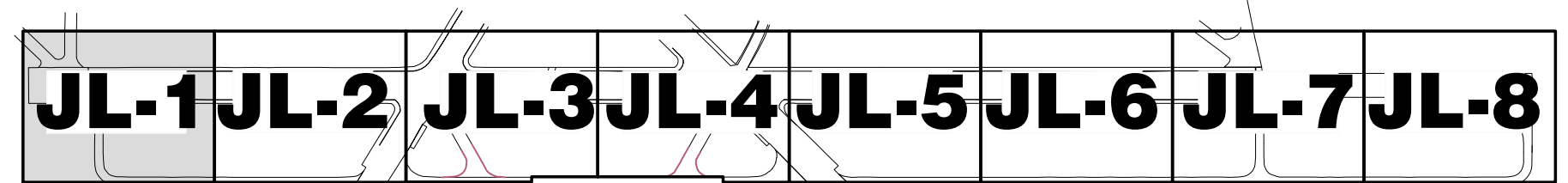
FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=30'	Sheet Number: JL-1
Drawing Number: JL-1	

MATCHLINE STA. 10+00, SEE DWG. JL-2



NOTE TO BIDDERS: THIS JOINT LAYOUT PLAN IS AN EXAMPLE FOR BIDDING PURPOSES. A FULL JOINT LAYOUT WILL BE DEVELOPED IF THE CONCRETE ALTERNATE IS AWARDED.

- LEGEND**
- NEW CONCRETE PAVEMENT (P-501) (SEE TYPICAL SECTIONS)
 - A** TYPE A UNTHICKENED ISOLATION JOINT
 - B** TYPE B HINGED CONTRACTION JOINT
 - D** TYPE D DUMMY CONTRACTION JOINT
 - F** TYPE F BUTT CONSTRUCTION JOINT
 - R** REINFORCED CONCRETE SLAB
 - NEW ASPHALT SHOULDER PAVEMENT
 - NEW ASPHALT TAXIWAY TIE-IN (2")

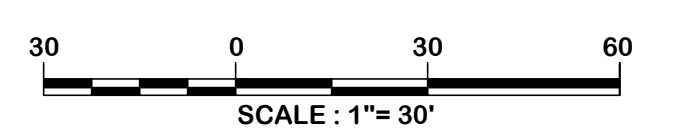


KEY PLAN

BASE BID: ALTERNATE NO. 1



Designer: BCM / NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

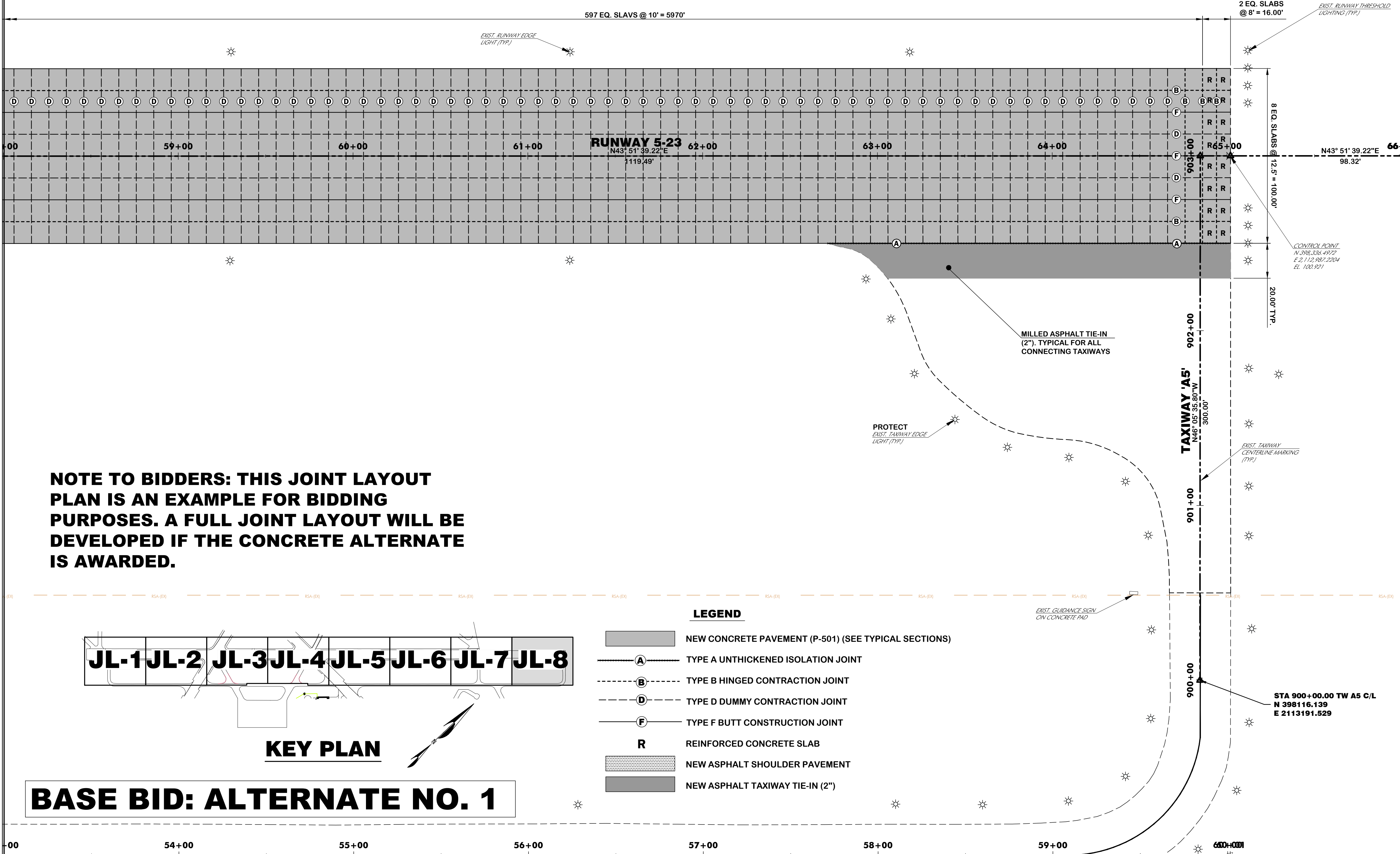
REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

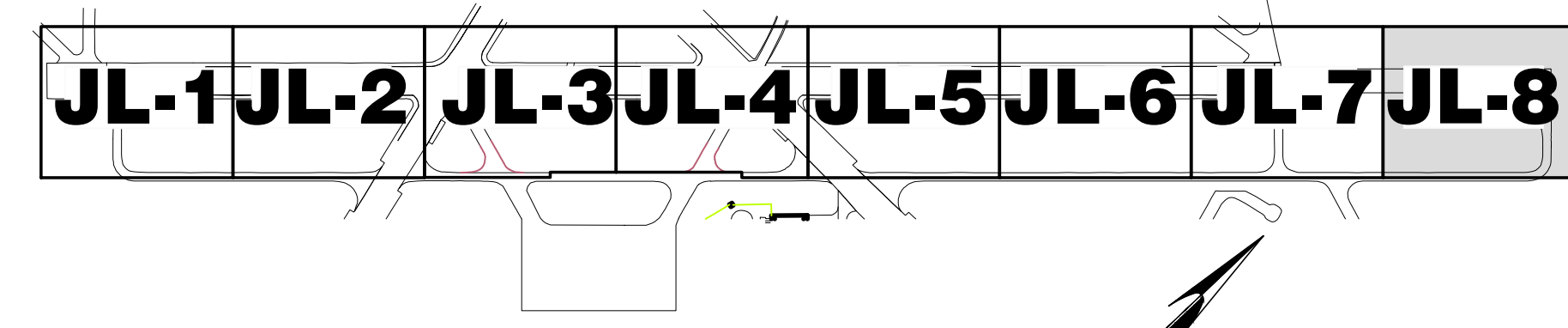
Drawing Name:
JOINT LAYOUT PLAN NO. 8

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
Drawing Number: JL-8	

MATCHLINE STA. 58+00, SEE DWG. JL-7



NOTE TO BIDDERS: THIS JOINT LAYOUT PLAN IS AN EXAMPLE FOR BIDDING PURPOSES. A FULL JOINT LAYOUT WILL BE DEVELOPED IF THE CONCRETE ALTERNATE IS AWARDED.



KEY PLAN

BASE BID: ALTERNATE NO. 1

- LEGEND**
- NEW CONCRETE PAVEMENT (P-501) (SEE TYPICAL SECTIONS)
 - TYPE A UNTHICKENED ISOLATION JOINT
 - TYPE B HINGED CONTRACTION JOINT
 - TYPE D DUMMY CONTRACTION JOINT
 - TYPE F BUTT CONSTRUCTION JOINT
 - REINFORCED CONCRETE SLAB
 - NEW ASPHALT SHOULDER PAVEMENT
 - NEW ASPHALT TAXIWAY TIE-IN (2")



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Technician: NWM
Project Number: 192930

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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

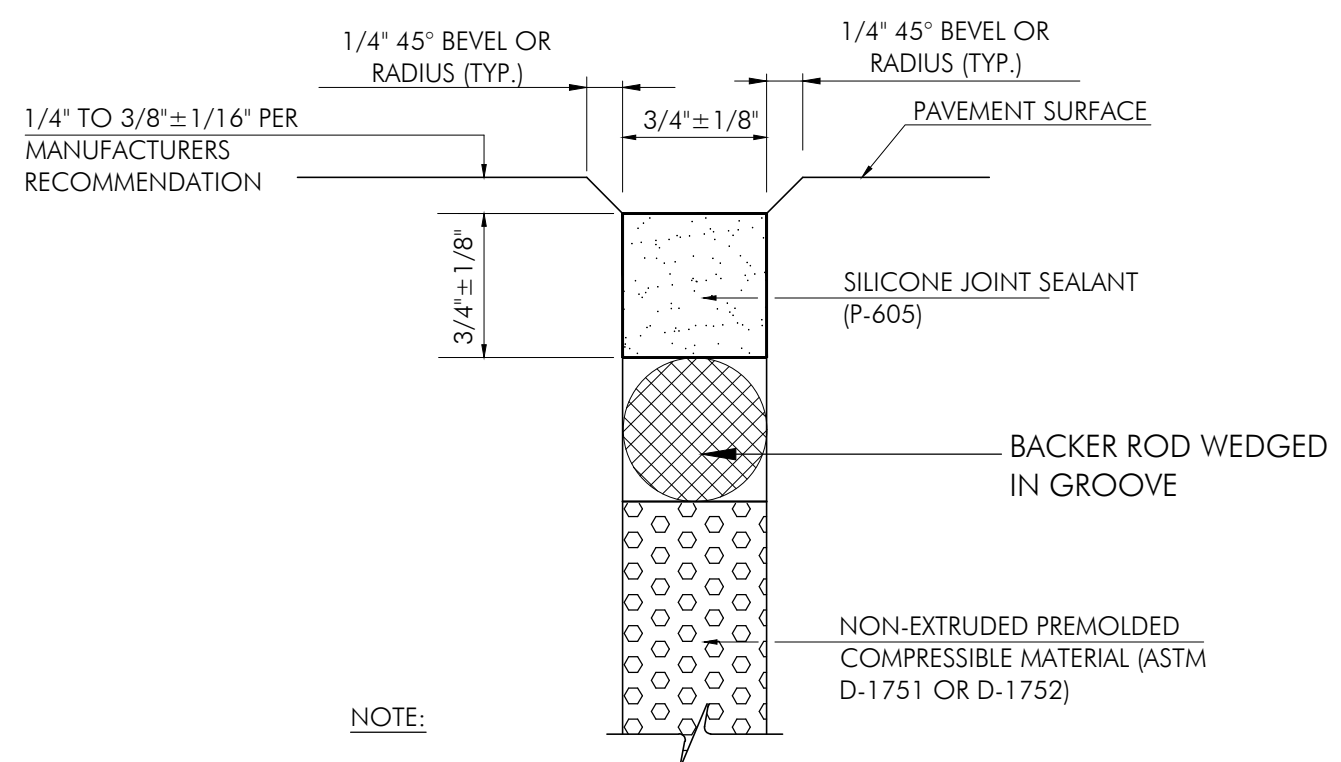
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JOINT DETAILS

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

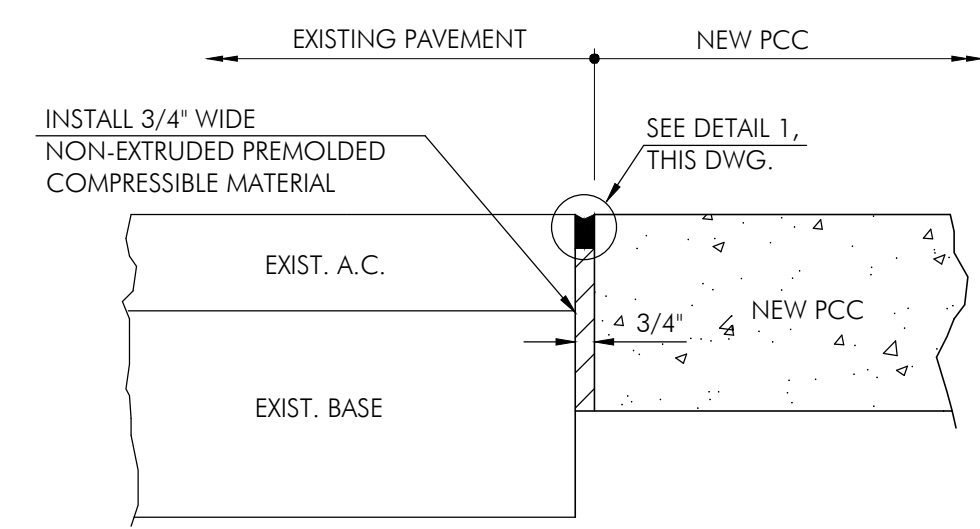
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Drawing Number:
JD-1



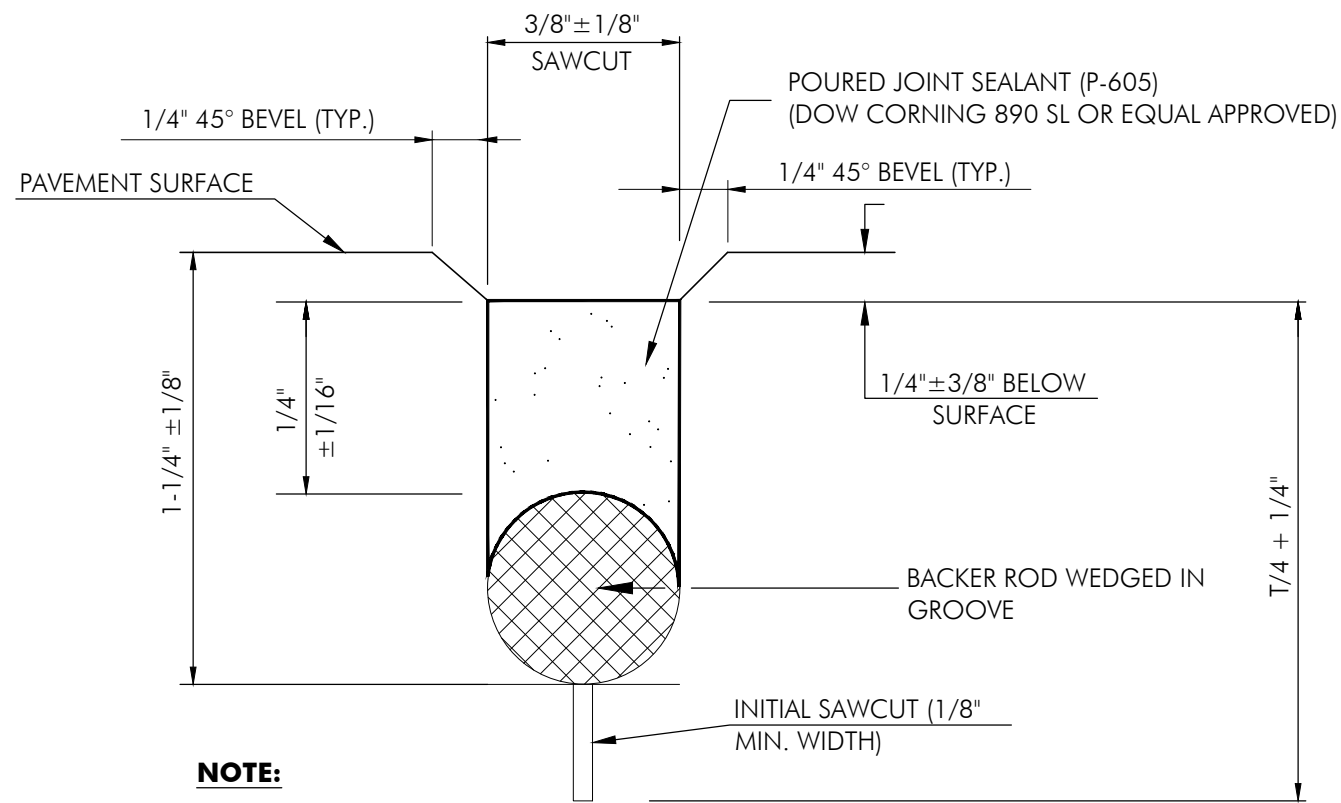
DETAIL 1 ISOLATION JOINT SEALANT

N.T.S.



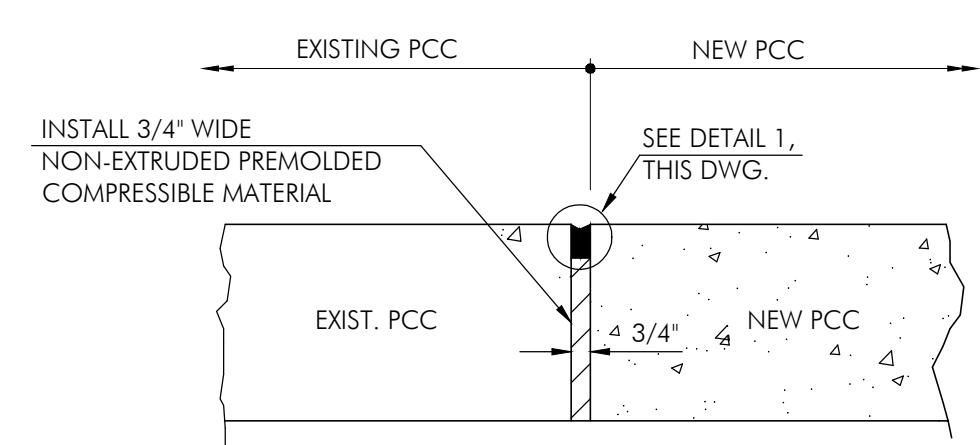
TYPE "A" UN-THICKENED EDGE ISOLATION JOINT AT ASPHALT/PCC INTERFACE

N.T.S.



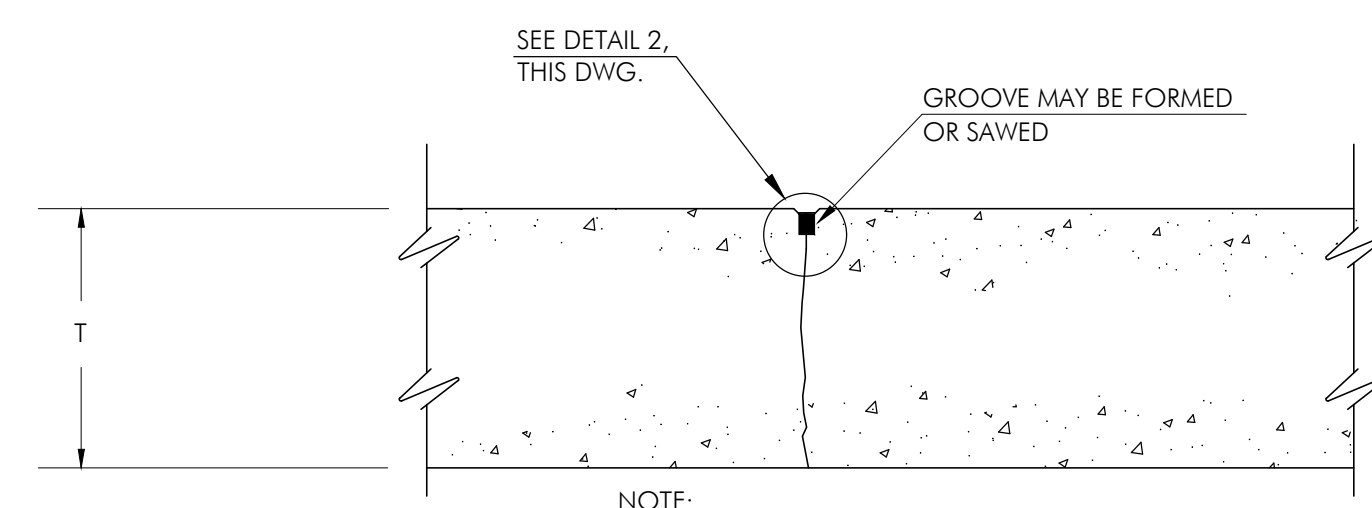
DETAIL 2 CONTRACTION JOINT SEALANT

N.T.S.



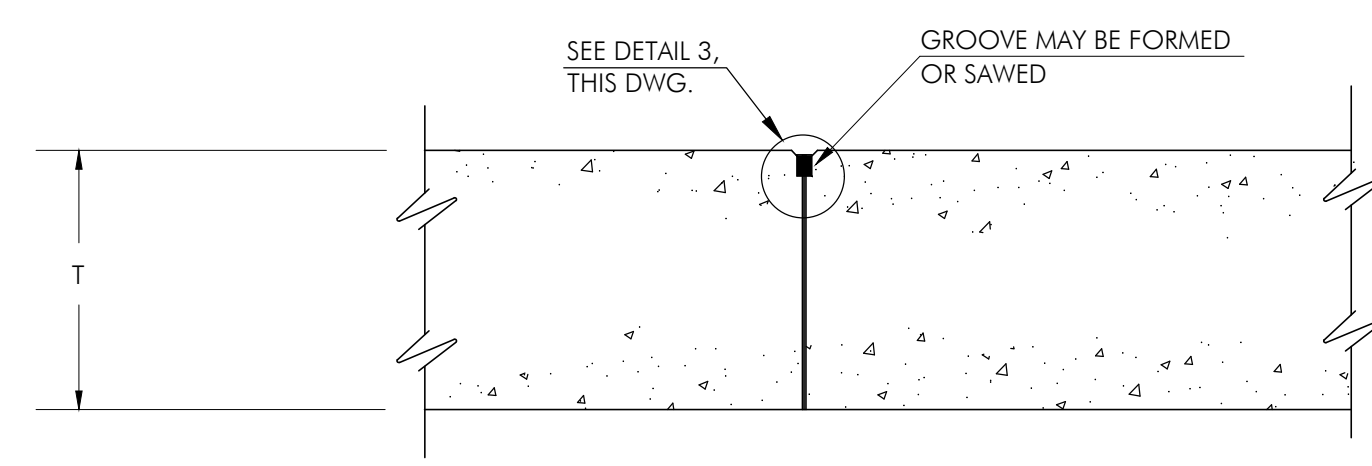
TYPE "A" UN-THICKENED EDGE ISOLATION JOINT AT PCC/PCC INTERFACE

N.T.S.



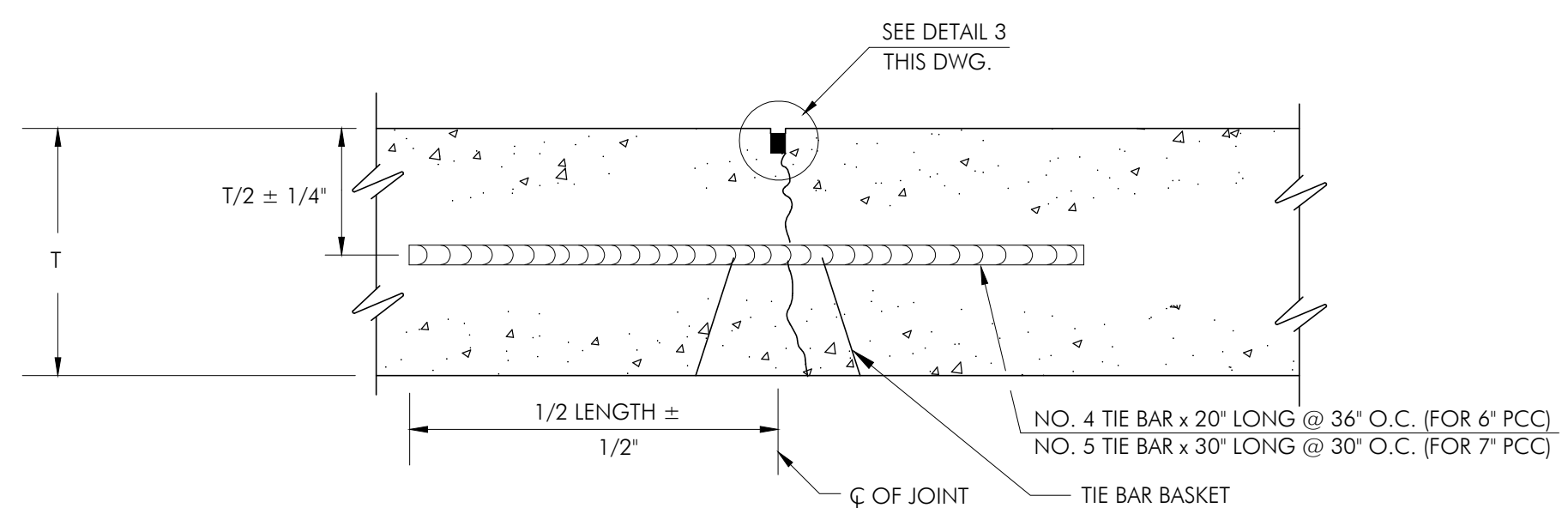
TYPE D - DUMMY CONTRACTION JOINT

N.T.S.



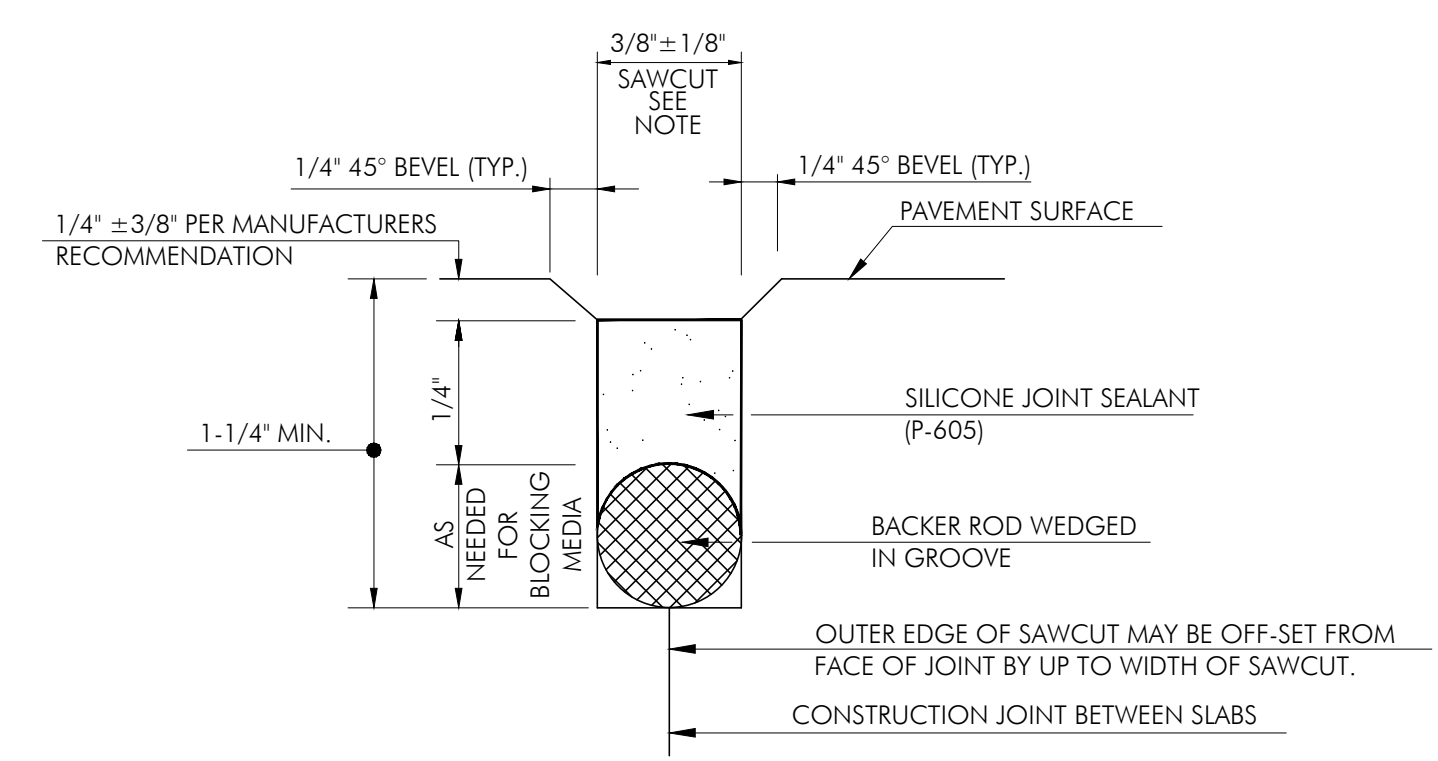
TYPE F - BUTT CONSTRUCTION JOINT

N.T.S.



TYPE B - HINGED CONTRACTION JOINT

N.T.S.



DETAIL 3 CONSTRUCTION JOINT SEALANT

N.T.S.

BASE BID: ALTERNATE NO. 1

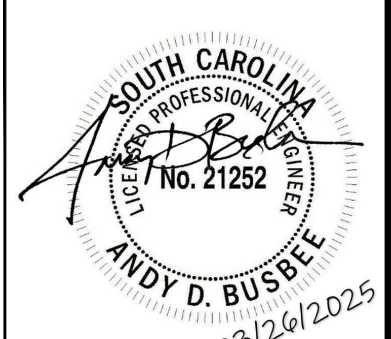
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Designer: ADB/NWM	Checked by: ADB
Technician: NWM	Project Number: 192930

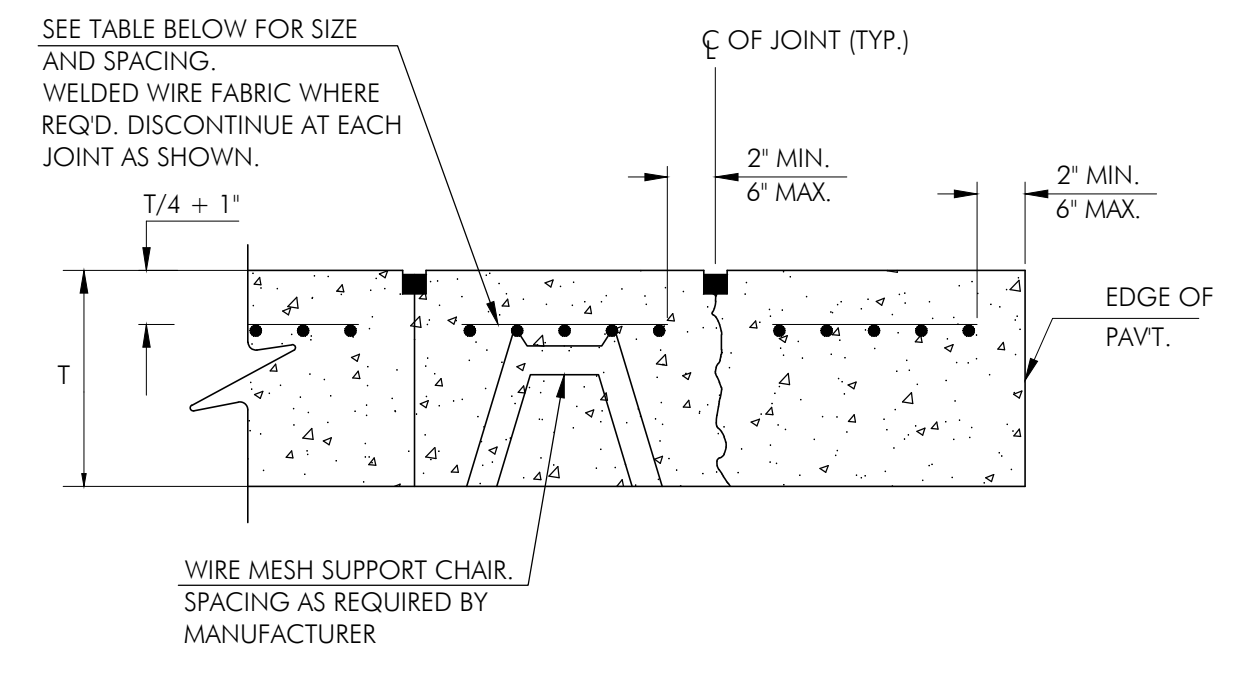
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
JOINT DETAILS

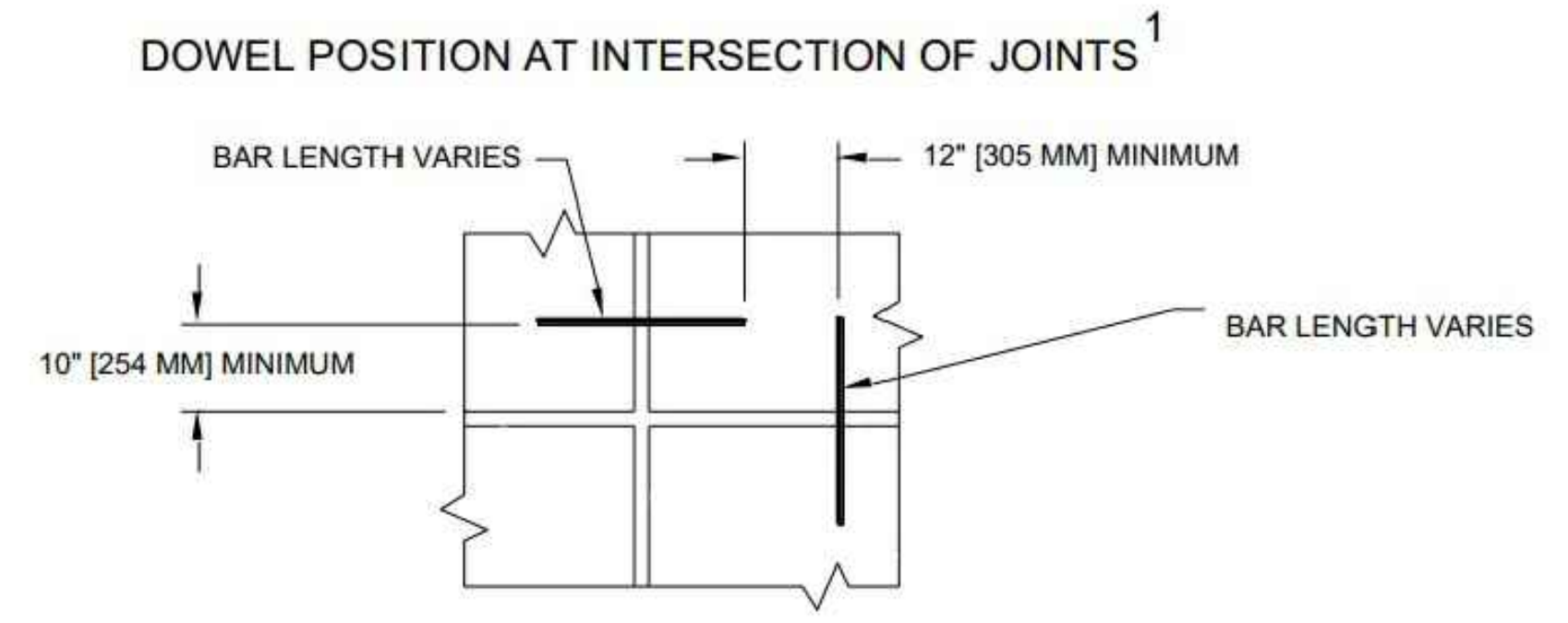
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Date: MARCH, 2025	Division: AIRPORTS
Scale: N.T.S.	Sheet Number: JD-2
Drawing Number: JD-2	



TYPICAL SECTION REINFORCED CONCRETE PAVEMENT SLABS

N.T.S.

WWF REINFORCING SCHEDULE		
Slab Thickness (T)	WWF Wire Size	WWF Spacing
5" to 9"	W2.9 x W2.9	6 x 6
10" to 13"	W4 x W4	6 x 6
14" to 15"	W4.5 x W4.5	6 x 6
16"	W5 x W5	6 x 6
16" to 18"	W5.5 x W5.5	6 x 6
19" to 20"	W6 x W6	6 x 6

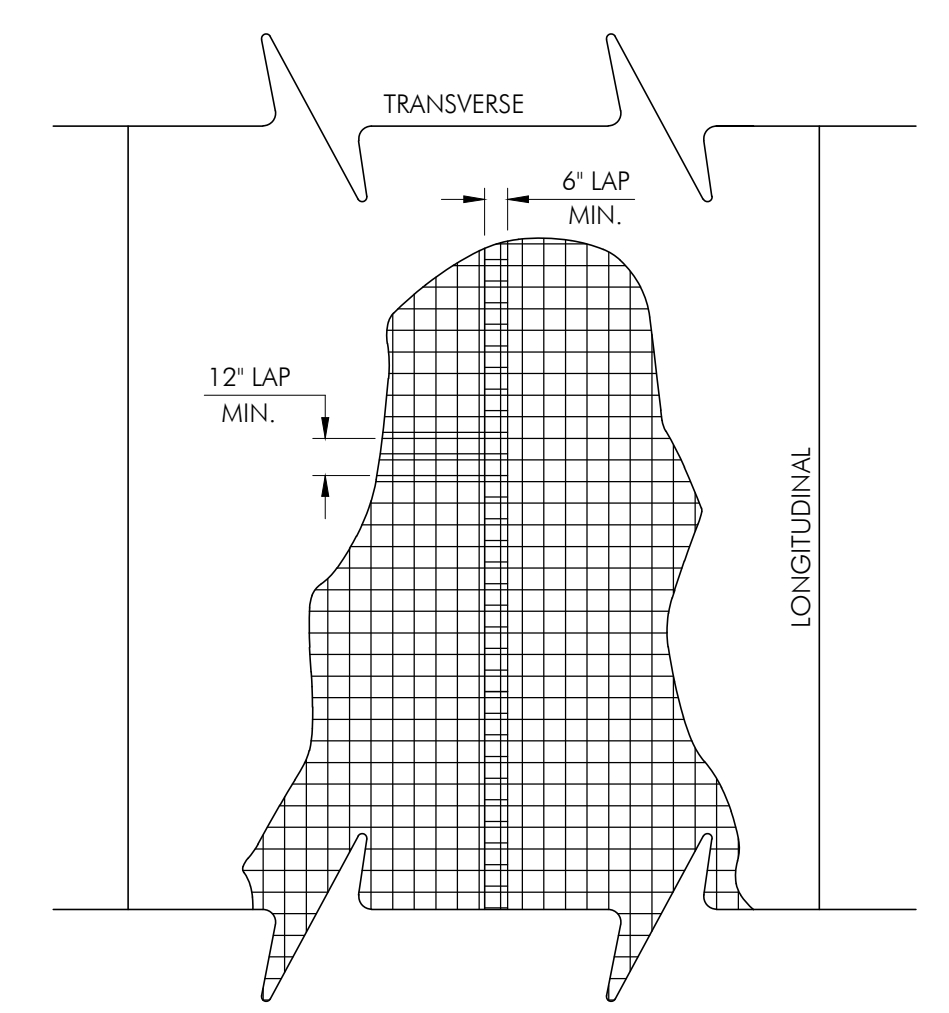


NOTES:

- ELIMINATE DOWEL, OR TIE BAR, FROM LONGITUDINAL JOINT AS NECESSARY TO MAINTAIN 12 INCH [300 MM] FROM END OF TRAVERSE DOWEL BARS.

DOWEL BAR SPACING

N.T.S.



PLAN WELDED WIRE FABRIC SPLICE

N.T.S.

BASE BID: ALTERNATE NO. 1

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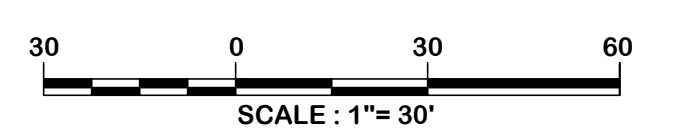
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Technician: LAW	Project Number: 192930



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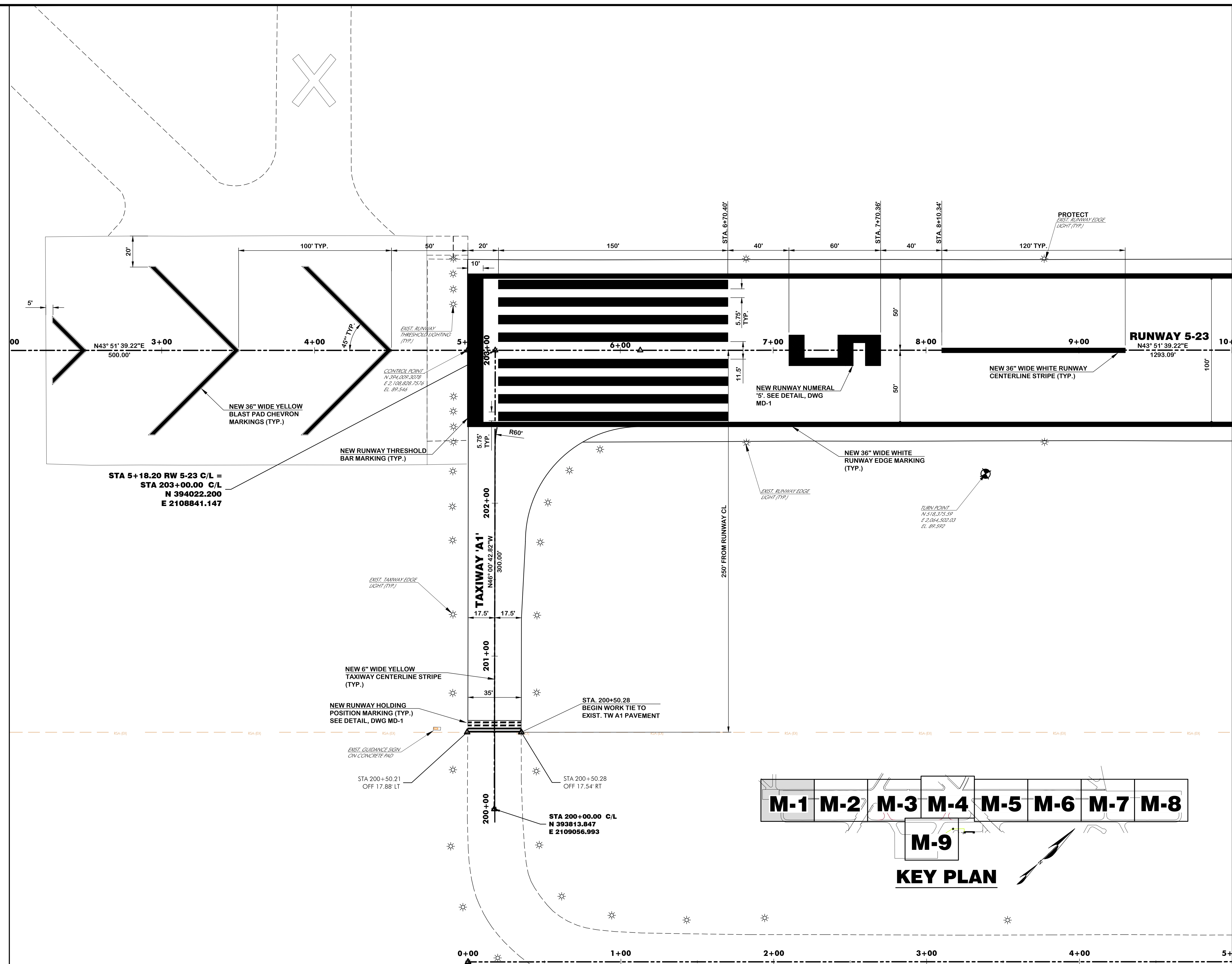
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
MARKING PLAN NO. 1

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1" = 30'	Sheet Number: M-1
Drawing Number: M-1	

MATCHLINE STA. 10+00, SEE DWG. M-2



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No.	Description	Date	By

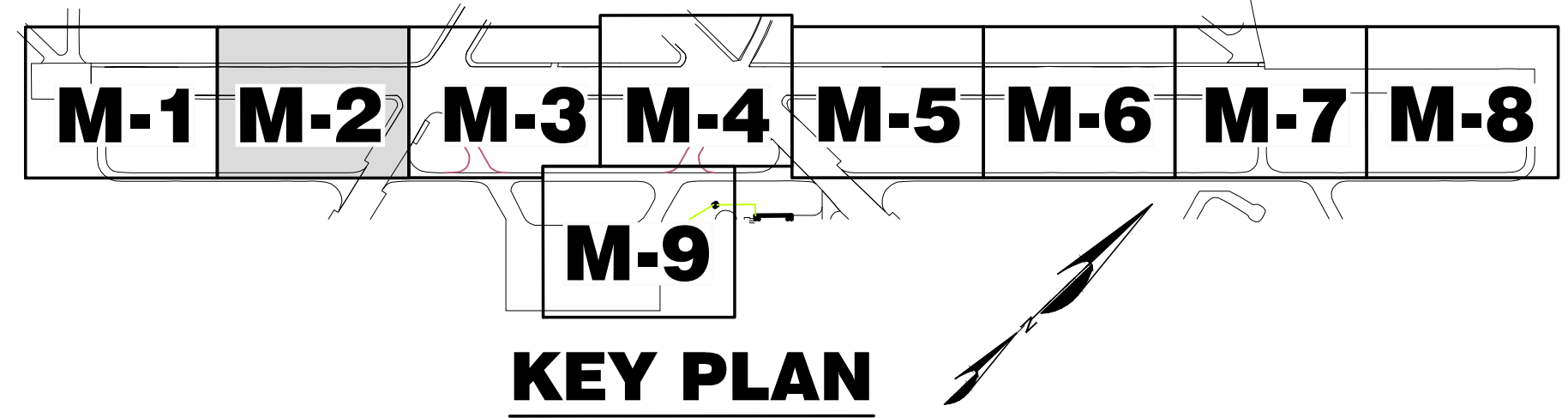
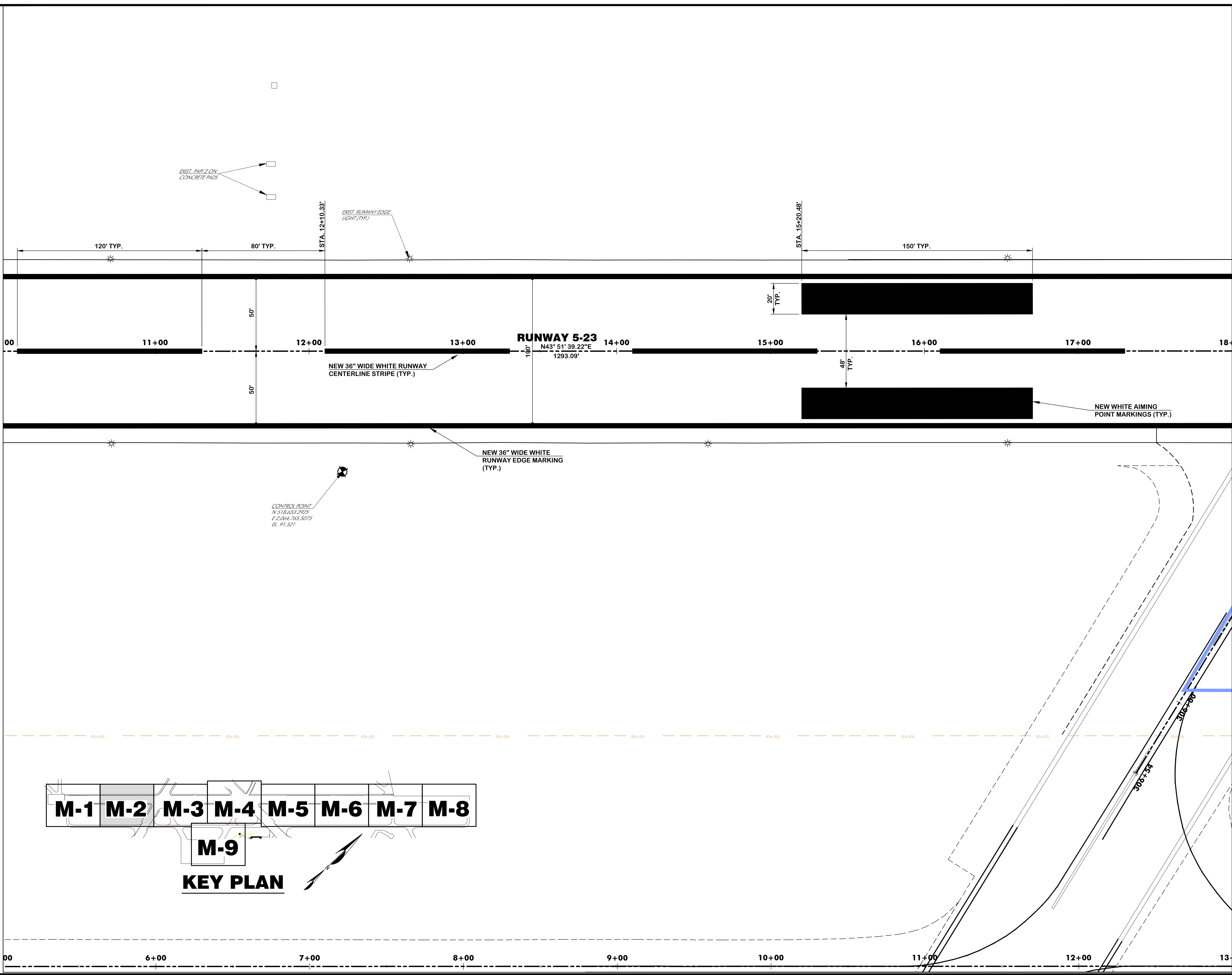
Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
MARKING PLAN NO. 2

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
Drawing Number: M-2	

MATCHLINE STA. 10+00, SEE DWG. M-1

MATCHLINE STA. 18+00, SEE DWG. M-3



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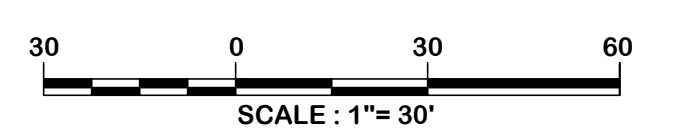
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Checked by: ADB
Technician: LAW
Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

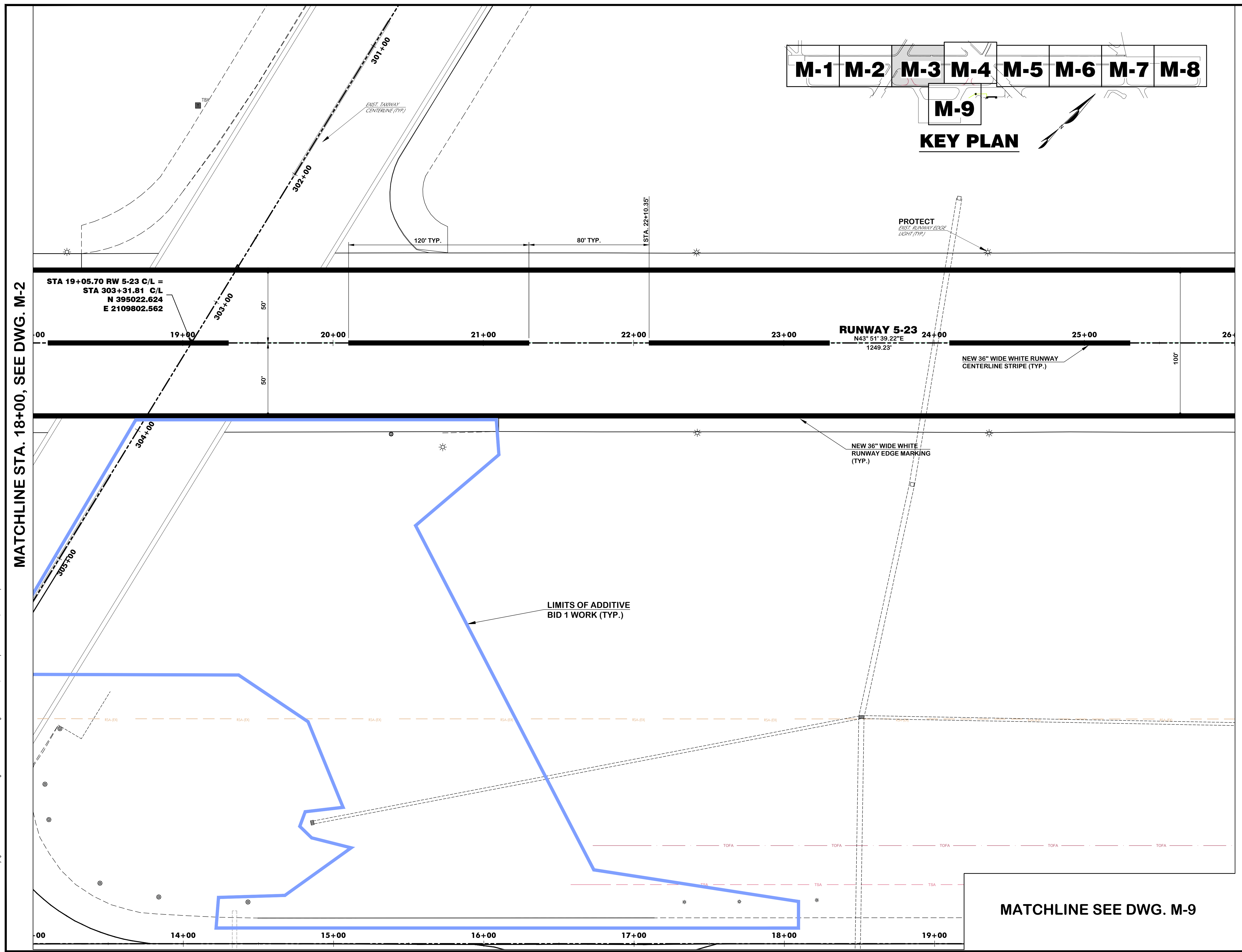
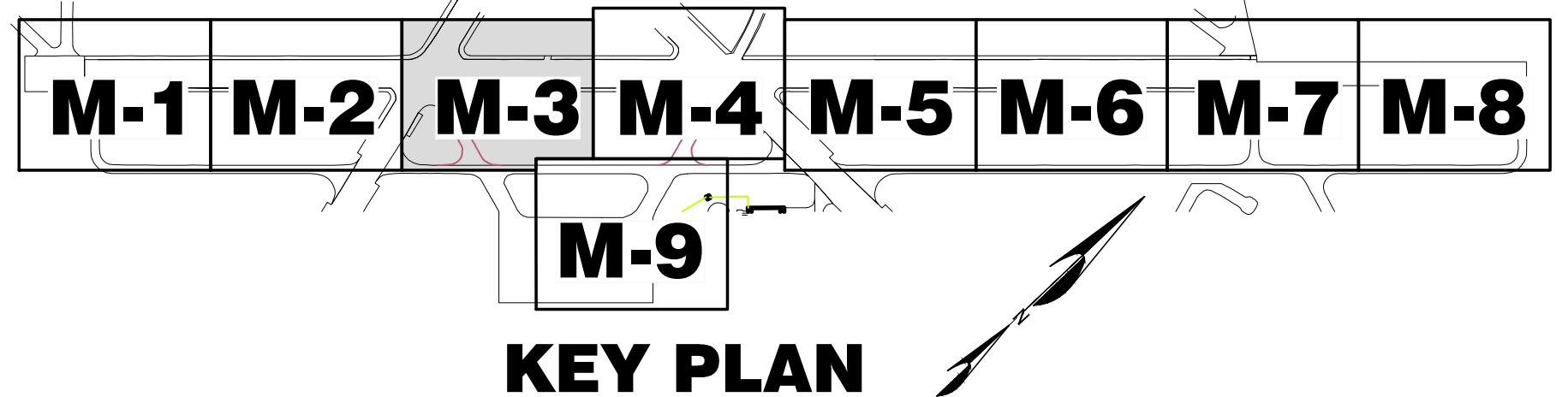
Drawing Name:
MARKING PLAN NO. 3

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number:
M-3



MATCHLINE STA. 18+00, SEE DWG. M-2

MATCHLINE STA. 26+00, SEE DWG. M-4

MATCHLINE SEE DWG. M-9

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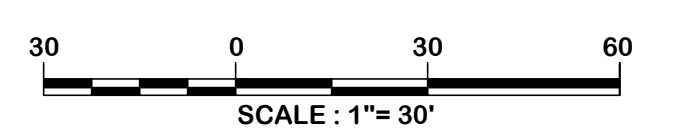
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Technician: LAW
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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
MARKING PLAN NO. 4

FAA A.I.P. Project Number:
3-45-0057-025-2025

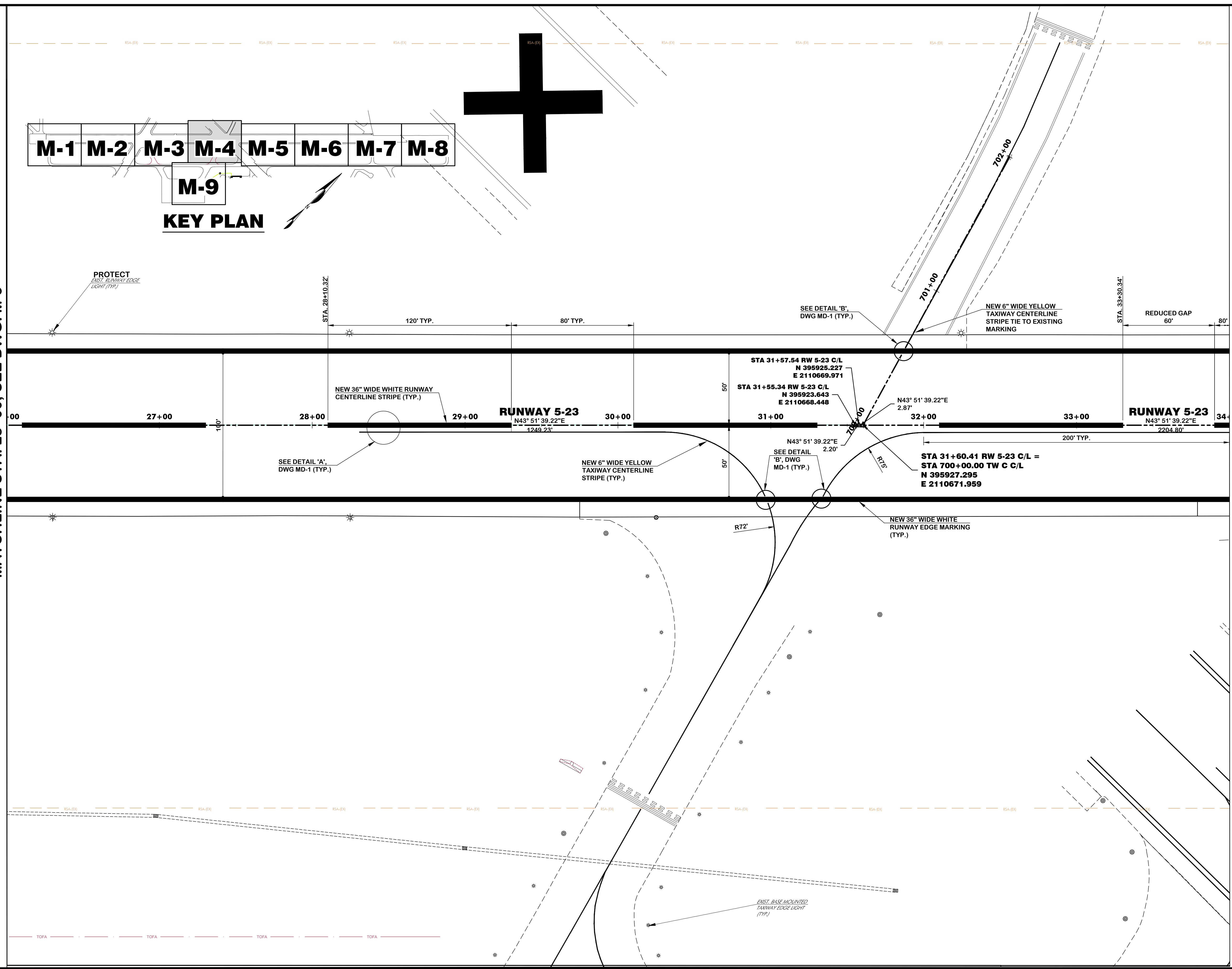
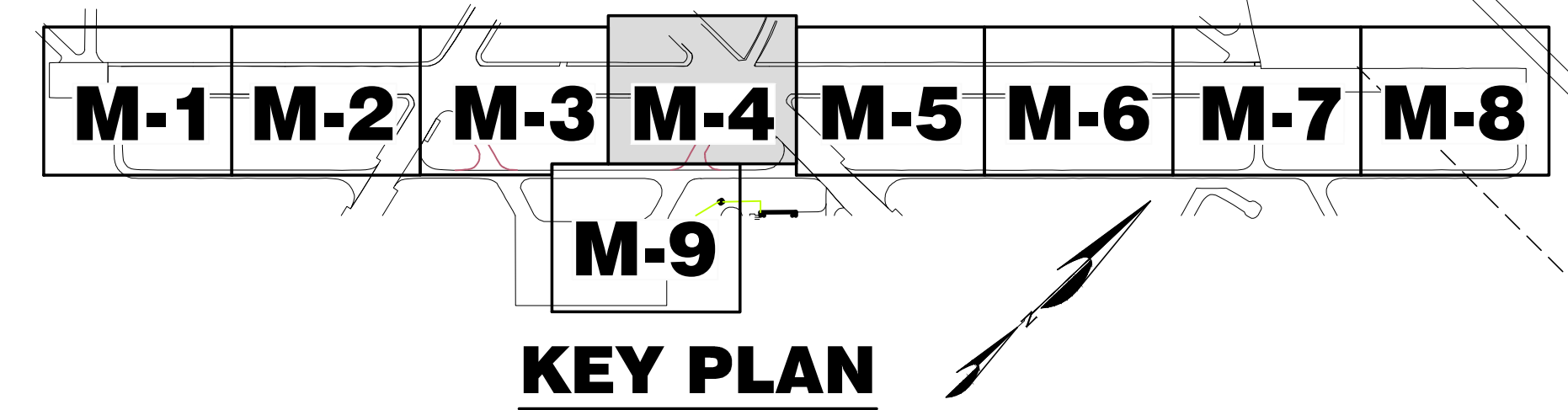
Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number:
M-4

MATCHLINE STA. 28+00, SEE DWG. M-3

MATCHLINE STA. 34+00, SEE DWG. M-5



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Technician: LAW
Project Number: 192930



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No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

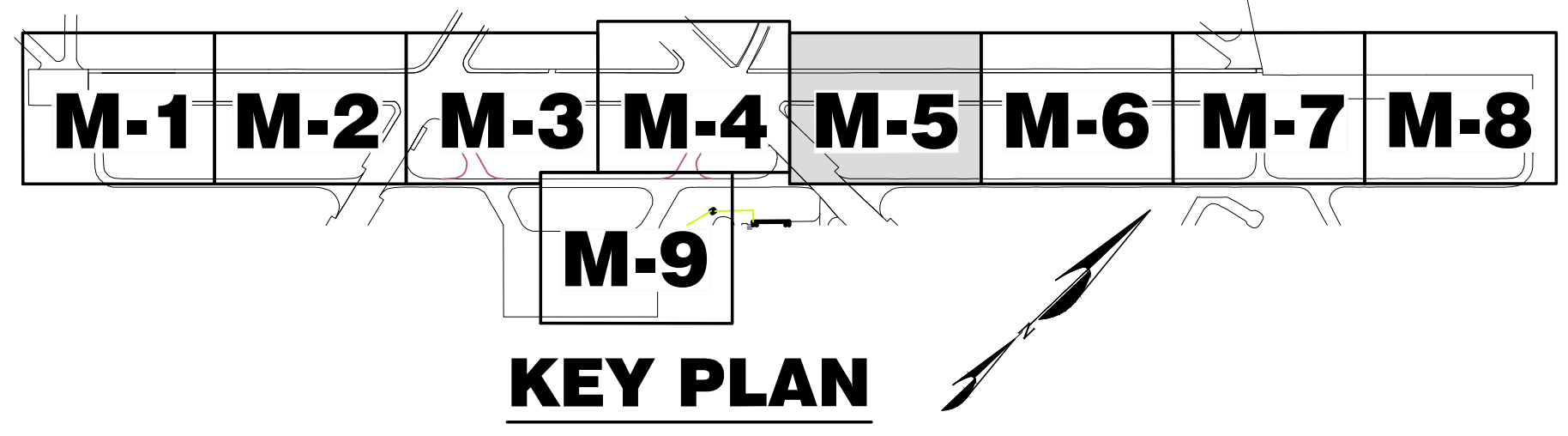
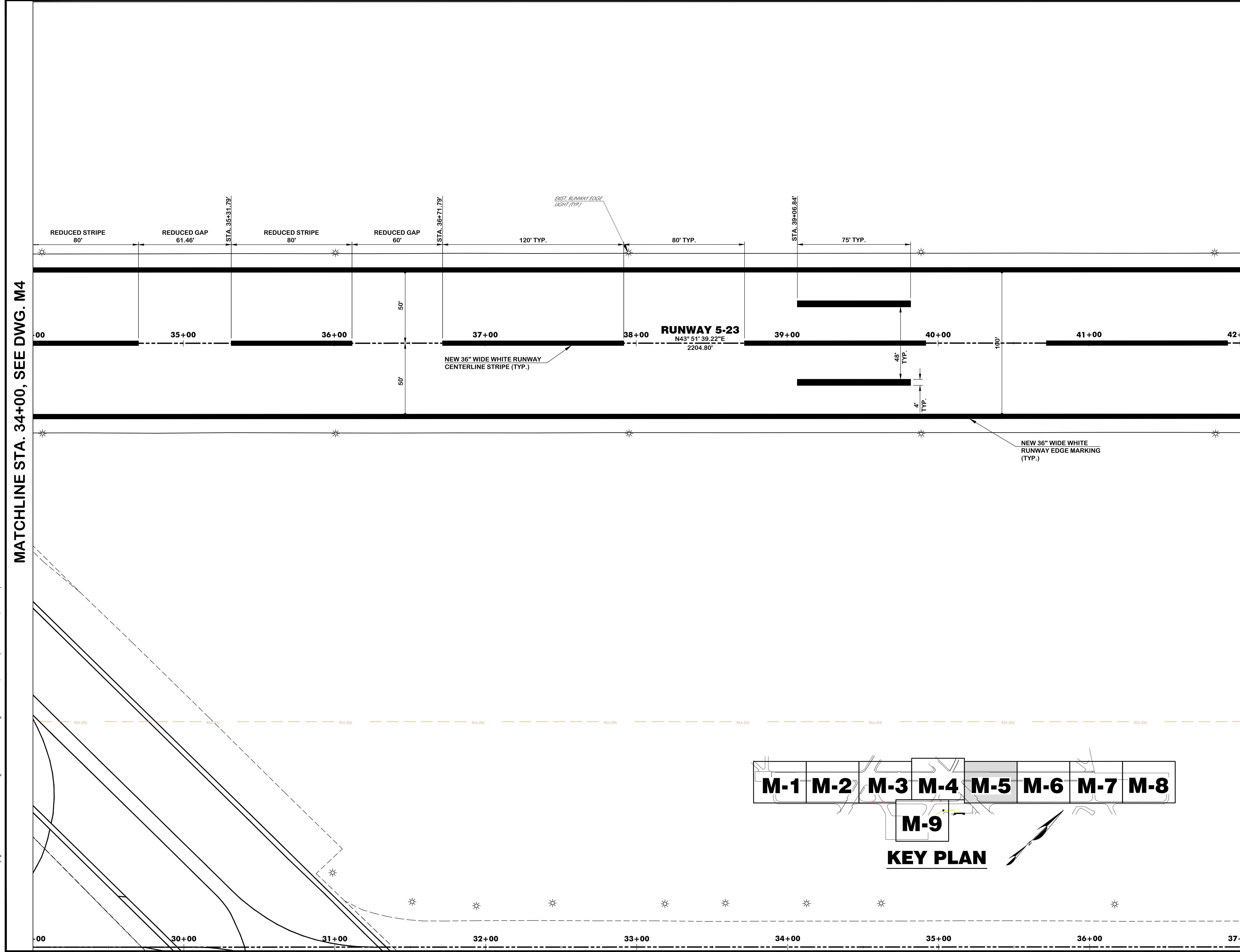
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FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number:
M-5



MATCHLINE STA. 34+00, SEE DWG. M-4

MATCHLINE STA. 4+00, SEE DWG. M-6

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Technician: LAW	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
MARKING PLAN NO. 6

FAA A.I.P. Project Number:
3-45-0057-025-2025

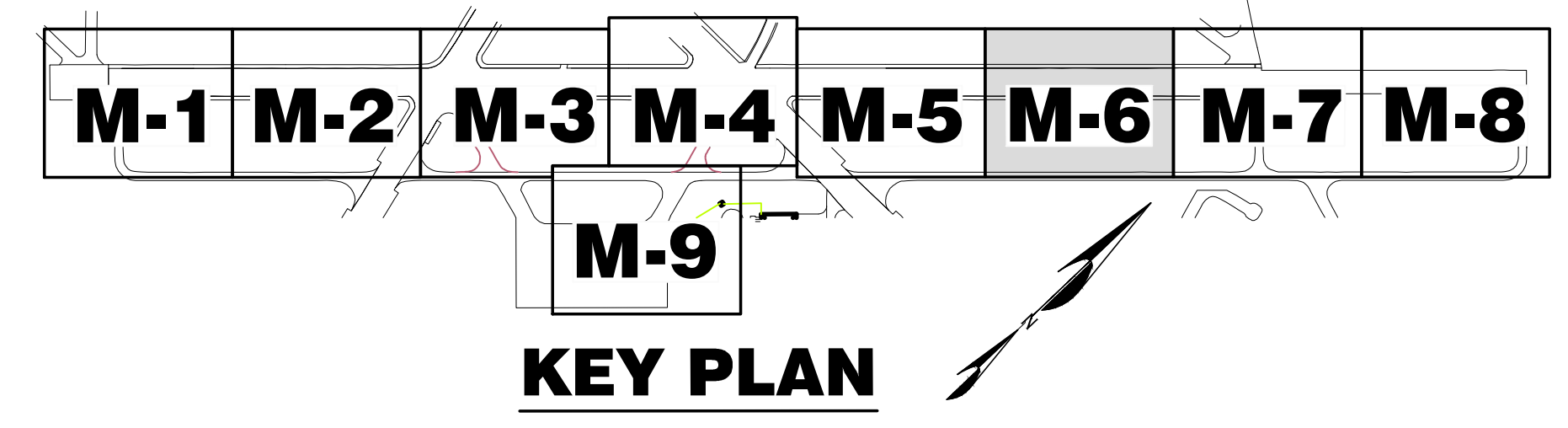
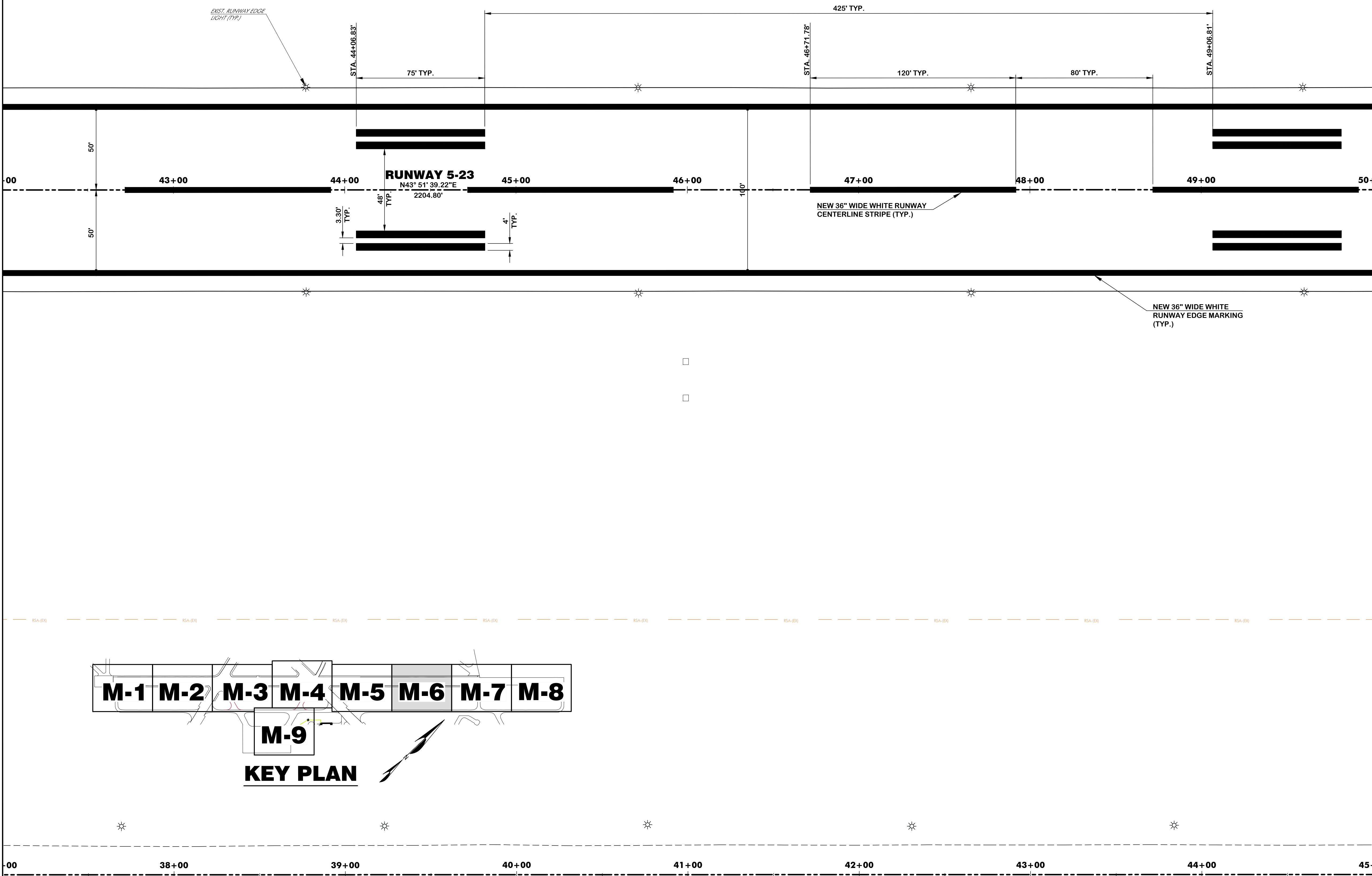
Date: MARCH, 2025	Division: AIRPORTS
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Scale: 1" = 30'	Sheet Number: -
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Drawing Number: M-6

MATCHLINE STA. 42+00, SEE DWG. M-5

MATCHLINE STA. 50+00, SEE DWG. M-7



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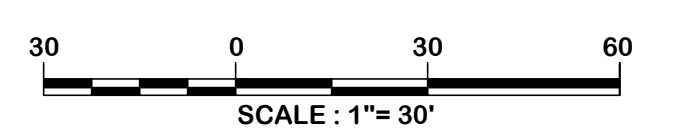
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Checked by: ADB
Technician: LAW
Project Number: 192930



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No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

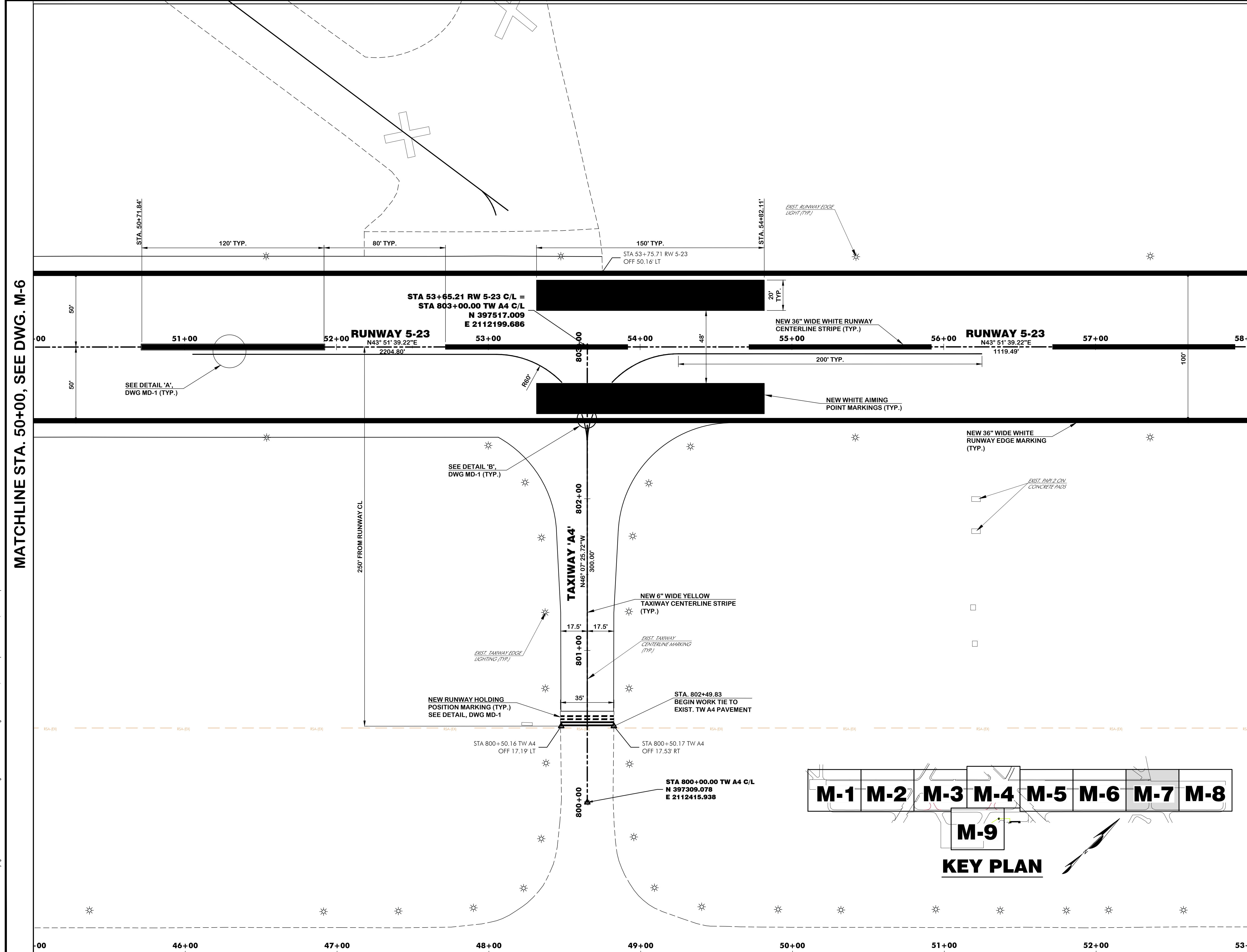
Drawing Name:
MARKING PLAN NO. 7

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"= 30'** Sheet Number: **-**

Drawing Number: **M-7**



MATCHLINE STA. 50+00, SEE DWG. M-6

MATCHLINE STA. 58+00, SEE DWG. M-8

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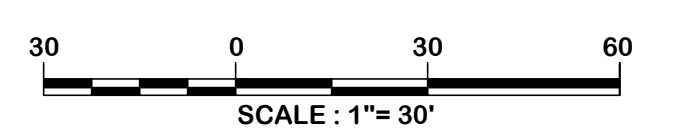
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Designer: BCM / LAW
Checked by: ADB
Technician: LAW
Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
MARKING PLAN NO. 8

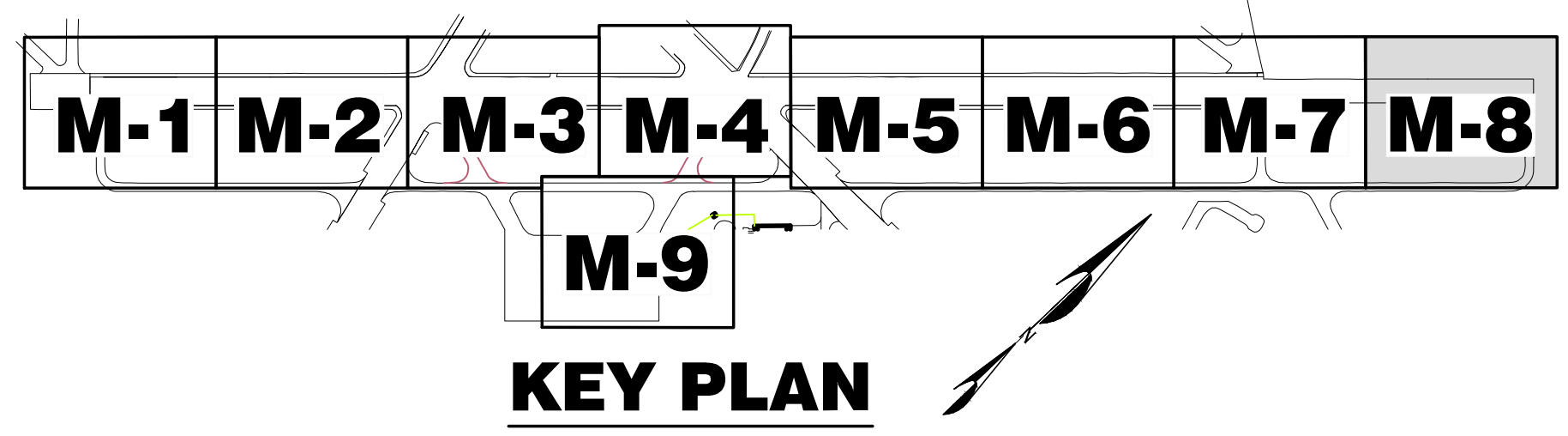
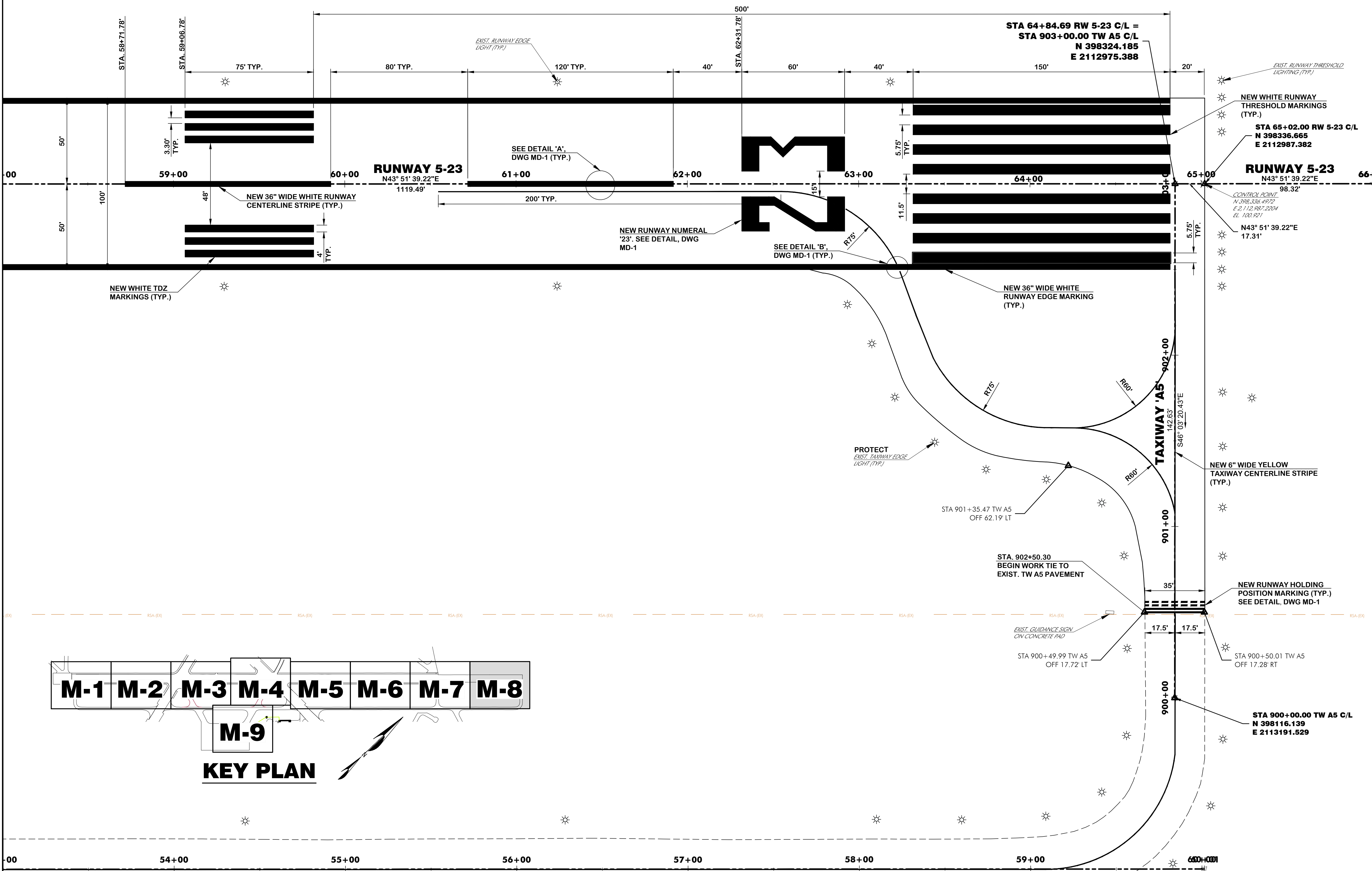
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3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

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Drawing Number: **M-8**

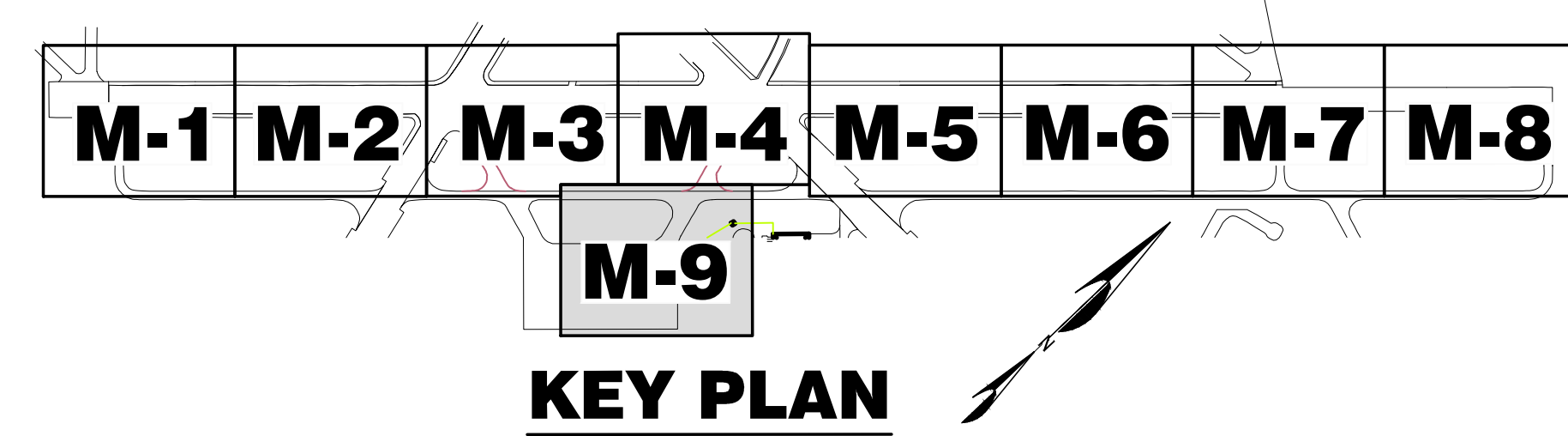
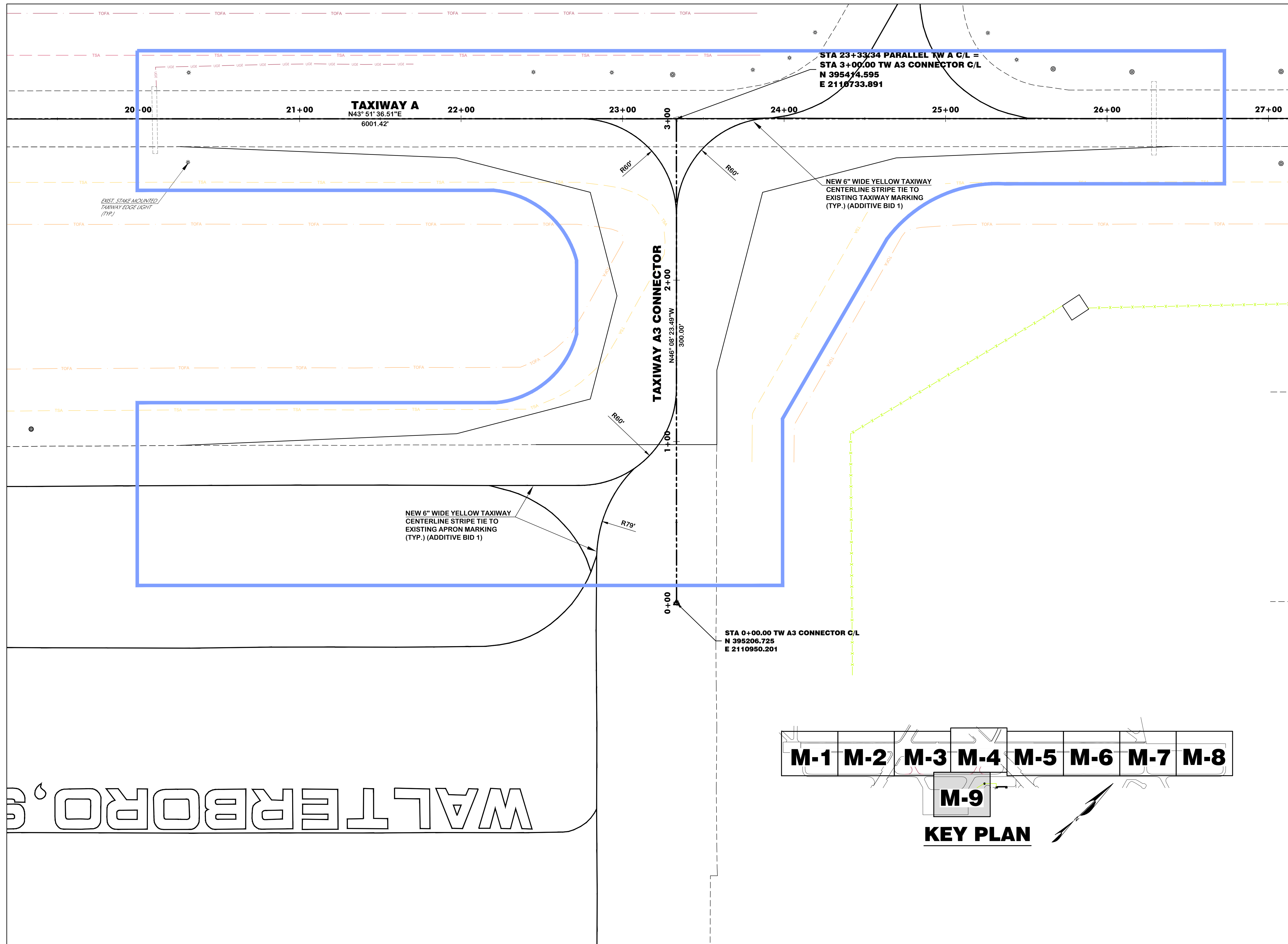
MATCHLINE STA. 58+00, SEE DWG. M-7



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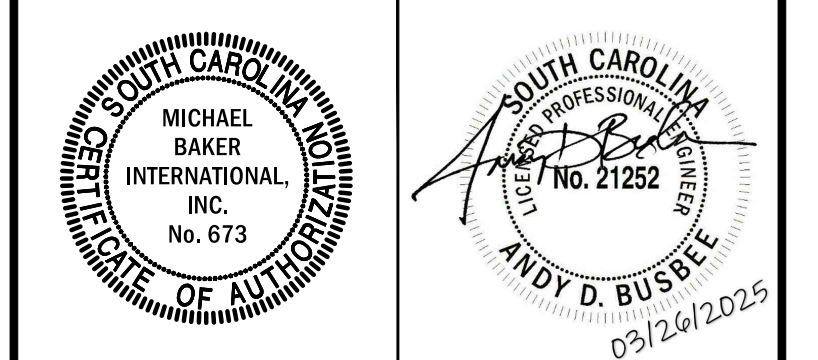
MATCHLINE SEE DWG. M-3

MATCHLINE SEE DWG. M-4

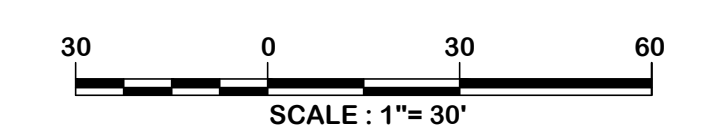


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Designer: BCM / LAW	Checked by: ADB
Technician: LAW	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
MARKING PLAN NO. 9

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"= 30'	Sheet Number: -
Drawing Number: M-9	

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Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: CMP	Project Number: 192930

- NOTES :
- ALL RUNWAY MARKINGS ARE WHITE, ALL TAXIWAY MARKINGS ARE YELLOW, UNLESS OTHERWISE NOTED.
 - RUNWAY CENTERLINE STRIPES SHALL BE 18 INCHES WIDE, 120 FEET LONG WITH 80 FOOT SPACING, UNLESS OTHERWISE NOTED.
 - TAXIWAY CENTERLINE STRIPES SHALL BE 6 INCHES WIDE.
 - ALL MARKINGS ON CONCRETE PAVEMENT SHALL BE OUTLINED WITH 6" WIDE NON-REFLECTORIZED BLACK PAINT.

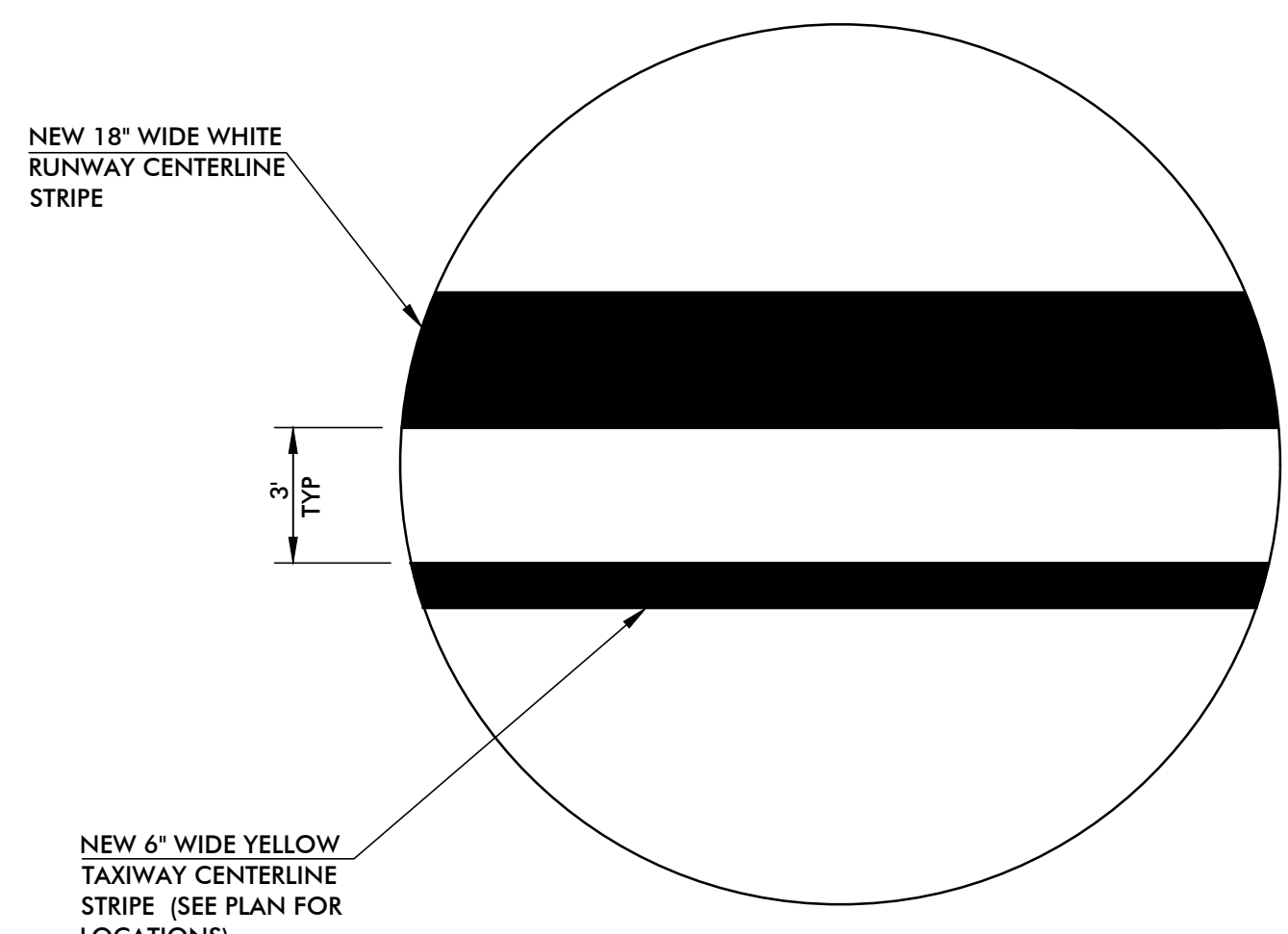
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

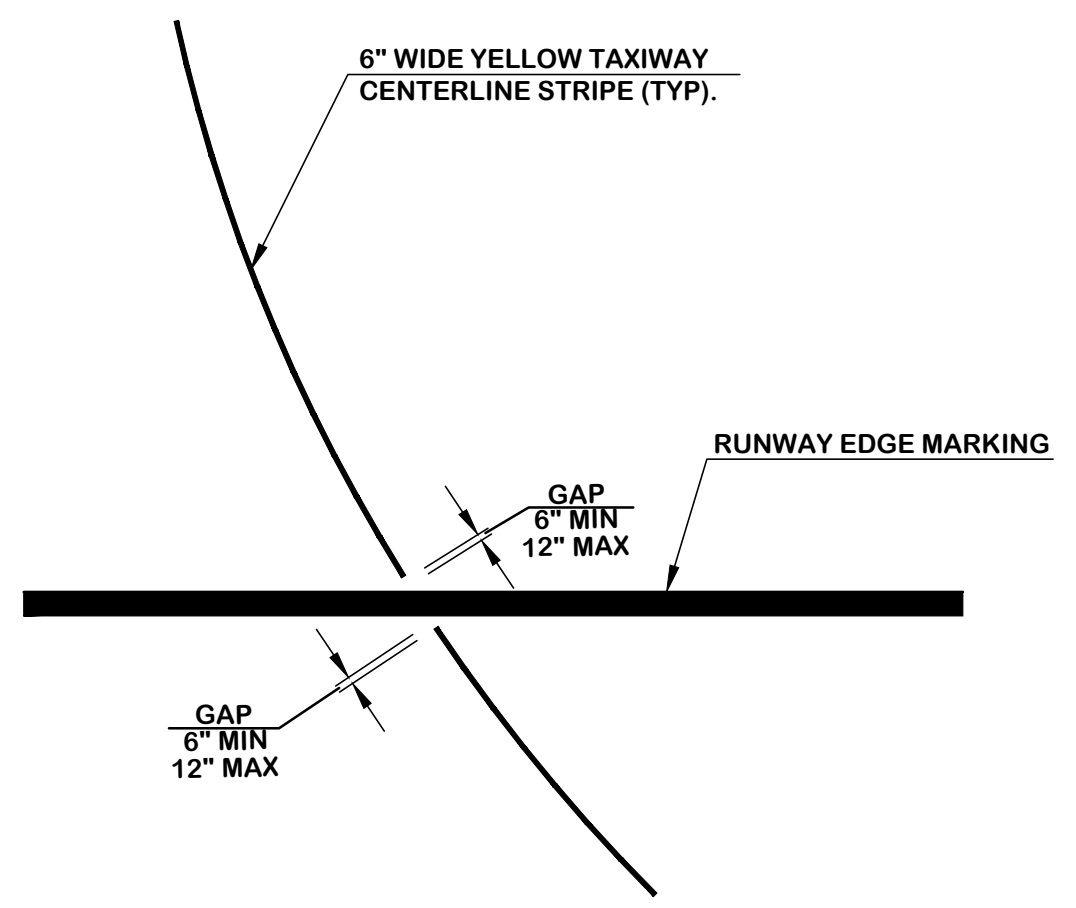
Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
MARKING DETAILS

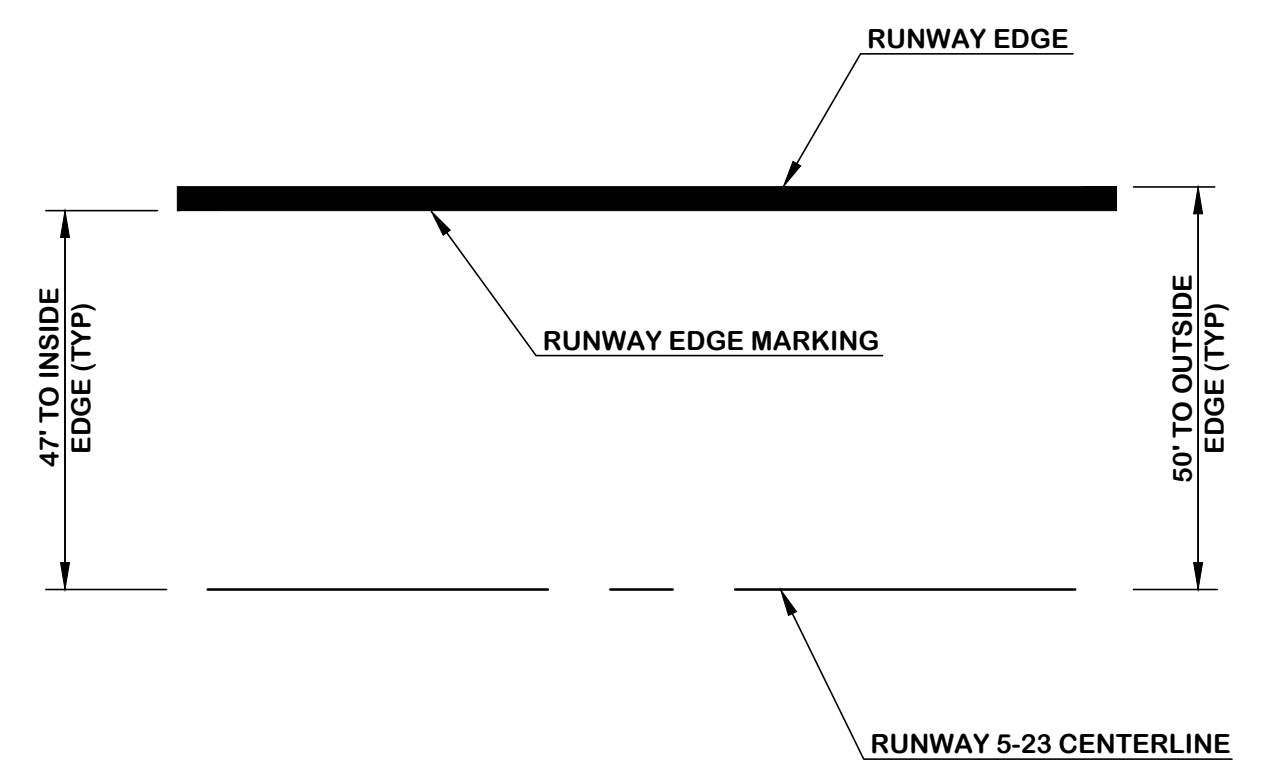
FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: N.T.S.	Sheet Number: MD-1



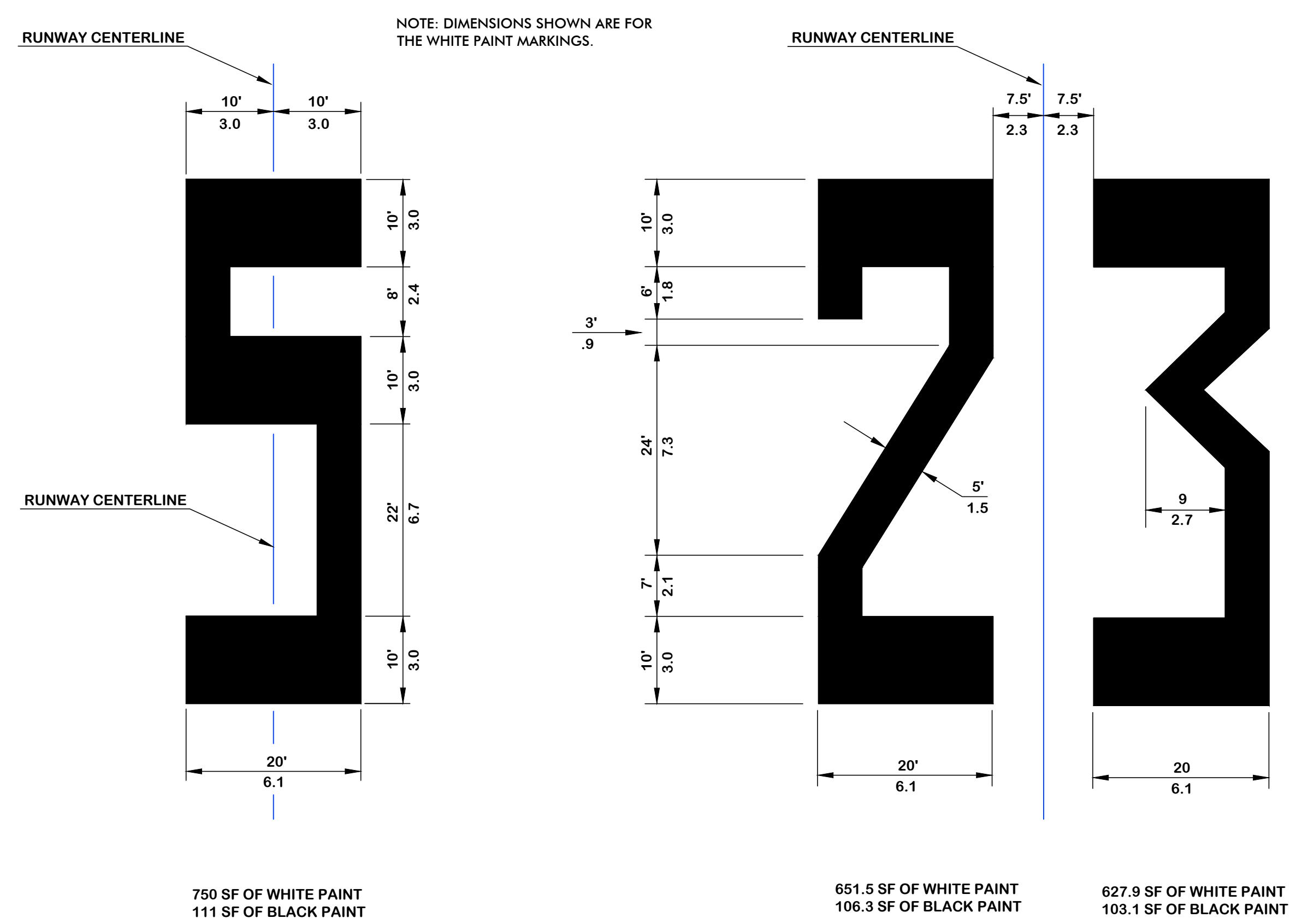
DETAIL 'A'
TAXIWAY CENTERLINE AND RUNWAY CENTERLINE STRIPE DETAIL
N.T.S.



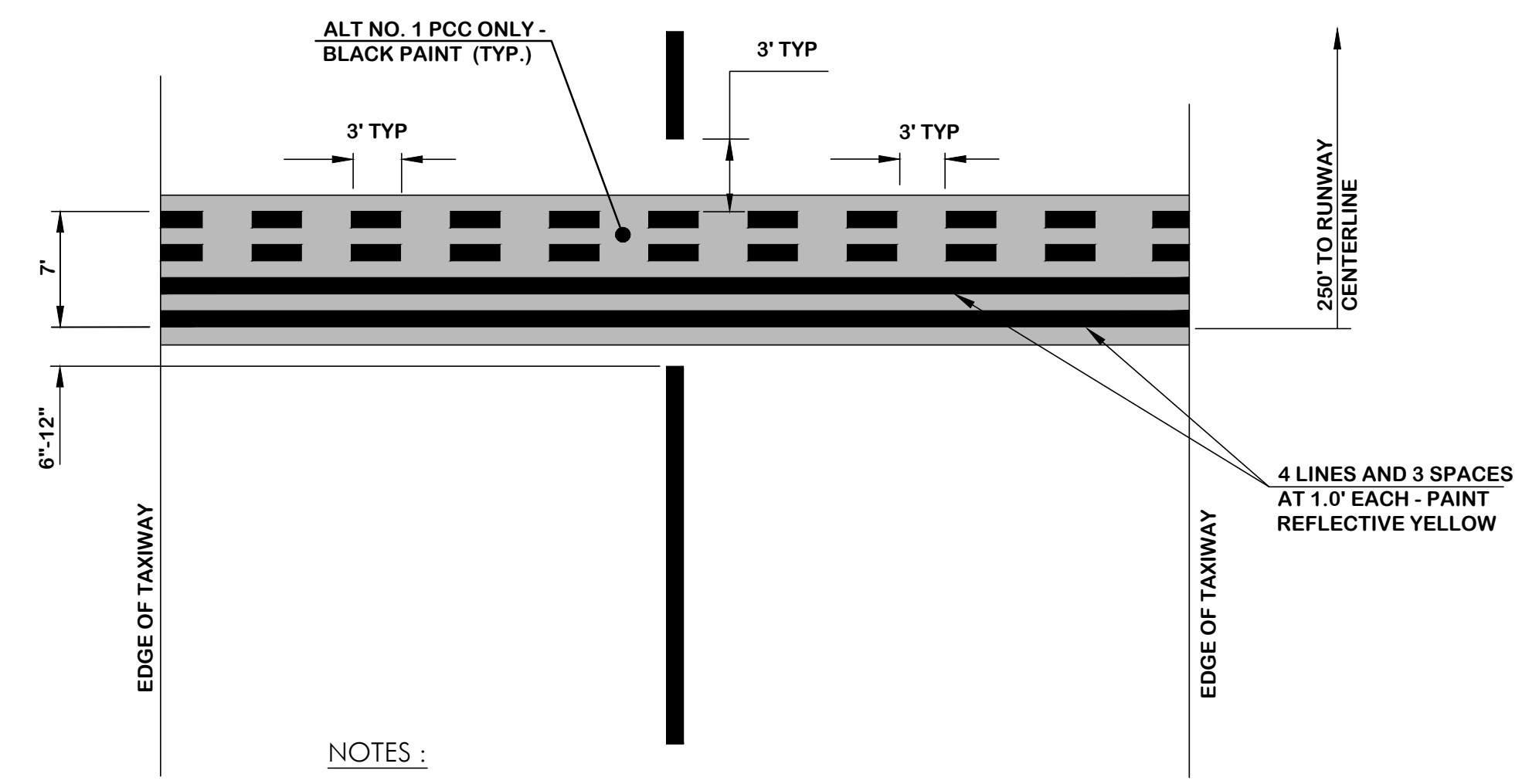
DETAIL 'B'
LINE SKIP DETAIL
N.T.S.



DETAIL 'C' RUNWAY EDGE STRIPE DETAIL
N.T.S.



RUNWAY NUMERALS
N.T.S.



- NOTES :
- DIMENSION SHOWN ARE FOR THE YELLOW PAINT MARKINGS.

RUNWAY HOLDING POSITION MARKING DETAIL
N.T.S.

- NOTES :
- DIMENSION SHOWN ARE FOR THE WHITE PAINT MARKINGS.
 - ALL NUMERALS ARE HORIZONTALLY SPACED 15 FEET (4.6 METERS) APART.
 - DIMENSIONS ARE EXPRESSED THUS: $\frac{\text{FEET}}{\text{METERS}}$ e.g. $\frac{30'}{9}$

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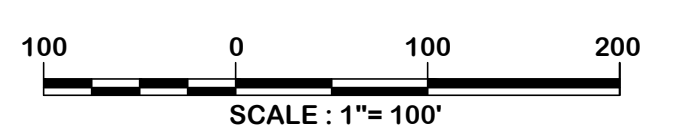
WALTERBORO, SOUTH CAROLINA

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700 Huger Street
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Designer: BCM / LAW
Checked by: ADB
Technician: LAW
Project Number: 192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

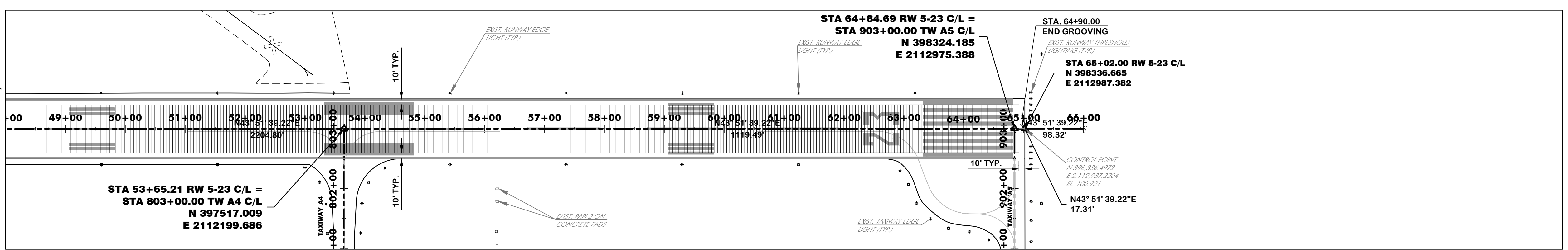
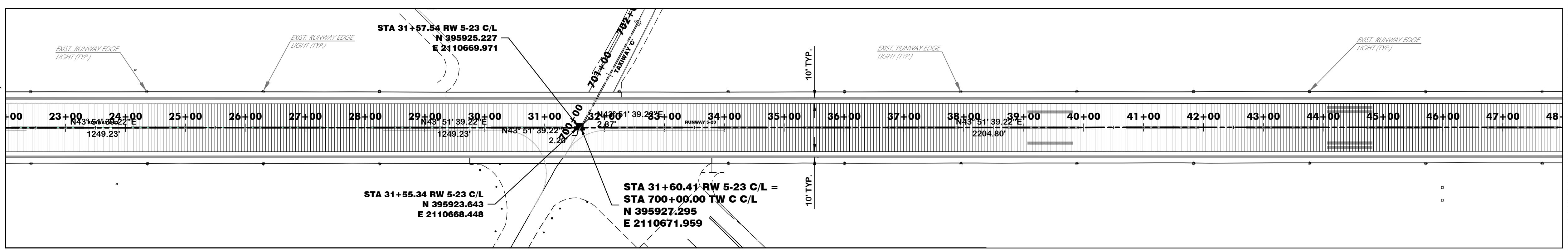
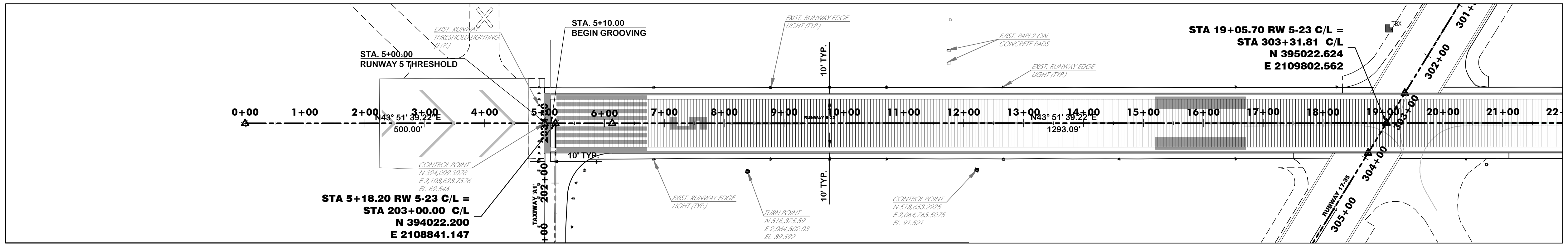
Drawing Name:
GROOVING PLAN

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: Sheet Number:

1" = 100'
Drawing Number:
GP-1



GROOVING DETAILS AND NOTES

GROOVING NOTES:

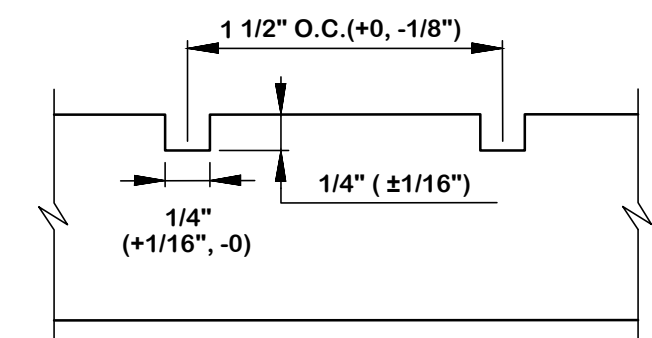
SAW-CUT GROOVES. For pavements, transverse grooves shall be saw cut in the pavement forming a 1/4 inch wide by 1/4 inch deep by 1-1/2 inches center to center configuration. They shall be saw-cut transversely in the runway pavement to within 10 feet of the runway pavement edge to allow adequate space for equipment operation. The maximum transverse saw cut grooves shall not exceed 130 feet. The tolerances for the saw cut grooves shall meet the following:

- Alignment tolerance.**
Plus or minus 1-1/2 inches in alignment for 130 feet and shall be perpendicular to the runway centerline.
- Groove tolerance.**
Minimum depth 3/16 inch, except that not more than 60 percent of the grooves shall be less than 1/4 inch.
Maximum depth 5/16 inch.
Minimum width 1/4 inch.
Maximum width 5/16 inch.
Center to center spacing
Minimum spacing 1-3/8 inches
Maximum spacing 1-1/2 inches.

Cleanup of waste material shall be continuous during the grooving operation. Waste material shall be disposed of in an approved manner. Waste material shall not be allowed to enter the airport storm or sanitary sewer system.

LEGEND

SAW-CUT GROOVES (86,407 SY)



TYPICAL GROOVE PATTERN

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SPECIFICATION	SIZE
ROCK PAD THICKNESS	6 INCHES
ROCK PAD WIDTH	24 FEET
ROCK PAD LENGTH	100 FEET
ROCK PAD STONE SIZE	D = 2-3 INCHES

South Carolina Department of Health and Environmental Control
CONSTRUCTION ENTRANCE
 STANDARD DRAWING NO. SC-06 PAGE 1 of 2
 FEBRUARY 2014 DATE
 NOT TO SCALE

SILT FENCE INSTALLATION

FLAT-BOTTOM TRENCH DETAIL

V-SHAPED TRENCH DETAIL

SILT FENCE - GENERAL NOTES

- Do not place silt fence across channels or in other areas subject to concentrated flows. Silt fence should not be used as a velocity control BMP. Concentrated flows are any flows greater than 0.5 cfs.
- Maximum sheet or overland flow path length to the silt fence shall be 100 feet.
- Maximum slope steepness [normal (perpendicular) to the fence line] shall be 2:1.
- Silt fence joints, when necessary, shall be completed by one of the following options:
 - Wrap each fabric together at a support post with both ends fastened to the post, with a 1-foot overlap silt fence by installing 3-foot spaced support post to which the new silt fence roll is rolled to new roll with heavy-duty plastic ties, or
 - Overlap entire width of each silt fence roll from one support post to the next support post.
- Attach filter fabric to the steel posts using heavy-duty plastic ties that are evenly spaced within the top 8-inches of the fabric.
- Install the silt fence perpendicular to the direction of the stormwater flow and place the silt fence the proper distance from the toe of steep slopes to provide sediment storage and access for maintenance and cleanup.
- Install Silt Fence Checks (Tie-Backs) every 50-100 feet, dependent on slope, along silt fence that is installed with slope and where concentrated flows are expected or are documented along the proposed installed silt fence.

South Carolina Department of Health and Environmental Control
SILT FENCE
 STANDARD DRAWING NO. SC-03 Page 1 of 2
 FEBRUARY 2014 DATE
 NOT TO SCALE

SEDIMENT TUBE INSTALLATION

SEDIMENT TUBE SPACING

SLOPE	MAX. SEDIMENT TUBE SPACING
LESS THAN 2%	150-FEET
2%	100-FEET
3%	75-FEET
4%	50-FEET
5%	40-FEET
6%	30-FEET
GREATER THAN 6%	25-FEET

South Carolina Department of Health and Environmental Control
SEDIMENT TUBES
 STANDARD DRAWING NO. SC-05 PAGE 1 of 2
 FEBRUARY 2014 DATE
 NOT TO SCALE

CONSTRUCTION ENTRANCE - GENERAL NOTES

- Stabilized construction entrances should be used at all points where traffic will egress/ingress a construction site onto a public road or any impervious surfaces, such as parking lots.
- Install a non-woven geotextile fabric prior to placing any stone.
- Install a culvert pipe across the entrance when needed to provide positive drainage.
- The entrance shall consist of 2-inch to 3-inch D50 stone placed at a minimum depth of 6-inches.
- Minimum dimensions of the entrance shall be 24 feet wide by 100 feet long, and may be modified as necessary to accommodate site constraints.
- The edges of the entrance shall be tapered out towards the road to prevent tracking at the edge of the entrance.
- Divert all surface runoff and drainage from the stone pad to a sediment trap or basin or other sediment trapping structure.
- Limestone may not be used for the stone pad.

CONSTR. ENTRANCE - INSPECTION & MAINTENANCE

- The key to functional construction entrances is weekly inspections, routine maintenance, and regular sediment removal.
- Regular inspections of construction entrances shall be conducted once every calendar week and, as recommended, within 24-hours after each rainfall event that produces 1/2-inch or more of precipitation.
- During regular inspections, check for mud and sediment buildup and pad integrity. Inspection frequencies may need to be more frequent during long periods of wet weather.
- Reshape the stone pad as necessary for drainage and runoff control.
- Wash or replace stones as needed and as directed by site inspector. The stone in the entrance should be washed or replaced whenever the entrance fails to reduce the amount of mud being carried off-site by vehicles. Frequent washing will extend the useful life of stone pad.
- Immediately remove mud and sediment tracked or washed onto adjacent impervious surfaces by brushing or sweeping. Flushing should only be used when the water can be discharged to a sediment trap or basin.
- During maintenance activities, any broken pavement should be repaired immediately.
- Construction entrances should be removed after the site has reached final stabilization. Permanent vegetation should replace areas from which construction entrances have been removed, unless area will be converted to an impervious surface to serve post-construction.

South Carolina Department of Health and Environmental Control
CONSTRUCTION ENTRANCE
 STANDARD DRAWING NO. SC-06 PAGE 2 of 2
 FEBRUARY 2014 DATE
 GENERAL NOTES

SILT FENCE - POST REQUIREMENTS

- Silt fence posts must be 48-inch long steel posts that meet, at a minimum, the following physical characteristics:
 - Composed of a high strength steel with a minimum yield strength of 50,000 psi and a nominal "T" length of 1.48-inches.
 - Weight 1.25 pounds per foot (± 8%).
- Posts shall be equipped with projections to aid in fastening of filter fabric.
- Steel posts may need to have a metal soil stabilization plate welded near the bottom when installed along steep slopes or installed in loose soils. The plate should have a minimum cross section of 17 square inches and be composed of 1/2 gauge steel, at a minimum. The metal soil stabilization plate should be completely buried.
- Install posts to a minimum of 24-inches. A minimum height of 1- to 2- inches above the fabric shall be maintained, and a maximum height of 3 feet shall be maintained above the ground.
- Post spacing shall be at a maximum of 6-feet on center.

SILT FENCE - FABRIC REQUIREMENTS

- Silt fence must be composed of woven geotextile filter fabric that consists of the following requirements:
 - Composed of fibers consisting of long chain synthetic polymers of at least 85% by weight of polypropylene, polyester, or polyamides that are formed into a network such that the filaments or yarns retain dimensional stability relative to each other.
 - Free of any treatment or coating which might adversely alter its physical properties after installation.
 - Free of any defects or flaws that significantly affect its physical and/or filtering properties, and
 - Have a minimum width of 36-inches.
- Use only fabric appearing on SC DOT's Qualified Products Listing (QPL), Approval Sheet #34, meeting the requirements of the most current edition of the SC DOT Standard Specifications for Highway Construction.
- 12-inches of the fabric should be placed within excavated trench and holed in when the trench is backfilled.
- Filter fabric shall be purchased in continuous rolls and cut to the length of the barrier to avoid joints.
- Filter fabric shall be installed at a minimum of 24-inches above the ground.

SILT FENCE - INSPECTION & MAINTENANCE

- The key to functional silt fence is weekly inspections, routine maintenance, and regular sediment removal.
- Regular inspections of silt fence shall be conducted once every calendar week and, as recommended, within 24-hours after each rainfall event that produces 1/2-inch or more of precipitation.
- Attention to sediment accumulations along the silt fence is extremely important. Accumulated sediment should be continuously monitored and removed when necessary.
- Remove accumulated sediment when it reaches 1/3 the height of the silt fence.
- Removed sediment shall be placed in stockpile storage areas or spread thinly across disturbed area. Stabilize the removed sediment after it is relocated.
- Check for areas where stormwater runoff has eroded a channel beneath the silt fence, or where the fence has sagged or collapsed due to runoff overtopping the silt fence. Install checks/tie-backs and/or reinstall silt fence, as necessary.
- Check for holes within the silt fence, areas where silt fence has begun to deteriorate, and for any other circumstance that may render the silt fence ineffective. Remove damaged silt fence and reinstall new silt fence immediately.
- Silt fence should be removed within 30 days after final stabilization is achieved and once it is removed, the resulting disturbed area shall be permanently stabilized.

South Carolina Department of Health and Environmental Control
SILT FENCE
 STANDARD DRAWING NO. SC-03 PAGE 2 of 2
 FEBRUARY 2014 DATE
 GENERAL NOTES

SEDIMENT TUBES - GENERAL NOTES

- Sediment tubes may be installed along contours, in drainage conveyance channels, and around inlets to help prevent off-site discharge of sediment-laden stormwater runoff.
- Sediment tubes are elongated tubes of compacted geotextiles, curled excelsior wood, natural coconut fiber, or hardwood mulch. Straw, pine needles, and leaf mulch-filled sediment tubes are not permitted.
- The outer netting of the sediment tube should consist of seamless, high-density polyethylene photodegradable materials treated with ultraviolet stabilizers or a seamless, high-density polyethylene non-degradable material.
- Sediment tubes, when used as checks within channels, should range between 18-inches and 24-inches depending on channel dimensions. Diameters outside this range may be allowed where necessary when approved.
- Curled excelsior wood, or natural coconut products that are rolled up to create a sediment tube are not allowed.
- Sediment tubes should be staked using wooden stakes (2-inch x 2-inch) or steel posts (standard "U" or "T" sections with a minimum weight of 1.25 pounds per foot) at a minimum of 48-inches in length placed on 2-foot centers.
- Install all sediment tubes to ensure that no gaps exist between the soil and the bottom of the tube. Manufacturer's recommendations should always be consulted before installation.
- The ends of adjacent sediment tubes should be overlapped 6-inches to prevent flow and sediment from passing through the field joint.
- Sediment tubes should not be stacked on top of one another, unless recommended by manufacturer.
- Each sediment tube should be installed in a trench with a depth equal to 1/5 the diameter of the sediment tube.
- Sediment tubes should continue up the side slopes a minimum of 1-foot above the design flow depth of the channel.
- Install stakes at a diagonal facing incoming runoff.

SEDIMENT TUBES - INSPECTION & MAINTENANCE

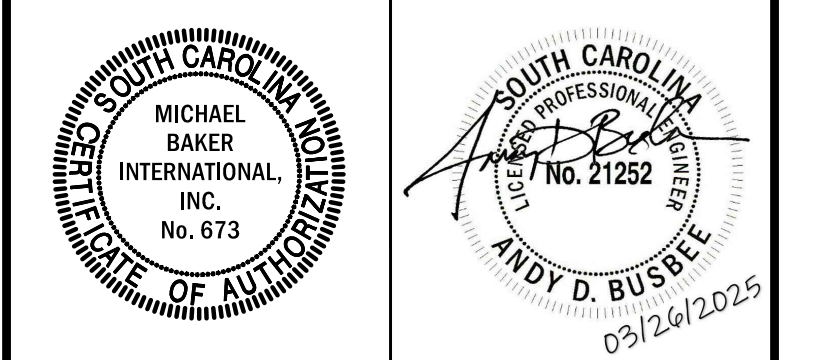
- The key to functional sediment tubes is weekly inspections, routine maintenance, and regular sediment removal.
- Regular inspections of sediment tubes shall be conducted once every calendar week and, as recommended, within 24-hours after each rainfall event that produces 1/2-inch or more of precipitation.
- Attention to sediment accumulations in front of the sediment tube is extremely important. Accumulated sediment should be continuously monitored and removed when necessary.
- Remove accumulated sediment when it reaches 1/3 the height of the sediment tube.
- Removed sediment shall be placed in stockpile storage areas or spread thinly across disturbed area. Stabilize the removed sediment after it is relocated.
- Large debris, trash, and leaves should be removed from in front of tubes when found.
- If erosion causes the edges to fall to a height equal to or below the height of the sediment tube, repairs should be made immediately to prevent runoff from bypassing tube.
- Sediment tubes should be removed after the contributing drainage area has been completely stabilized. Permanent vegetation should replace areas from which sediment tubes have been removed.

South Carolina Department of Health and Environmental Control
SEDIMENT TUBES
 STANDARD DRAWING NO. SC-05 PAGE 2 of 2
 FEBRUARY 2014 DATE
 GENERAL NOTES



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Designer: ADB/NWM	Checked by: ADB
Technician: MTW	Project Number: 192930

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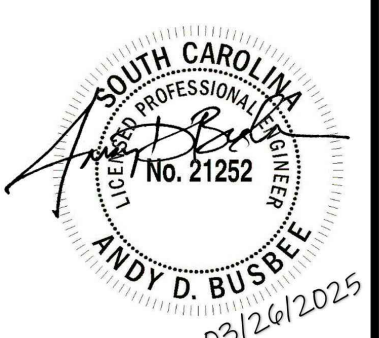
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
EROSION CONTROL DETAILS

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: AIRPORTS
Scale: N.T.S.	Sheet Number: EC-1



Designer: ADB/NWM Checked by: ADB

Technician: MTW Project Number: 192930

ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:

**RUNWAY 5-23
REHABILITATION**

Drawing Name:

**EROSION CONTROL
DETAILS**

FAA A.I.P. Project Number:

3-45-0057-025-2025

Date: MARCH, 2025 Division: AIRPORTS

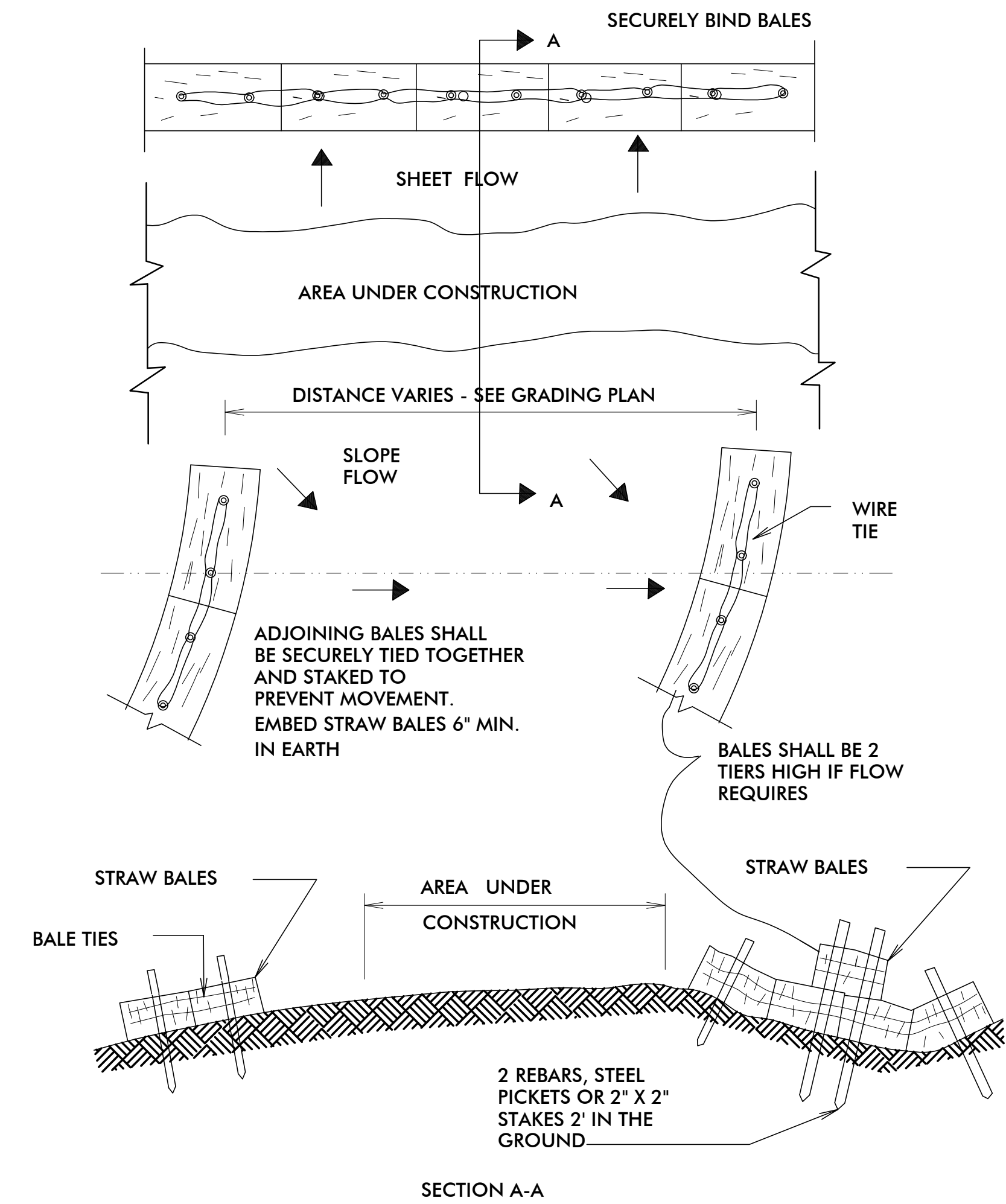
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Drawing Number:

EC-2

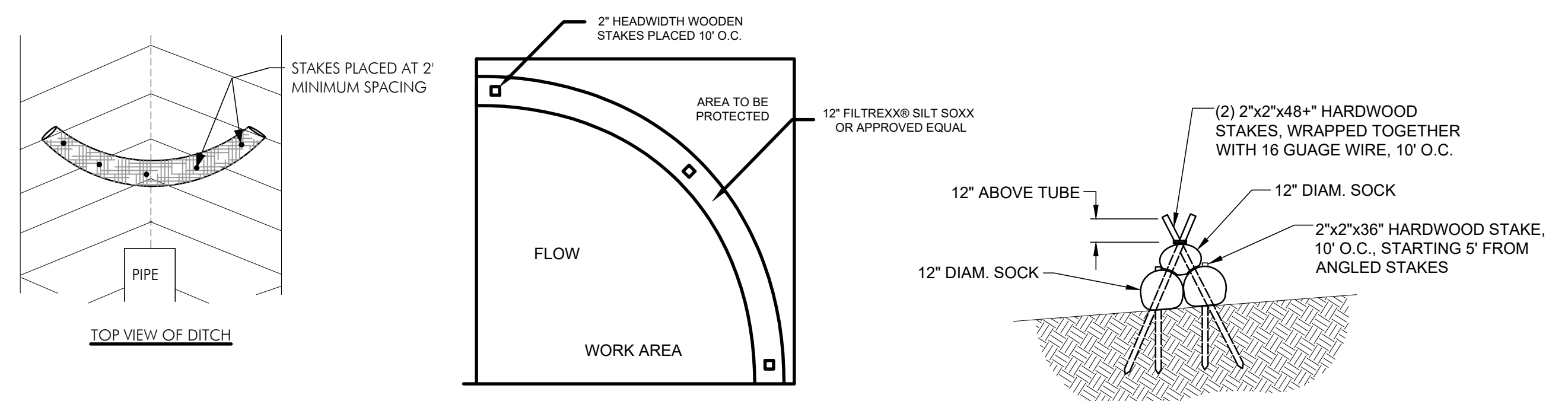
SCDHEC STANDARD NOTES

- IF NECESSARY, SLOPES, WHICH EXCEED EIGHT (8) VERTICAL FEET SHOULD BE STABILIZED WITH SYNTHETIC OR VEGETATIVE MATS, IN ADDITION TO HYDROSEEDING. IT MAY BE NECESSARY TO INSTALL TEMPORARY SLOPE DRAINS DURING CONSTRUCTION. TEMPORARY BERMS MAY BE NEEDED UNTIL THE SLOPE IS BROUGHT TO GRADE.
- STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN FOURTEEN (14) DAYS AFTER WORK HAS CEASED, EXCEPT AS STATED BELOW.
 - WHERE STABILIZATION BY THE 14TH DAY IS PRECLUDED BY SNOW COVER OR FROZEN GROUND CONDITIONS STABILIZATION MEASURES MUST BE INITIATED AS SOON AS PRACTICABLE.
 - WHERE CONSTRUCTION ACTIVITY ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH-DISTURBING ACTIVITIES WILL BE RESUMED WITHIN 14 DAYS, TEMPORARY STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE.
- ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED ONCE EVERY CALENDAR WEEK. IF PERIODIC INSPECTION OR OTHER INFORMATION INDICATES THAT A BMP HAS BEEN INAPPROPRIATELY, OR INCORRECTLY, THE PERMITTEE MUST ADDRESS THE NECESSARY REPLACEMENT OR MODIFICATION REQUIRED TO CORRECT THE BMP WITHIN 48 HOURS OF IDENTIFICATION.
- PROVIDE SILT FENCE AND/OR OTHER CONTROL DEVICES, AS MAY BE REQUIRED, TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEANED, GRADED, AND STABILIZED WITH GRASSING IMMEDIATELY AFTER THE UTILITY INSTALLATION. FILL, COVER, AND TEMPORARY SEEDING AT THE END OF EACH DAY ARE RECOMMENDED. IF WATER IS ENCOUNTERED WHILE TRENCHING, THE WATER SHOULD BE FILTERED TO REMOVE SEDIMENT BEFORE BEING PUMPED BACK INTO ANY WATERS OF THE STATE.
- ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFFSITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
- THE CONTRACTOR MUST TAKE NECESSARY ACTION TO MINIMIZE THE TRACKING OF MUD ONTO PAVED ROADWAY(S) FROM CONSTRUCTION AREAS AND THE GENERATION OF DUST. THE CONTRACTOR SHALL DAILY REMOVE MUD/SOIL FROM PAVEMENT, AS MAY BE REQUIRED.
- RESIDENTIAL SUBDIVISIONS REQUIRE EROSION CONTROL FEATURES FOR INFRASTRUCTURE AS WELL AS FOR INDIVIDUAL LOT CONSTRUCTION. INDIVIDUAL PROPERTY OWNERS SHALL FOLLOW THESE PLANS DURING CONSTRUCTION OR OBTAIN APPROVAL OF AN INDIVIDUAL PLAN IN ACCORDANCE WITH S.C REG. 72-300 ET SEQ. AND SCR100000.
- TEMPORARY DIVERSION BERMS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UPSLOPE RUNOFF AND/OR TO DIVERT SEDIMENT-LADEN WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
- ALL WATERS OF THE STATE (WOS), INCLUDING WETLANDS, ARE TO BE FLAGGED OR OTHERWISE CLEARLY MARKED IN THE FIELD. A DOUBLE ROW OF SILT FENCE IS TO BE INSTALLED IN ALL AREAS WHERE A 50-FOOT BUFFER CAN'T BE MAINTAINED BETWEEN THE DISTURBED AREA AND ALL WOS. A 10-FOOT BUFFER SHOULD BE MAINTAINED BETWEEN THE LAST ROW OF SILT FENCE AND ALL WOS.
- LITTER, CONSTRUCTION DEBRIS, OILS, FUELS, AND BUILDING PRODUCTS WITH SIGNIFICANT POTENTIAL FOR IMPACT (SUCH AS STOCKPILES OF FRESHLY TREATED LUMBER) AND CONSTRUCTION CHEMICALS THAT COULD BE EXPOSED TO STORM WATER MUST BE PREVENTED FROM BECOMING A POLLUTANT SOURCE IN STORM WATER DISCHARGES.
- A COPY OF THE SWPPP, INSPECTIONS RECORDS, AND RAINFALL DATA MUST BE RETAINED AT THE CONSTRUCTION SITE OR A NEARBY LOCATION EASILY ACCESSIBLE DURING NORMAL BUSINESS HOURS, FROM THE DATE OF COMMENCEMENT OF CONSTRUCTION ACTIVITIES TO THE DATE THAT FINAL STABILIZATION IS REACHED.
- INITIATE STABILIZATION MEASURES ON ANY EXPOSED STEEP SLOPE (3H:1V OR GREATER) WHERE LAND-DISTURBING ACTIVITIES HAVE PERMANENTLY OR TEMPORARILY CEASED, AND WILL NOT RESUME FOR A PERIOD OF 7 CALENDAR DAYS.
- MINIMIZE SOIL COMPACTION AND, UNLESS INFEASIBLE, PRESERVE TOPSOIL.
- MINIMIZE THE DISCHARGE OF POLLUTANTS FROM EQUIPMENT AND VEHICLE WASHING. WHEEL WASH WATER, AND OTHER WASH WATERS. WASH WATERS MUST BE TREATED IN A SEDIMENT BASIN OR ALTERNATIVE CONTROL THAT PROVIDES EQUIVALENT OR BETTER TREATMENT PRIOR TO DISCHARGE;
- MINIMIZE THE DISCHARGE OF POLLUTANTS FROM DEWATERING OF TRENCHES AND EXCAVATED AREAS. THESE DISCHARGES ARE TO BE ROUTED THROUGH APPROPRIATE BMPS (SEDIMENT BASIN, FILTER BAG, ETC.).
- THE FOLLOWING DISCHARGES FROM SITES ARE PROHIBITED:
 - WASTEWATER FROM WASHOUT OF CONCRETE, UNLESS MANAGED BY AN APPROPRIATE CONTROL;
 - WASTEWATER FROM WASHOUT AND CLEANOUT OF STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS;
 - FUELS, OILS, OR OTHER POLLUTANTS USED IN VEHICLE AND EQUIPMENT OPERATION AND MAINTENANCE; AND
 - SOAPS OR SOLVENTS USED IN VEHICLE AND EQUIPMENT WASHING.
- AFTER CONSTRUCTION ACTIVITIES BEGIN, INSPECTIONS MUST BE CONDUCTED AT A MINIMUM OF AT LEAST ONCE EVERY CALENDAR WEEK AND MUST BE CONDUCTED UNTIL FINAL STABILIZATION IS REACHED ON ALL AREAS OF THE CONSTRUCTION SITE.
- IF EXISTING BMPS NEED TO BE MODIFIED OR IF ADDITIONAL BMPS ARE NECESSARY TO COMPLY WITH THE REQUIREMENTS OF THIS PERMIT AND/OR SC'S WATER QUALITY STANDARDS, IMPLEMENTATION MUST BE COMPLETED BEFORE THE NEXT STORM EVENT WHENEVER PRACTICABLE. IF IMPLEMENTATION BEFORE THE NEXT STORM EVENT IS IMPRACTICABLE, THE SITUATION MUST BE DOCUMENTED IN THE SWPPP AND ALTERNATIVE BMPS MUST BE IMPLEMENTED AS SOON AS REASONABLY POSSIBLE.
- A PRE-CONSTRUCTION CONFERENCE MUST BE HELD FOR EACH CONSTRUCTION SITE WITH AN APPROVED ON-SITE SWPPP PRIOR TO THE IMPLEMENTATION OF CONSTRUCTION ACTIVITIES. FOR NON-LINEAR PROJECTS THAT DISTURB 10 ACRES OR MORE THIS CONFERENCE MUST BE HELD ON-SITE UNLESS THE DEPARTMENT HAS APPROVED OTHERWISE.

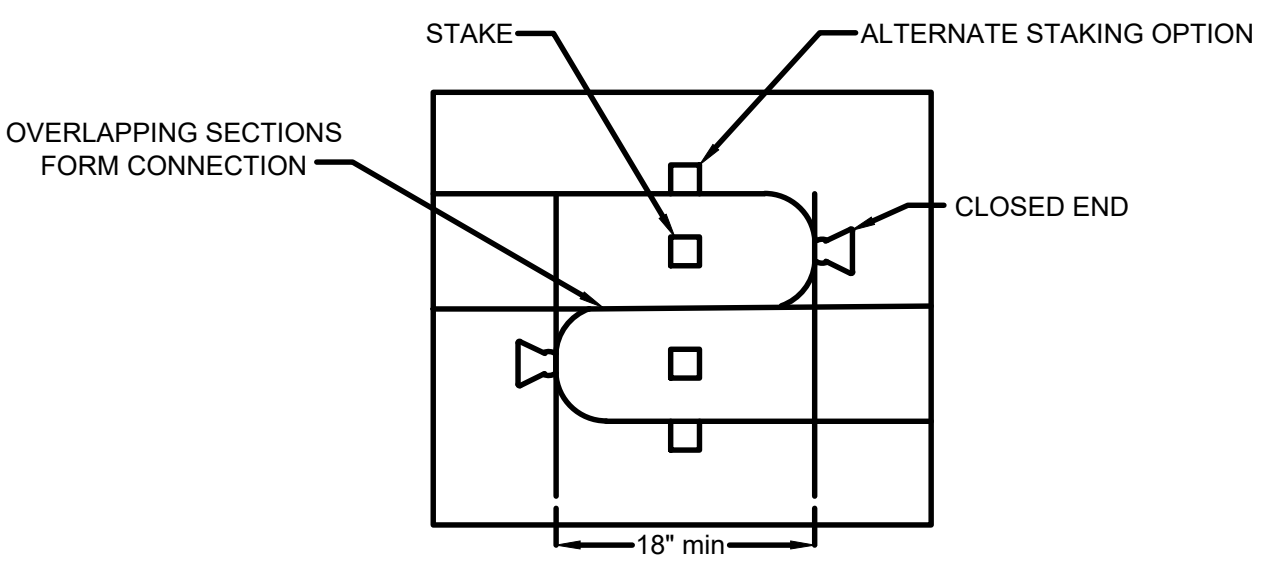


STRAW BALE SEDIMENT BARRIER/CHECK DAM

N.T.S.



COMPOST SOCK CONNECTION/ATTACHMENT DETAIL



INSPECTION AND MAINTENANCE:

- THE KEY TO FUNCTIONAL SEDIMENT DAM IS WEEKLY INSPECTION, ROUTINE MAINTENANCE, AND REGULAR SEDIMENT REMOVAL.
- ATTENTION TO SEDIMENT ACCUMULATION WITHIN THE SEDIMENT DAM IS EXTREMELY IMPORTANT. ACCUMULATED SEDIMENT SHOULD BE CONTINUALLY MONITORED IN THE DAM AND REMOVED WHEN NECESSARY.
- REMOVE ACCUMULATED SEDIMENT WHEN IT REACHES 50% OF THE DESIGNED SEDIMENT STORAGE VOLUME AS MARKED ON THE CLEANOUT STAKE.
- REMOVED SEDIMENT SHALL BE PLACED IN STOCKPILE STORAGE AREAS OR SPREAD THINLY ACROSS DISTURBED AREA. STABILIZE THE REMOVED SEDIMENT AFTER IT IS RELOCATED.
- REGULAR INSPECTIONS OF SEDIMENT DAMS SHOULD BE CONDUCTED ONCE EVERY CALENDAR WEEK AND, AS RECOMMENDED, WITHIN 24 HOURS AFTER EACH RAINFALL EVENT THAT PRODUCES 1/2 INCH OR MORE OF PRECIPITATION.
- DISTURBED AREAS RESULTING FROM THE REMOVAL OF THE SEDIMENT DAM SHOULD BE PERMANENTLY STABILIZED AND ADDITIONAL BMPS, SUCH AS SILT FENCE, SHOULD BE UTILIZED TO HANDLE STORMWATER RUNOFF FROM THIS DISTURBED AREA UNTIL FINAL STABILIZATION IS REACHED.
- TEMPORARY COMPOST FILTER SOCK SEDIMENT DAMS SHALL BE PAID PER EACH UNDER ITEM C-102-3.

4
25

TEMPORARY COMPOST FILTER SOCK SEDIMENT DAM
NOT TO SCALE

Z:\Michael\CLIENT\Walterboro lowcountry\gmi\2025\w-5-23 rehab\rehab\02-drawings\RBV-RVIS-04-EC DETAIL.dwg Modified: Mar 06, 2025 - 4:29pm Plotter: Mar 26, 2025 - 4:58pm Lauren Watson

BASE BID SEEDING SCHEDULE

ITEM T-901

GRASSING AND MULCHING

GENERAL

901-1.1 DESCRIPTION: Work described in this section includes permanent grassing by either seeding and hydroseeding of grass over areas indicated in the drawings and in areas disturbed within the Contractor's staging area, haul routes, and all other areas outside the limits of construction.

901-1.2 DEFINITIONS: Satisfactory stand of grass: Full cover over designated areas, with living grass free of weeds and without bare spots.

901-1.3 SUBMITTALS: Certificates: Submit certificates for all seed. Indicate grass species, weed content, and percent germination.

901-1.4 QUALITY ASSURANCE:

A. These specifications are to be used as a guide to enable the Contractor to develop a satisfactory stand of grass. The Contractor shall visit the site and acquaint himself as to the nature and condition of the soil and the extent of the work required prior to bidding.

B. Seed shall be furnished separately or in mixtures in standard containers with the seed name, lot number, net weight, percentages of purity and of germination and hard seed, and percentage of maximum weed seed content clearly marked for each kind of seed.

MATERIALS

901-2.1 FERTILIZER:

- 1. Regular type: Nitrogen content derived from organic or inorganic sources; bearing manufacturer's statement of analysis. Minimum requirements: 12% nitrogen, 8% phosphoric acid, 8% potash.
2. Slow release type: 50% of nitrogen is in slow release form, content derived from organic or inorganic sources; bearing manufacturer's statement of analysis. Minimum requirements: 12% nitrogen, 4% phosphoric acid, 10% potash.
3. Commercial mixed type: Nitrogen content derived from organic or inorganic sources, bearing manufacturer's statement of analysis. Minimum requirements: 10% nitrogen, 10% phosphoric acid, 10% potash.

901-2.2 LIME: Ground limestone containing not less than 85% carbonates; 50% passing 100 mesh sieve and 90% passing 20 mesh sieve. Coarser material is acceptable if application rates are increased to achieve quantities and depth specified.

901-2.3 SEED: The grass seed to be furnished shall have a maximum of two and one-half percent (2-1/2%) of weed seed and a minimum of ninety percent (90%) by weight of pure live seed. The seed shall conform to the requirements of Federal Specification JJJ-S-181.

SEEDING SCHEDULE: For all disturbed areas.

Table with 4 columns: Common Name of Seed, Pounds Per Acre, Total Seed Mixture, Planting Dates. Includes Bermuda Grass (Hulled) and Kobe Lespedeza.

Table with 4 columns: Common Name of Seed, Pounds Per Acre, Total Seed Mixture, Planting Dates. Includes Bermuda Grass (50% Hulled, 50% Unhulled), Kobe Lespedeza, Kentucky 31 Fescue, Annual Ryegrass*, and Rye Grain*.

*These grasses are not to be used by themselves, but should be used in conjunction with one of the Bermuda grasses specified.

901-2.4 HYDROMULCH: Wood cellulose fiber containing no germination inhibiting or growth inhibiting agents. Characteristics shall be as follows:

- 1. Percent moisture content: 10.0% (+ 2.0)
2. Percent organic matter: 99.4% (+ 0.2%).
3. Percent ash content: 0.6% (+ 0.2%).
4. pH: 4.8 (+ 0.5%).
5. Water holding capacity: 1050 grams water/100 grams fiber, minimum.

901-2.5 DRY MULCH: Clean, seed free stray of hay, wheat, rye, oats, barley, straw or manure.

901-2.6 ASPHALT SPRAY MULCH: Meeting ASTM D 977-79a, Grade SS-1 with 57%-59% residual asphalt, mixed with equal parts of water.

901-2.7 STAKING PEGS: Staking pegs: 3/4" diameter by 8" long softwood.

901-2.8 WATER: Clean, potable.

CONSTRUCTION METHODS

901-3.1 PREPARATION:

- A. Place topsoil as specified in Section T-905 on area to be grassed.
B. Check soil pH. Add lime as required to achieve a soil pH of at least 5.5 but at a minimum of 1.5 tons/acre. Apply lime and mix into soil during seedbed preparation. Apply fertilizer to seedbed at rate of 1000 lbs. per acre or equivalent of 10_10_10 fertilizer.
C. Rake areas to be grassed, filling depressions and removing stones over 2" in size, sticks and rubbish.
D. After preparation of soil, level areas to be seeded and bring to finish grade. Hand rake smooth, allowing for settlement

901-3.2 SEEDING:

- A. Uniformly distribute the first half of the seed mixture over the designated areas using a seed drill and then uniformly distribute the last half of the seed mixture over the designated areas by means of a mechanical spreader. Seed mixture shall be sown at the rate shown in the SEEDING SCHEDULE, immediately after fertilization.
B. Sow half of seed with spreader moving in one direction; the remainder with spreader moving at right angle to first sowing.
C. Cover seed lightly with minimum of 1/4" of soil by hand raking.
D. Firm seeded area by rolling with a light roller (40_65 lbs. per foot of width for clay soil, and 150_200 lbs. per foot of width for sandy or light soils).
E. Mulch seeded areas having slopes of four to one or greater with dry mulch or hydromulch. Use asphalt spray mulch or hydromulch within safety areas of airfield pavements (See Typical Section sheet for prescribed safety area distances).

901-3.3 DRY MULCH:

- A. Apply dry mulch at rate of 2_3 tons per acre to provide a uniform loose depth between 1_1/2 and 3 inches.
B. Secure mulch with a very thin layer of topsoil, stakes and wire mesh, or asphalt binder as approved by Engineer.
1. Peg and string method _ secure mulch using stakes and wire pins spaced on 5 foot centers. String binder twine diagonally and at right angles.
2. Manufactured netting may be used if approved by Engineer.

901-3.4 HYDROSEEDING:

- A. Apply seed/fertilizer/hydromulch mixture in water slurry. Dispense using hydraulic mulching equipment in following minimum quantities:
1. Fertilizer: 1000 lbs./acre.
2. Hydromulch: 1200_1500 lbs./acre.
3. Seed: Please see SEEDING SCHEDULE for appropriate percentages for each respective growing season.

901-3.5 MAINTENANCE:

- A. Maintain grassed areas at a height of not to exceed 2" until final acceptance of the project. Repair damaged mulch.
B. Regrass areas larger than one sq. ft. not having a uniform stand of grass. Eradicate weeds which appear in grassed areas.
C. At completion of grassing operation, remove excess soil and debris from project site.
D. Protect seeded areas against traffic or other use by warning signs or barricades, as approved by the Engineer. Surfaces gullied or otherwise damaged following seeding shall be repaired by regrading and reseeding as directed.
E. The Contractor shall apply 30 units of nitrogen per acre after the grass has attained a height of 1/2 to 3/4 inch. Approximately four (4) weeks afterwards, apply a second application of 30 units of nitrogen per acre.
F. Contractor shall establish a good stand of grass of uniform green color and density to the satisfaction of the Engineer. If at the time when the contract has been otherwise completed, it is not possible to make an adequate determination of the color, density, and uniformity of such stand of grass, payment for the unaccepted portions of the areas seeded out of season will be withheld until such time as these requirements have been met. In lieu of retaining the value of grassing not acceptable to the Engineer, the Contractor may submit a bond to the Owner (providing the Owner agrees to this option) for the amount of grassing that has not achieved a good stand of grass of uniform green color and density and receive payment for the unacceptable grassing.

METHOD OF MEASUREMENT

901-4.1 The quantity of seeding and mulching to be paid for shall be the number of acres, measured on the ground surface, completed and accepted. All areas within the Contractor's staging area, haul routes and all other areas outside the limits of construction to be grassed shall not be measured for payment. Grassing of these areas shall be considered incidental to and included in the lump sum price bid for item C-105 Mobilization.

BASIS OF PAYMENT

901-5.1 The quantity, determined as provided above, will be paid for at the contract unit price per acre, or fraction thereof, for the pay item listed below, which price and payment shall be full compensation for furnishing and placing all material, including mulching, watering and maintenance of seeded areas as directed by the Engineer, and for all labor, equipment, tools, and incidentals necessary to complete the work prescribed in this item.

Watering or mowing will not be measured or paid for as a separate item. It shall be incidental cost to Grassing as stated above.

Payment will be made under:

Item T-901 Grassing and Mulching-per Acre

END ITEM T-901

ADDITIVE BID SEEDING SCHEDULE

Table with 4 columns: SCHEDULE NO., COMMON NAME OF SEED, POUNDS/ACRE RURAL URBAN1, PLANTING DATES. Includes rows for Common Bermuda (Hulled), Weeping Lovegrass, Sericea Lespedeza (Scarified), Common Bermuda (Unhulled), Sericea Lespedeza (Unhulled, Unscarified), Reseeding Crimson Clover, Annual Rye Grass, Rye Grain, and Centipede.

NOTES:

- 1. INCLUDES RURAL AREAS ADJACENT TO WELL-DEVELOPED LAWNS.
2. NOT REQUIRED ON SHOULDERS, MEDIANS, ETC. AND ON SLOPES UNDER 5 FEET IN HEIGHT.
3. DO NOT USE GIANT BERMUDA SEED INCLUDING NK-37.
4. INOCULATE RESEEDING CRIMSON CLOVER IN ACCORDANCE WITH SUBSECTION 810.2.4. DO NOT PLANT CLOVER IN MEDIANS OR IN RURAL AREAS ADJACENT TO WELL-DEVELOPED LAWNS.
5. APPLY ONE-HALF OF LIME RATES AND ONE-HALF OF MAINTENANCE FERTILIZER RATES. FERTILIZER CENTIPEDE AT THE APPLICATION RATE OF 20 POUNDS PER ACRE OF 16-4-8 OR 15-0-15 FERTILIZERS IN MAY AND REPEAT IN AUGUST.
6. THE USE OF ITALIAN RYE GRASS IS PROHIBITED ON ALL PROJECTS.
1. AREAS TO BE GRASSED SHALL BE DEFINED AS ALL AREAS OF SITE WITHIN THE GRADING LIMITS AND NOT OCCUPIED BY PAVING, CRUSHED STONE SURFACING OR STRUCTURES. GRASSING SHALL INCLUDE FINAL SHAPING, LIMING, FERTILIZING AND SEEDING OR SODDING.
2. LIME SHALL BE AGRICULTURAL GRADE, GROUND LIMESTONE. GROUND LIMESTONE SHALL CONTAIN NOT LESS THAN 85% OF CALCIUM CARBONATE CONTENT EQUIVALENT AND SHALL BE SUCH A FINENESS THAT 90% WILL PASS THROUGH A NO. 20 SIEVE AND NOT LESS THAN 50% THROUGH A NO. 100 SIEVE.
3. FERTILIZER SHALL BE GRADE 10-10-10 COMPLETE FERTILIZER OF UNIFORM COMPOSITION, FREE-FLOWING AND SUITABLE FOR APPLICATION WITH EQUIPMENT, DELIVERED TO SITE IN BAGS LABELED WITH MANUFACTURER'S GUARANTEED ANALYSIS, AND SHALL CONFORM TO ALL STATE AND FEDERAL REGULATIONS.
4. SEEDS SHALL BE MIXTURE AS APPROVED BY THE ENGINEER AND SHALL MEET REQUIREMENTS OF SEED LAWS OF THE STATE AND THE U.S. DEPARTMENT OF AGRICULTURE RULES AND REGULATIONS UNDER FEDERAL SEED ACT IN EFFECT ON DATE BIDS ARE RECEIVED. SEED SHALL BE DELIVERED IN STANDARD CONTAINERS. SEED WHICH HAS BECOME WET, MOLDY OR DAMAGED IN TRANSIT OR STORAGE WILL NOT BE ACCEPTABLE.
5. MULCH SHALL CONSIST OF SMALL GRAIN STRAW OF GOOD QUALITY, CLEAN, FREE OF NOXIOUS WEEDS, AND REASONABLY FREE OF OTHER WEEDS. SPREAD MULCH AT A RATE OF 1 TON PER ACRE ON SLOPES UP TO 8.0 % AND AT A RATE OF 1-1/2 TONS PER ACRE FROM 8.0% UP TO A SLOPE OF 3 TO 1.
6. SPREAD LIME AT A RATE OF 1,000 LBS. PER ACRE.
7. FERTILIZER SHALL BE DISTRIBUTED UNIFORMLY AT A RATE OF 1,000 LBS. PER ACRE AND SHALL BE INCORPORATED INTO SOIL TO A DEPTH OF AT LEAST 2" BY DISKING AND HARROWING.
8. SPREAD SEED AT A RATE AS NOTED ON THE DRAWINGS.
9. IMMEDIATELY AFTER FERTILIZING AND SEEDING HAVE BEEN COMPLETED, ENTIRE AREA SHALL BE COMPACTED BY MEANS OF A CULTIPACKER, ROLLER, OR APPROVED EQUIPMENT WEIGHING APPROXIMATELY 90 LBS. PER LINEAR FOOT OF ROLLER. USE OF SPECIFIED MULCH, APPLIED AT A RATE OF 1 - 2 TONS/ACRE (USE THE HIGHER RATE FOR 3 TO 1 SLOPES OR GREATER), WITH ASPHALT EMULSION TYPE SS-1, MS-2, RS-1, OR RS-2, SHALL BE USED TO PROTECT SITE AGAINST EROSION.
10. AREAS THAT REQUIRE RE-FERTILIZATION AND/OR RE-SEEDING WILL BE DESIGNATED BY THE ENGINEER. WHEN ANY PORTION OF SURFACE BECOMES GULLED OR OTHERWISE DAMAGED FOLLOWING SEEDING, OR SEEDLINGS HAVE BEEN WINTER-KILLED OR OTHERWISE DESTROYED, AFFECTED PORTION SHALL BE REPAIRED TO RE-ESTABLISH CONDITION AND GRADE OF SOIL PRIOR TO SEEDLING AND SHALL BE RE-SEED AS SPECIFIED ABOVE.
11. ALL DISTURBED AREAS ARE TO BE GRASSED IMMEDIATELY AFTER CONSTRUCTION IN THE AREA. AT NO TIME WILL AN AREA BE LEFT BARE FOR MORE THAN 14 DAYS AFTER COMPLETION OF CONSTRUCTION.
12. PERMANENT GRASS SHALL BE PROVIDED FOR ALL DISTURBED AREAS. SEED SHALL BE A MINIMUM 90% PURITY AND 80% GERMINATION. AREAS TO HAVE GRASS APPLIED SHALL BE SCARIFIED CULTIVATED TO A DEPTH OF 3 INCHES, WITH ALL CLODS OR CLUMPS BROKEN UP AND FOREIGN MATERIAL AND DEBRIS REMOVED. FERTILIZER AND LIME SHALL BE THOROUGHLY WORKED INTO THE SOIL, AND THE SURFACE RAKED SMOOTH BEFORE APPLYING SEED. SEED SHALL BE APPLIED EVENLY AT THE MINIMUM RATE AND RAKED IN LIGHTLY. MULCH SHALL BE APPLIED AT THE RATE AS SPECIFIED ABOVE.
13. CONTRACTOR SHALL WATER AS NEEDED UNTIL GRASS IS ESTABLISHED.
14. ALL DISTURBED AREAS SHALL BE HYDROSEEDED UNLESS SHOWN ON PLANS TO RECEIVE COMPOST BLANKET.
15. NO BURNING IS ALLOWED ON THE PROJECT SITE.
16. TEMPORARY SEEDING AND MULCHING SHALL BE PAID PER ACRE UNER ITEM C-102-5.
17. PERMANENT SEEDING AND MULCHING SHALL BE PAID PER ACRE UNDER ITEM T-901-1.



WALTERBORO, SOUTH CAROLINA

Michael Baker INTERNATIONAL

Michael Baker International 700 Huger Street Columbia, South Carolina 29201



Designer: ADB/NWM

Checked by: ADB

Technician: MTW

Project Number: 192930

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Drawing Name: EROSION CONTROL DETAILS

Table with 2 columns: FAA A.I.P. Project Number: 3-45-0057-025-2025, Date: MARCH, 2025, Division: AIRPORTS, Scale: N.T.S., Sheet Number, Drawing Number: EC-3



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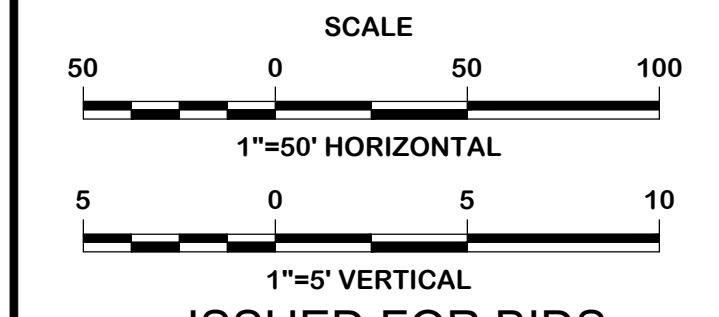
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Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



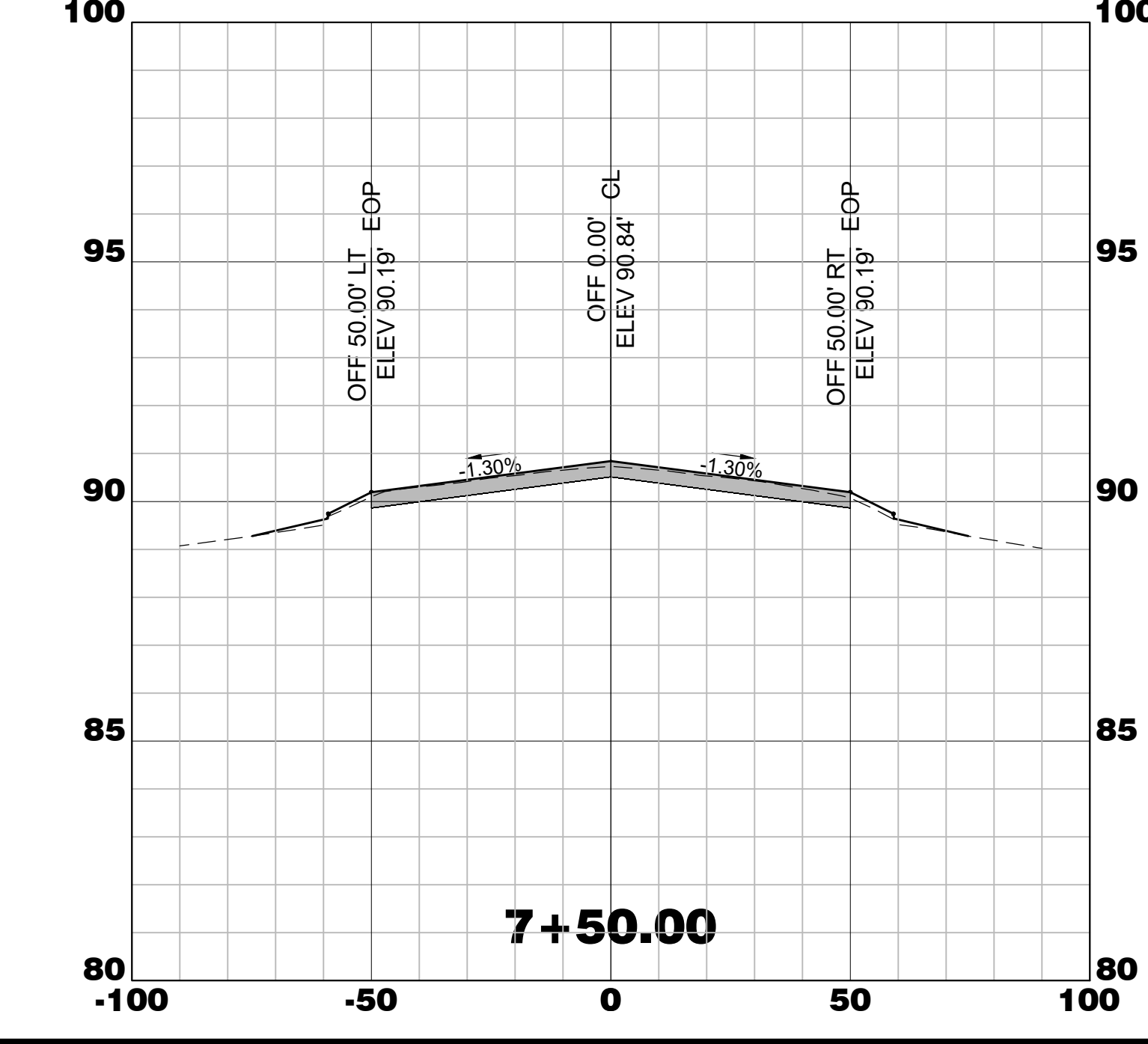
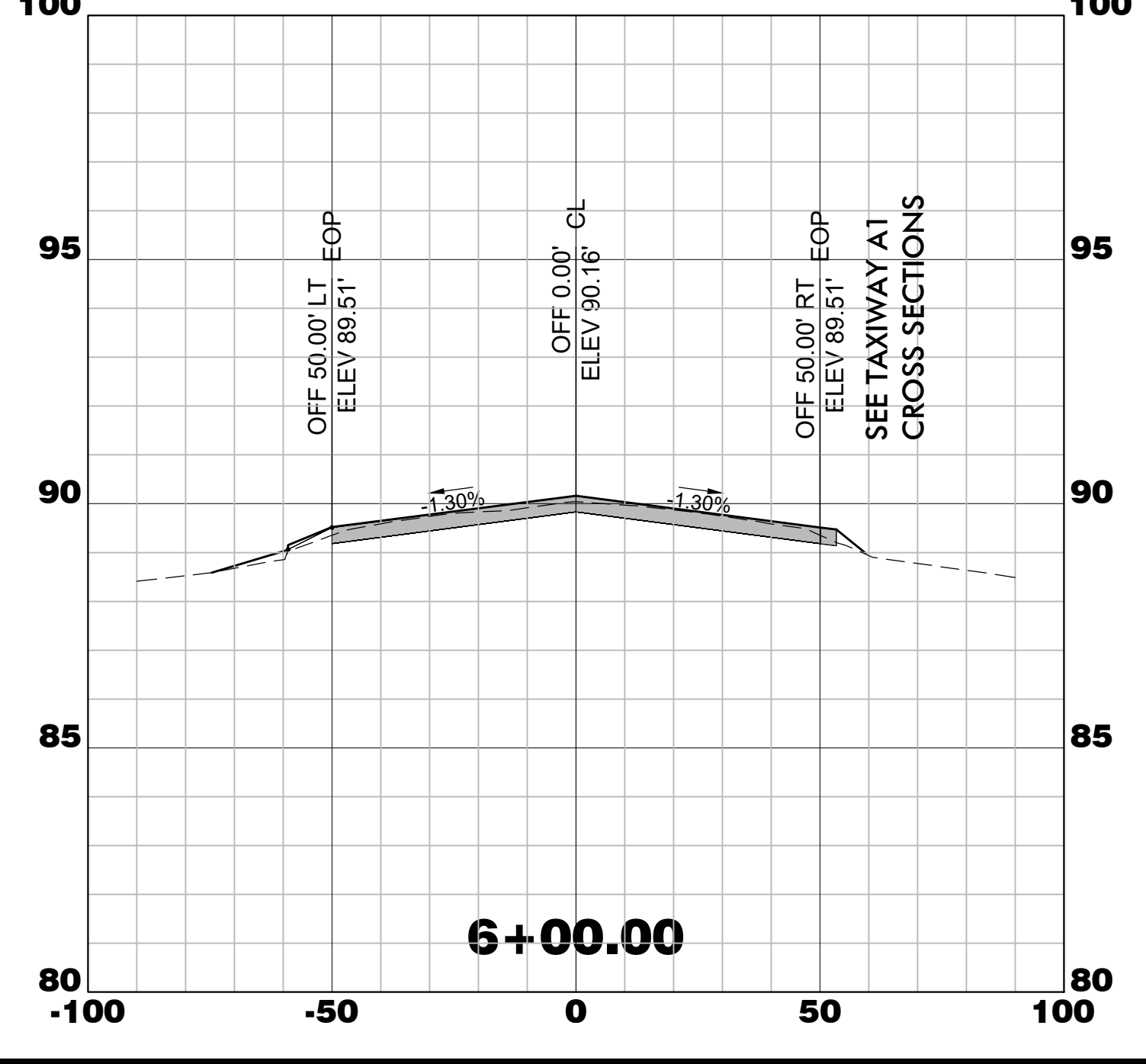
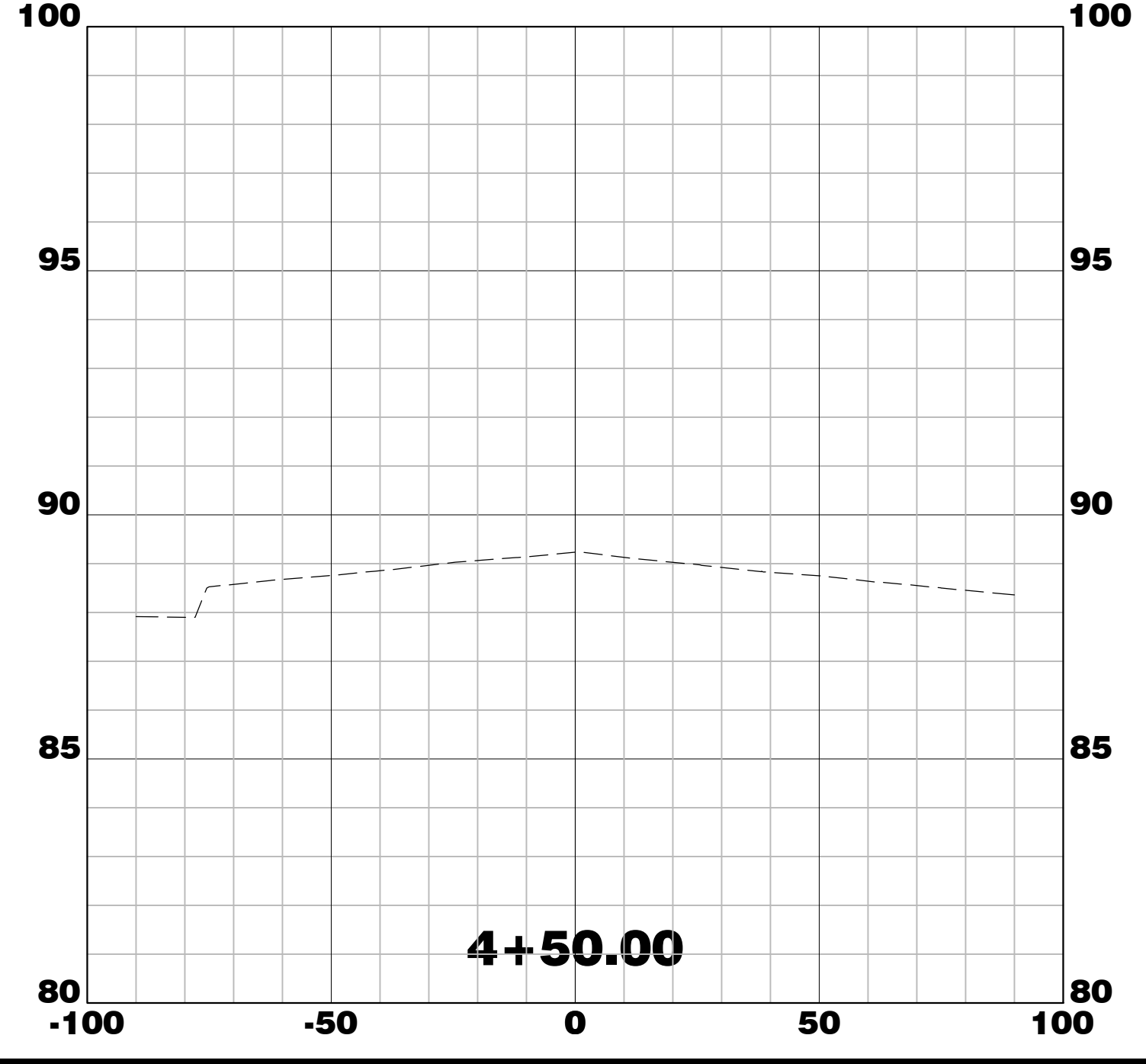
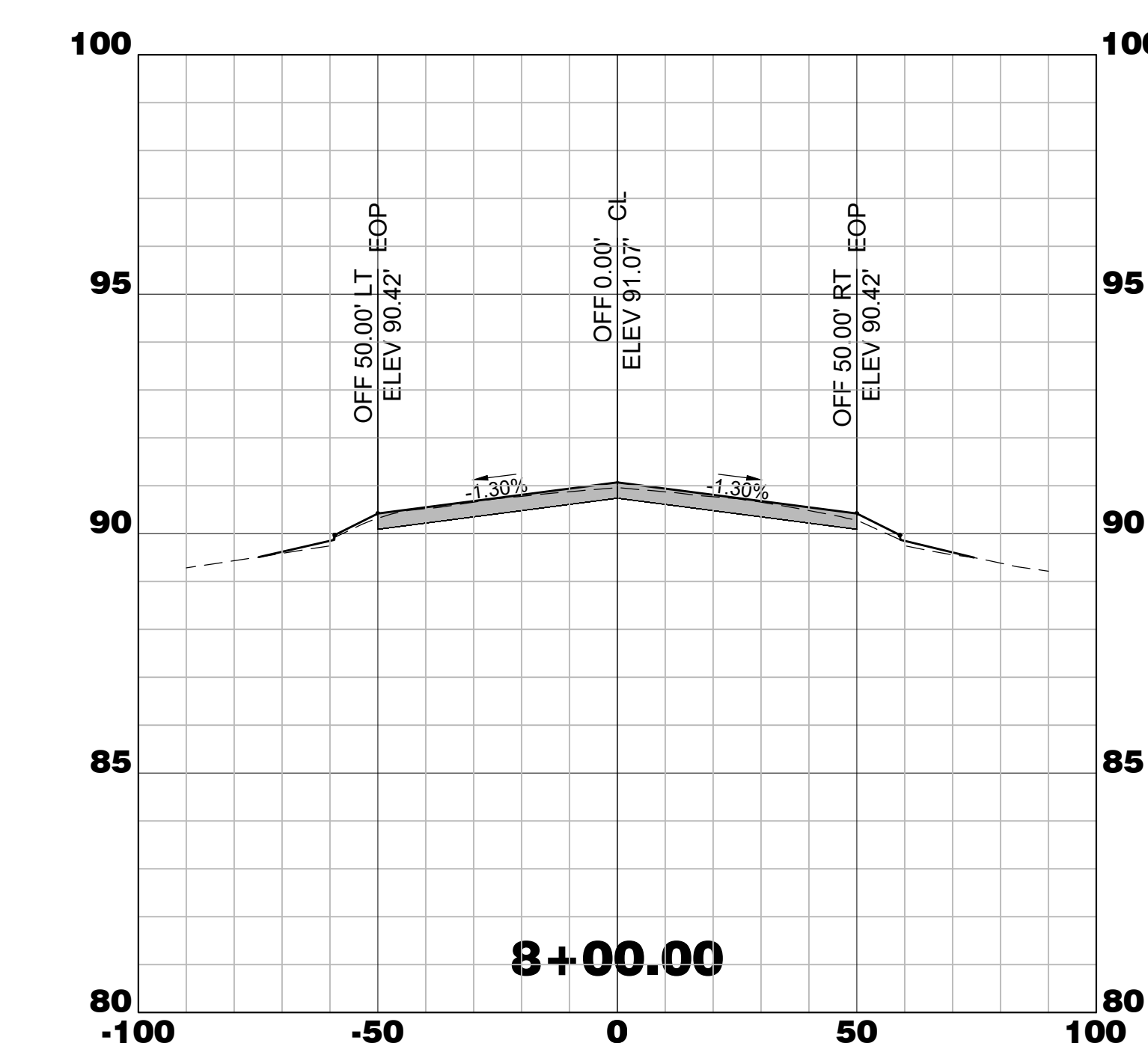
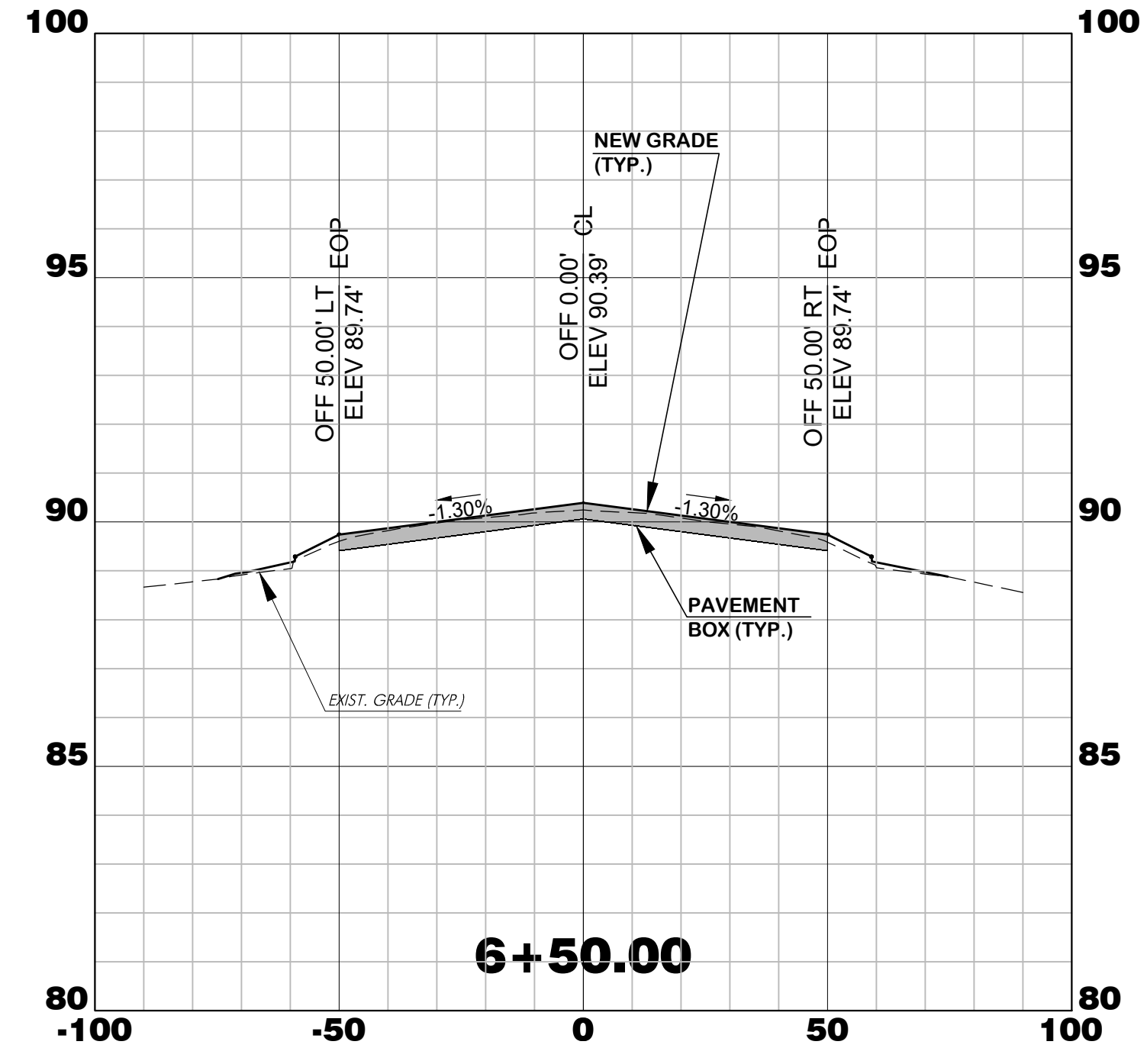
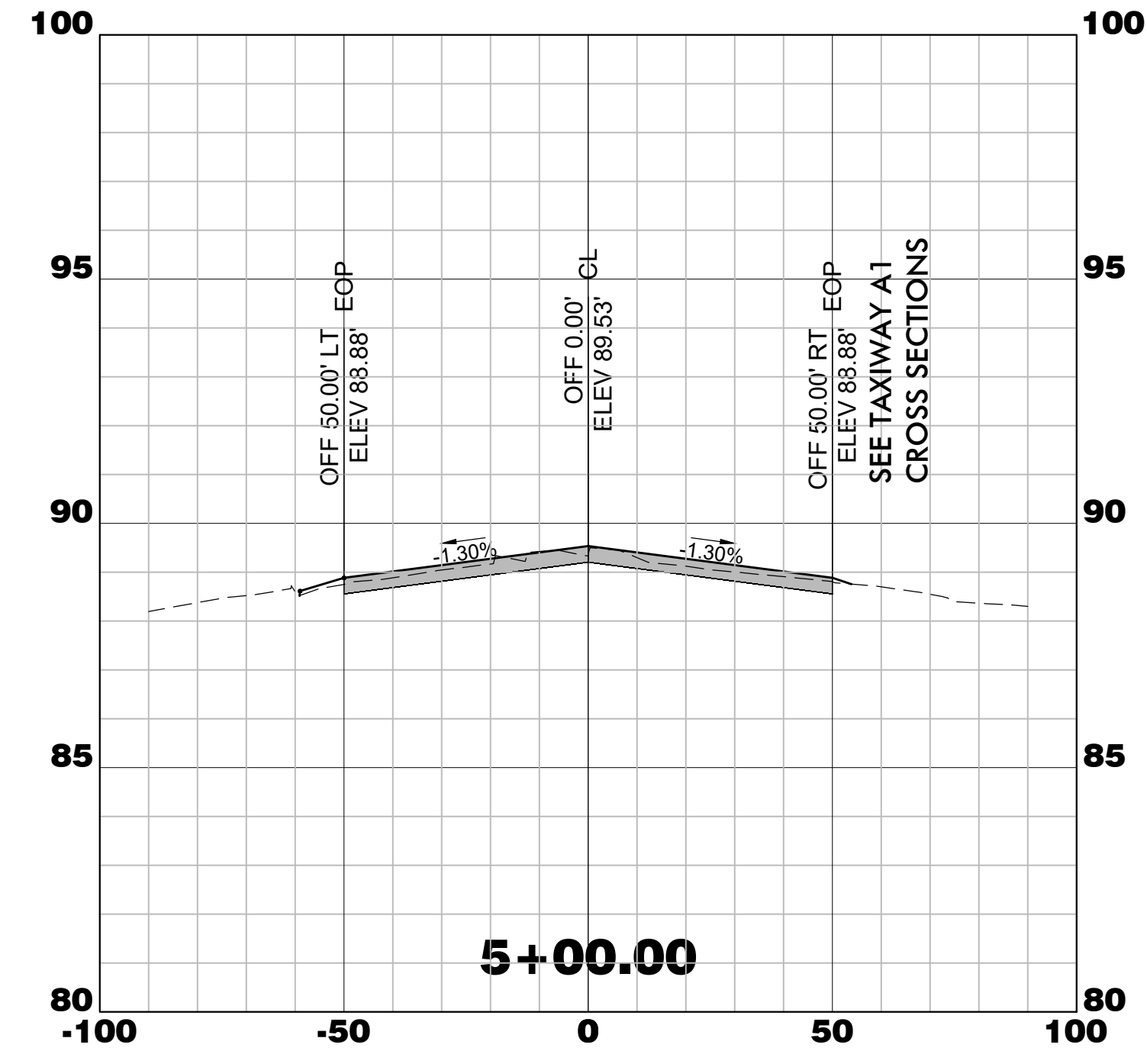
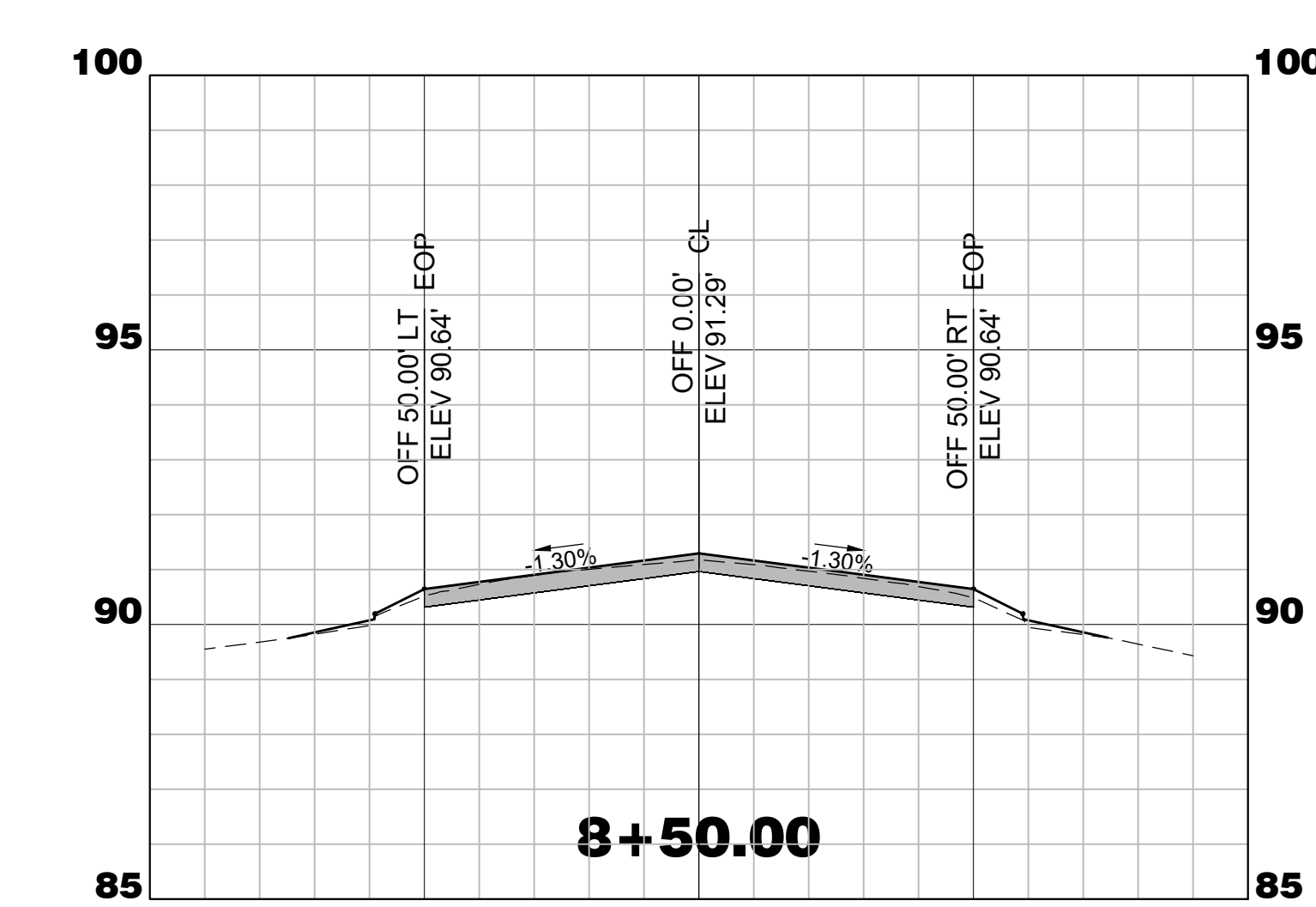
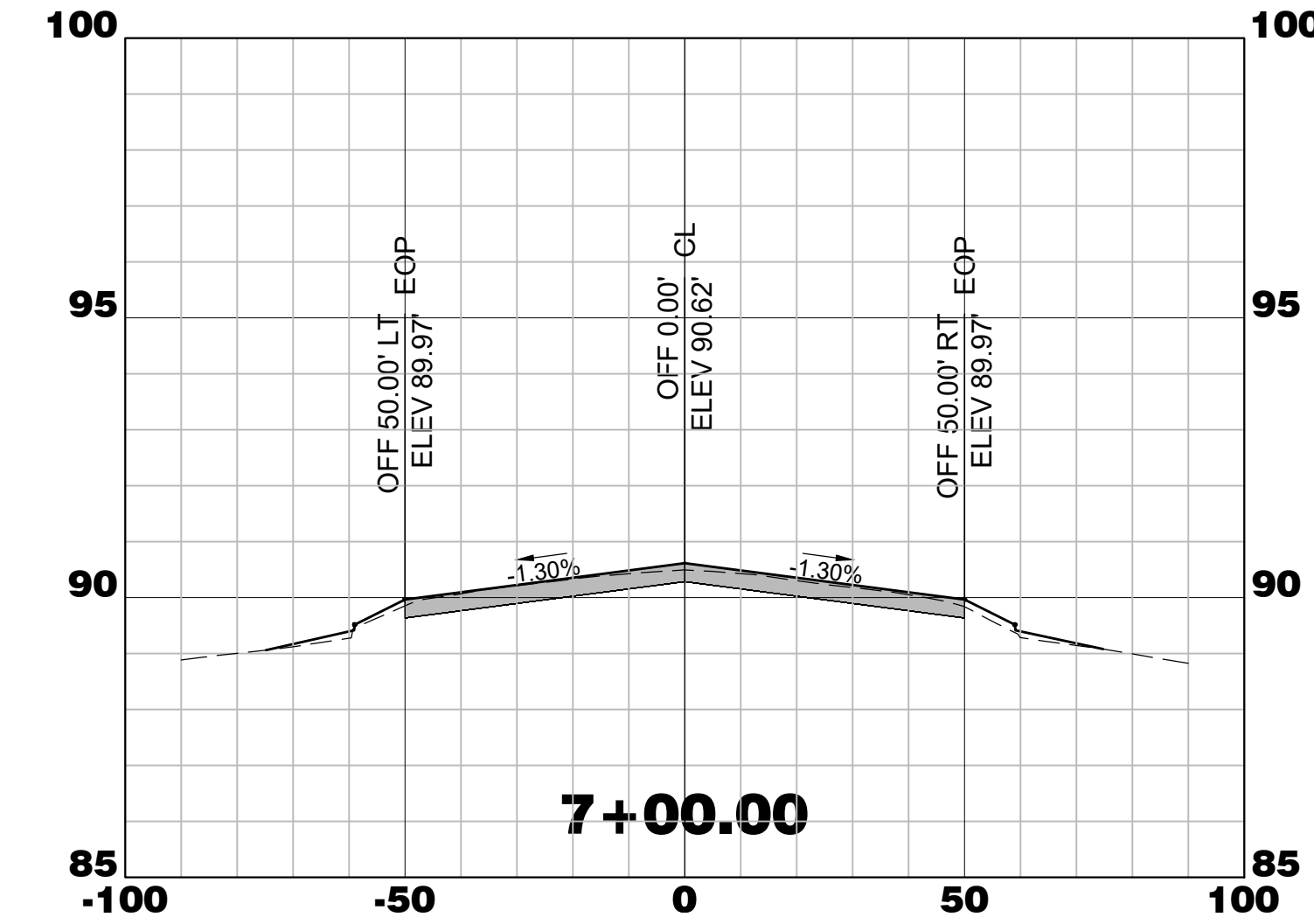
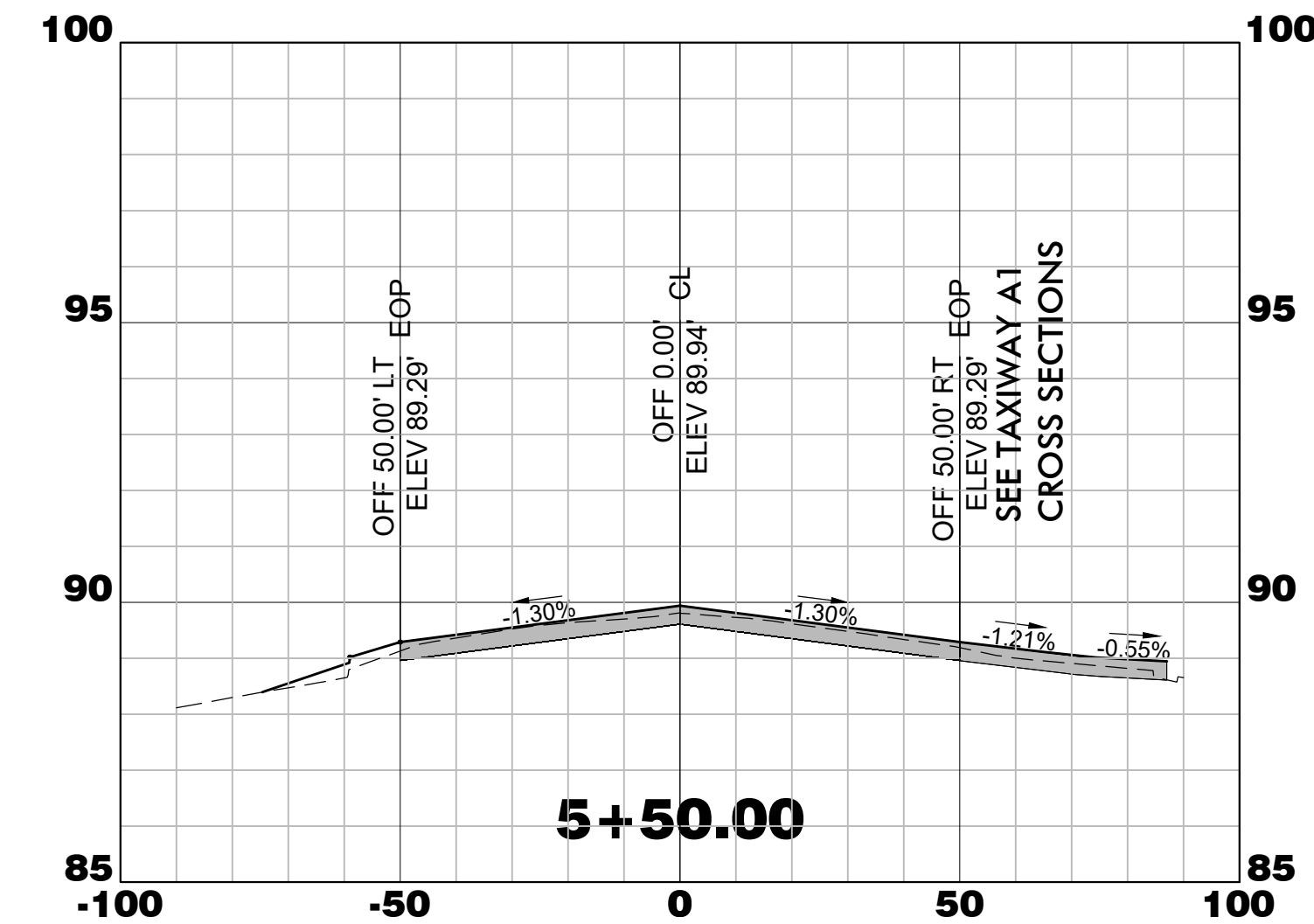
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No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
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Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ.	Sheet Number:
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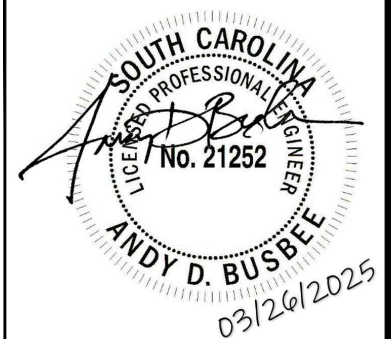


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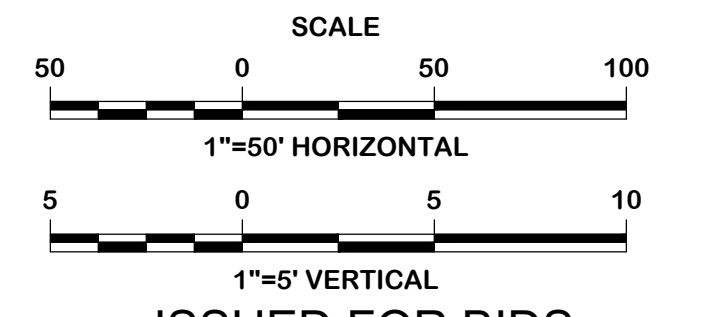
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Technician: NWM	Project Number: 192930



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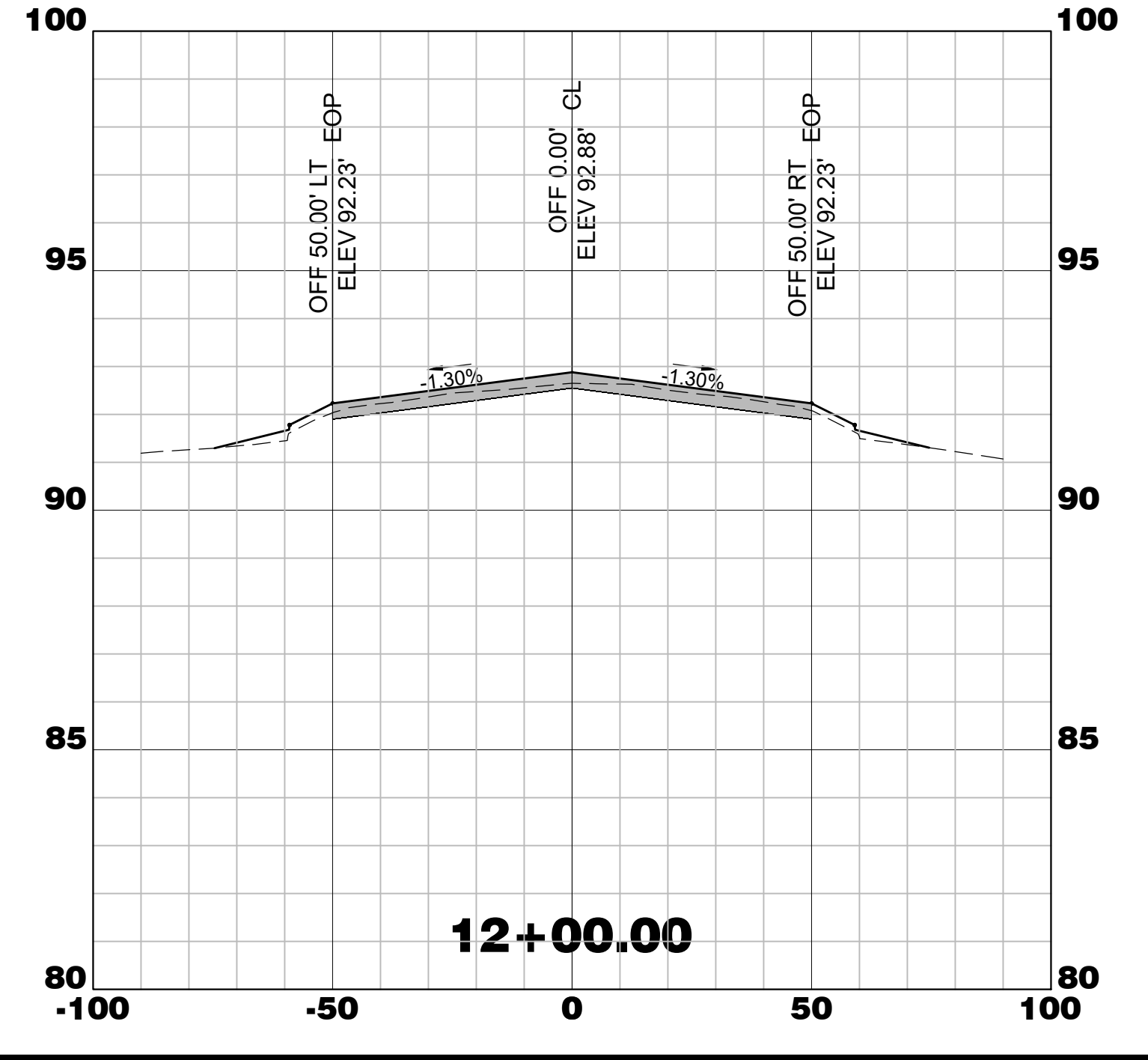
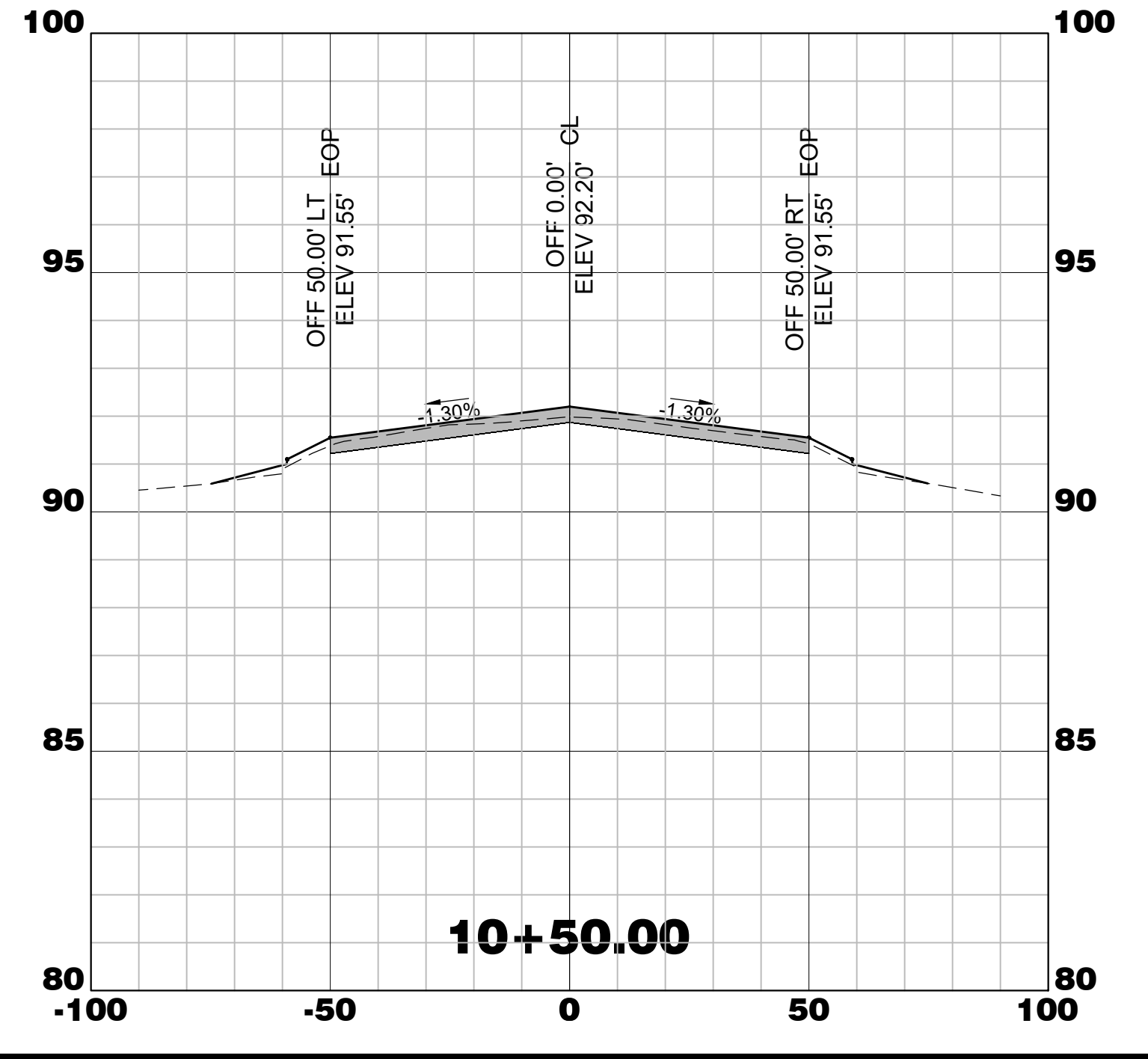
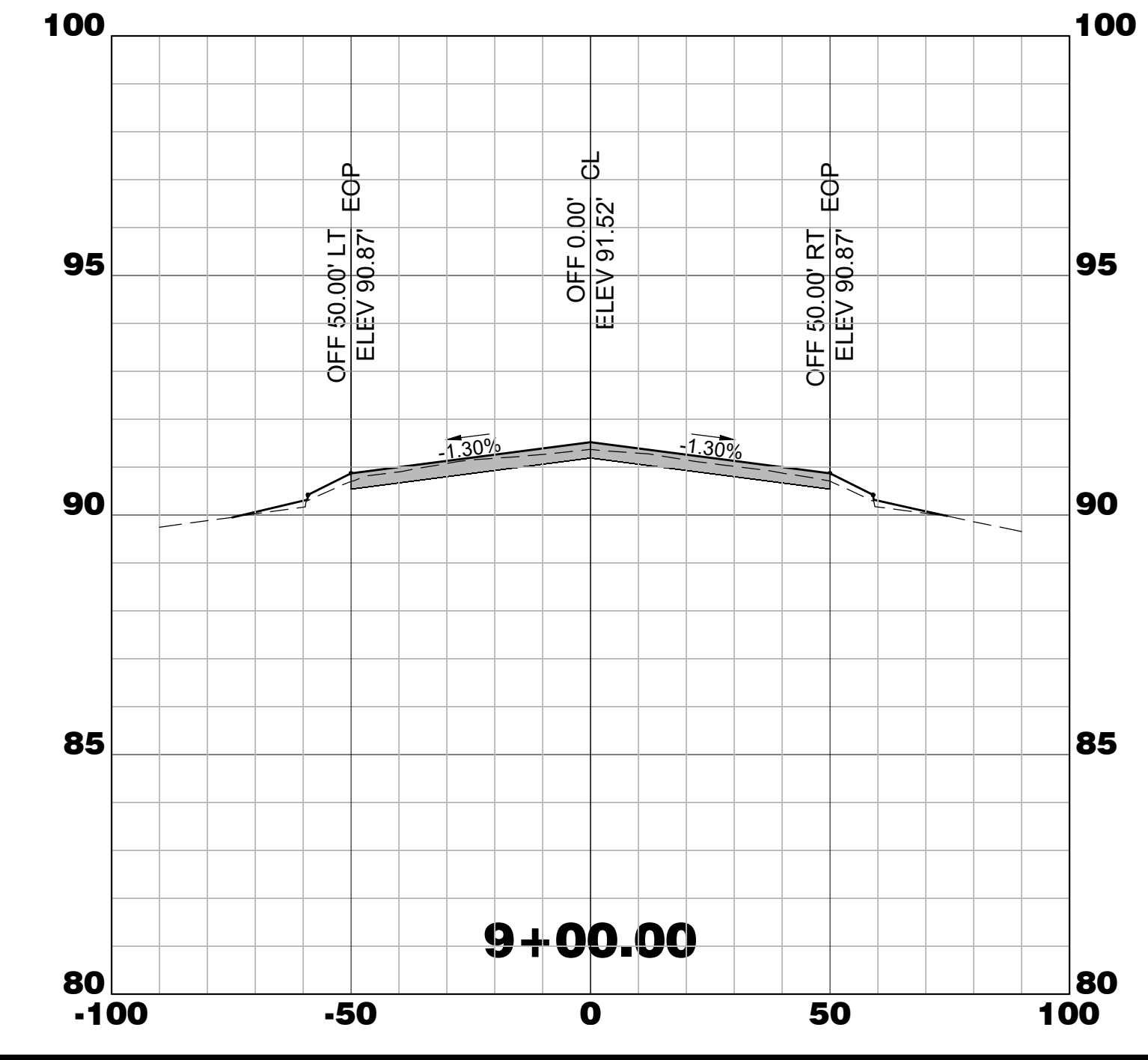
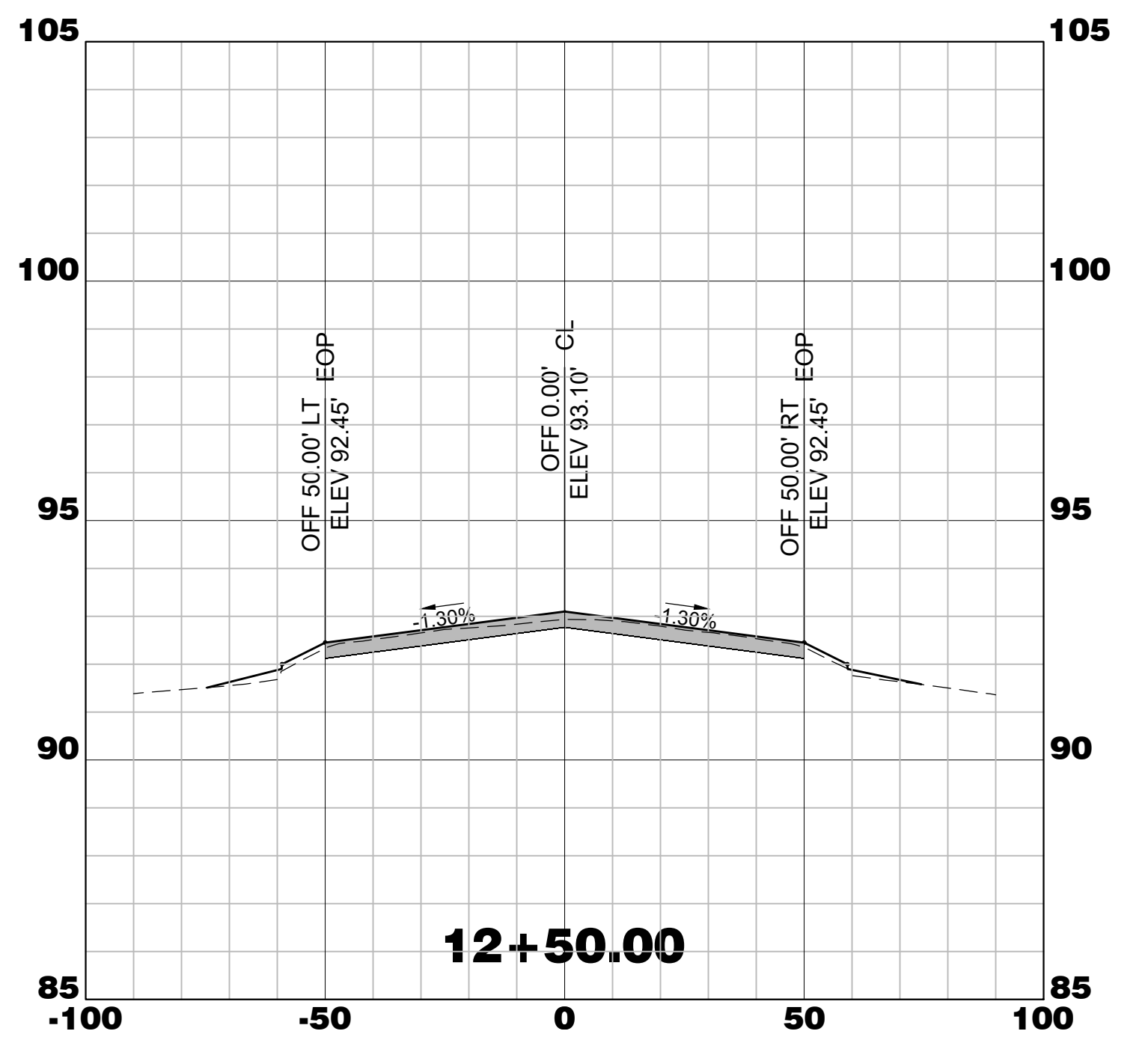
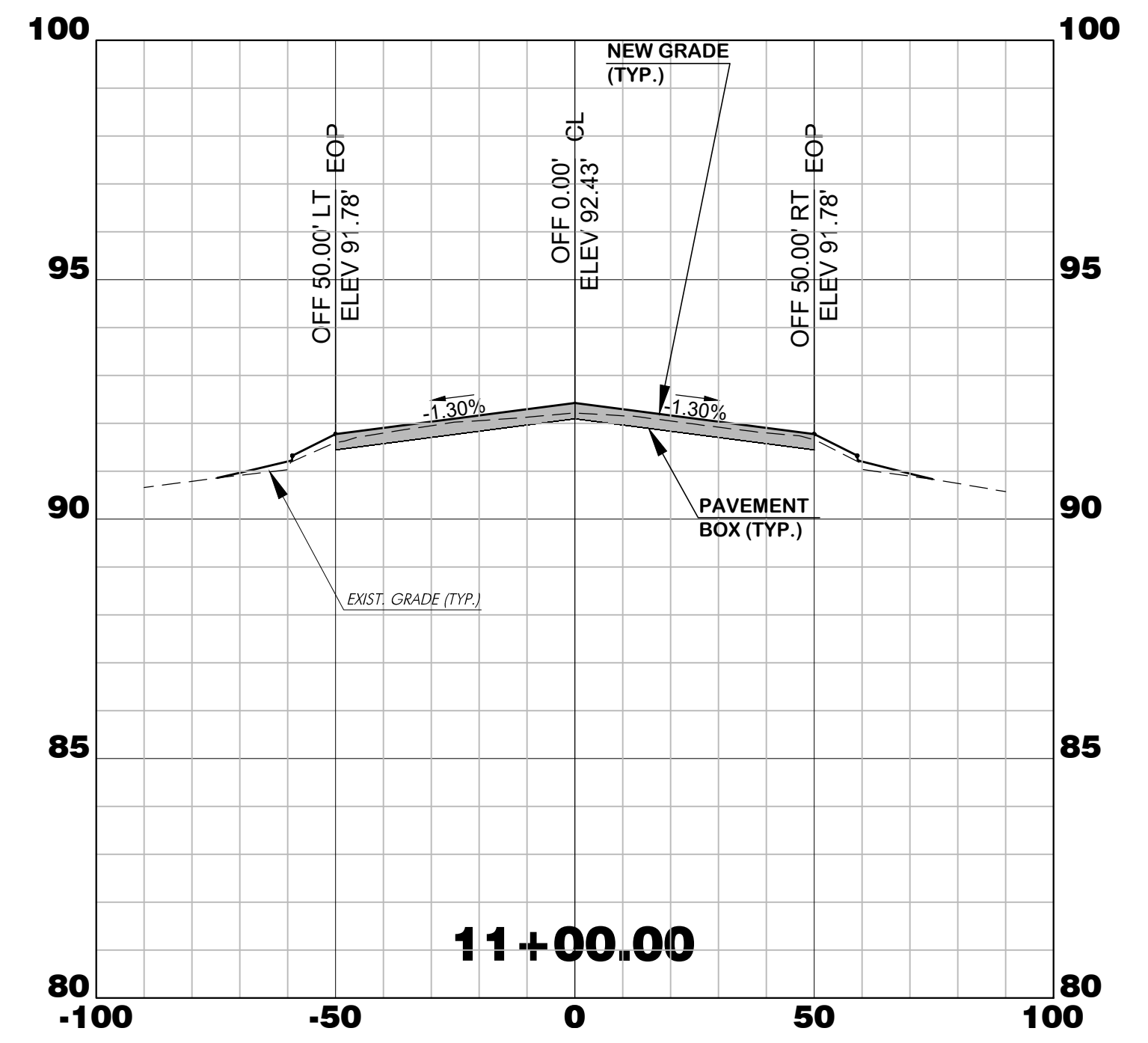
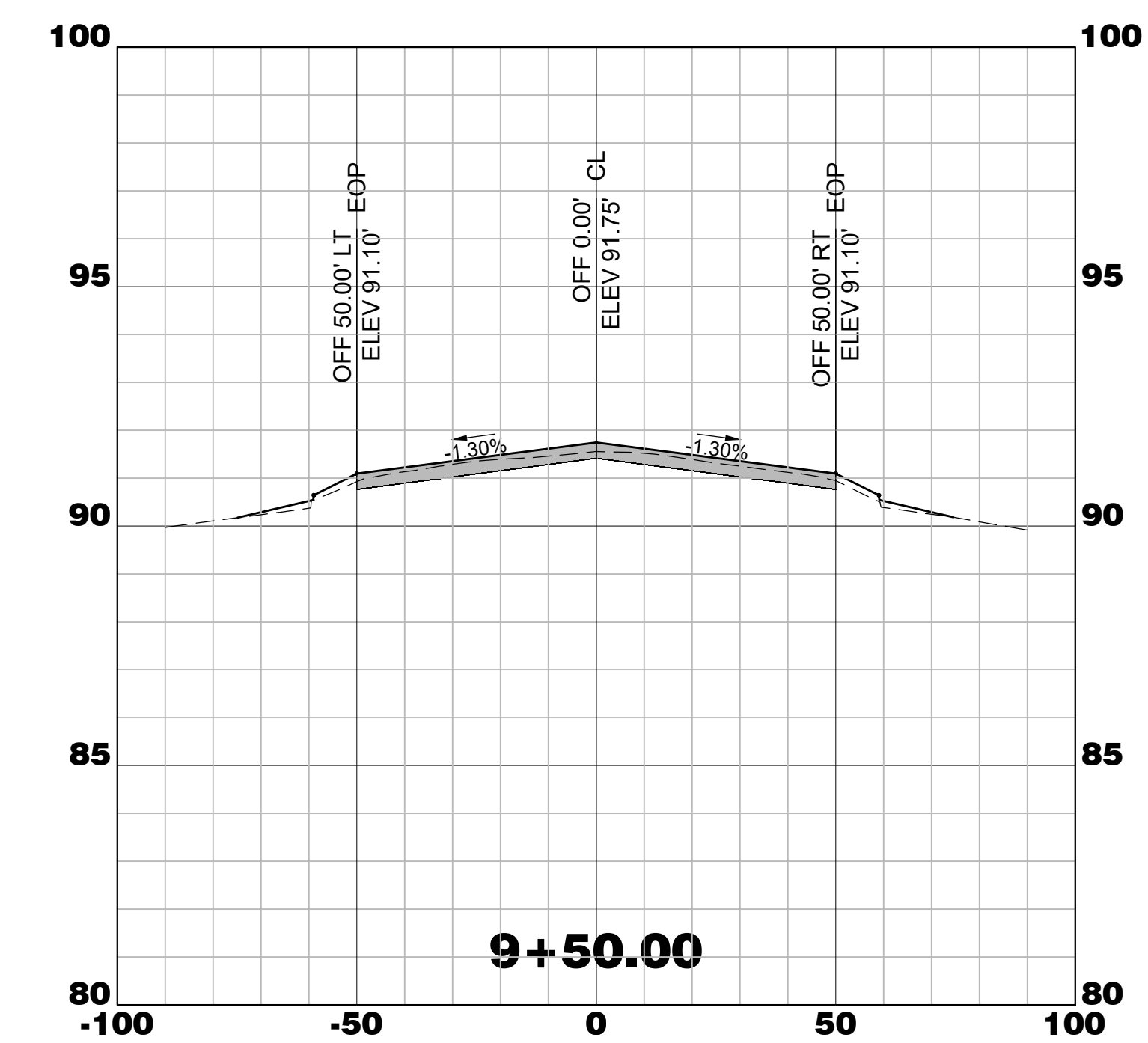
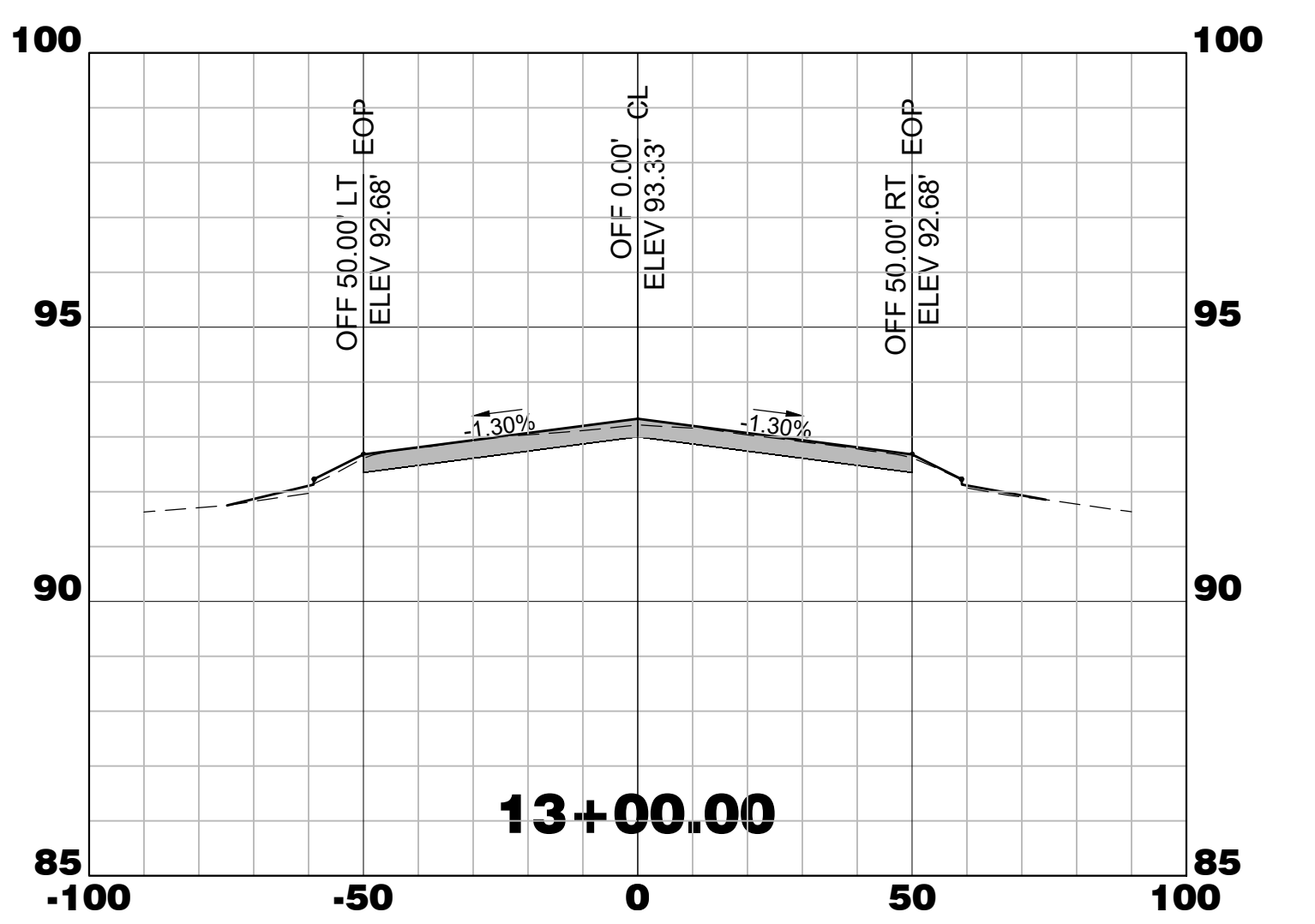
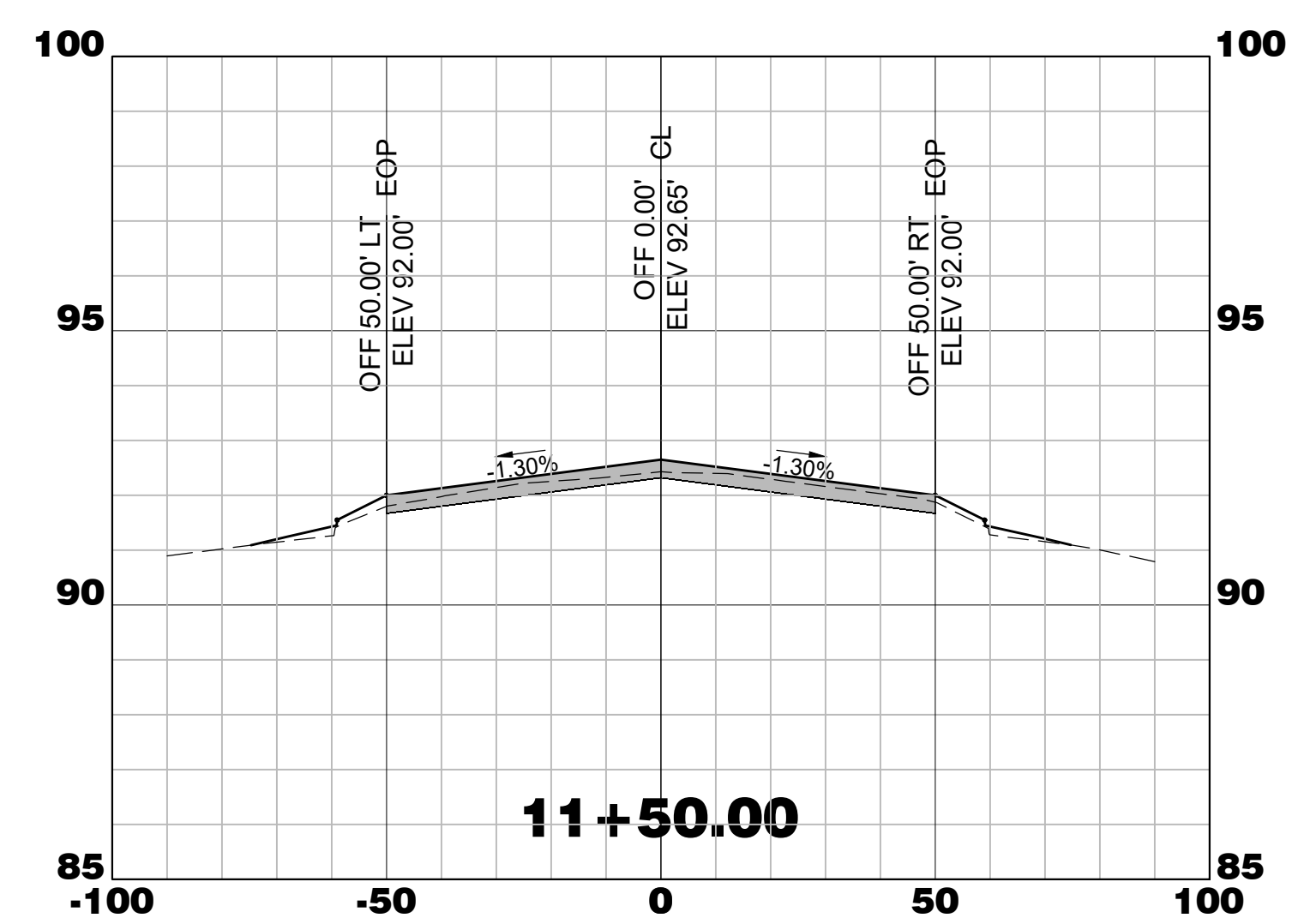
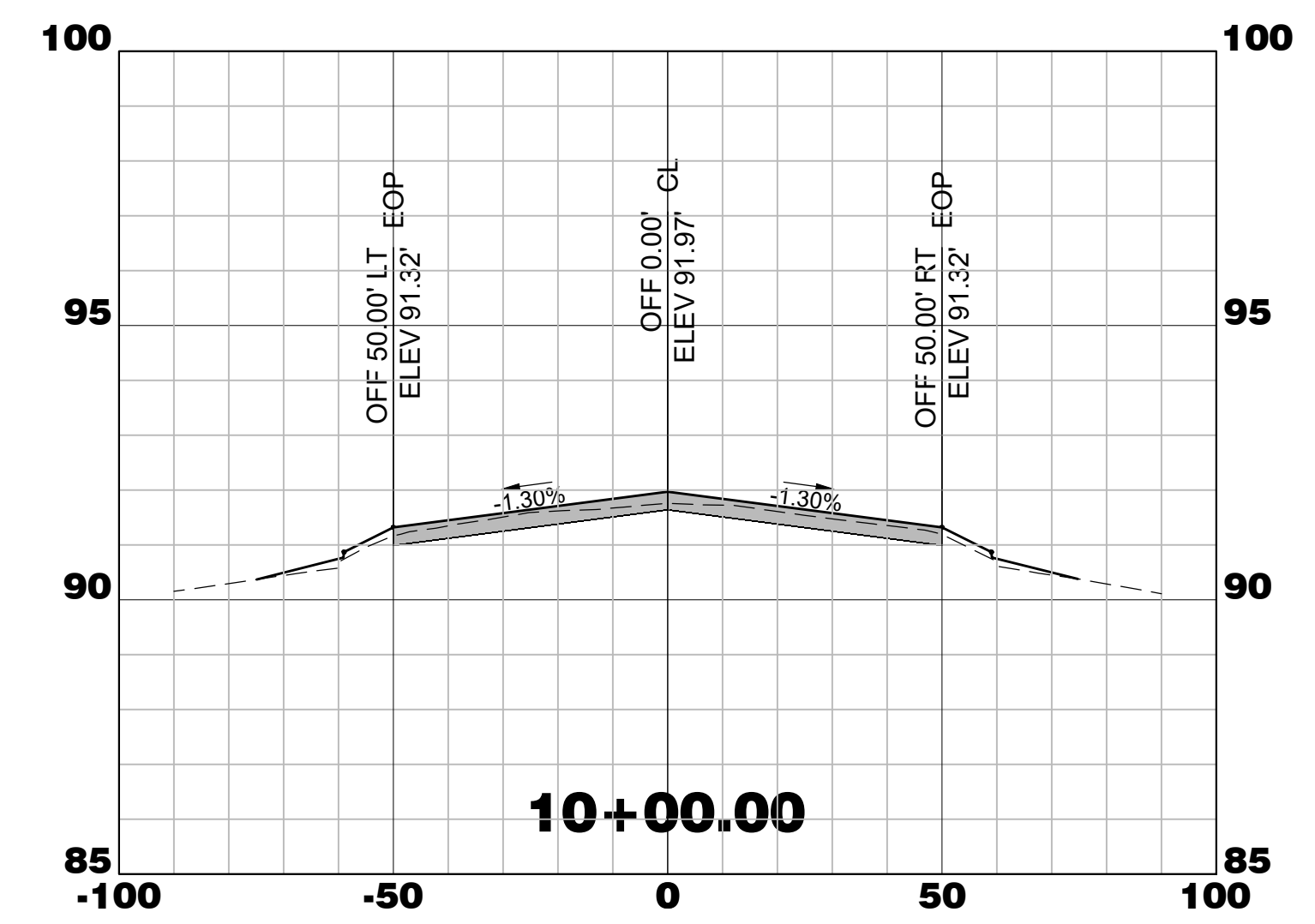
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Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"=50 HORIZ.** Sheet Number:

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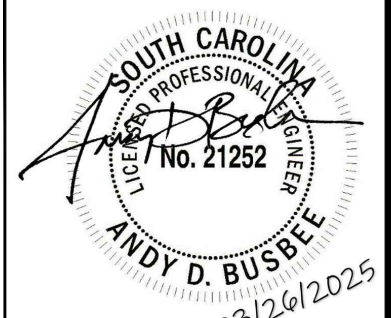


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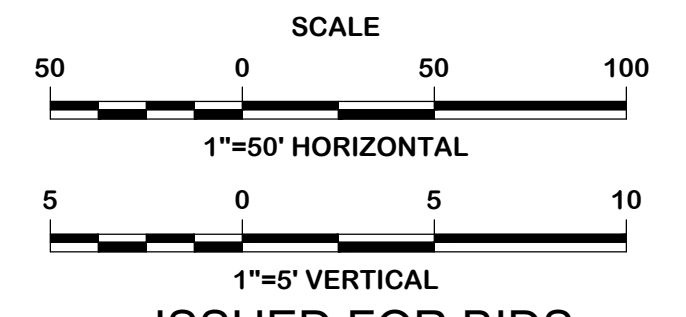
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Technician: NWM	Project Number: 192930



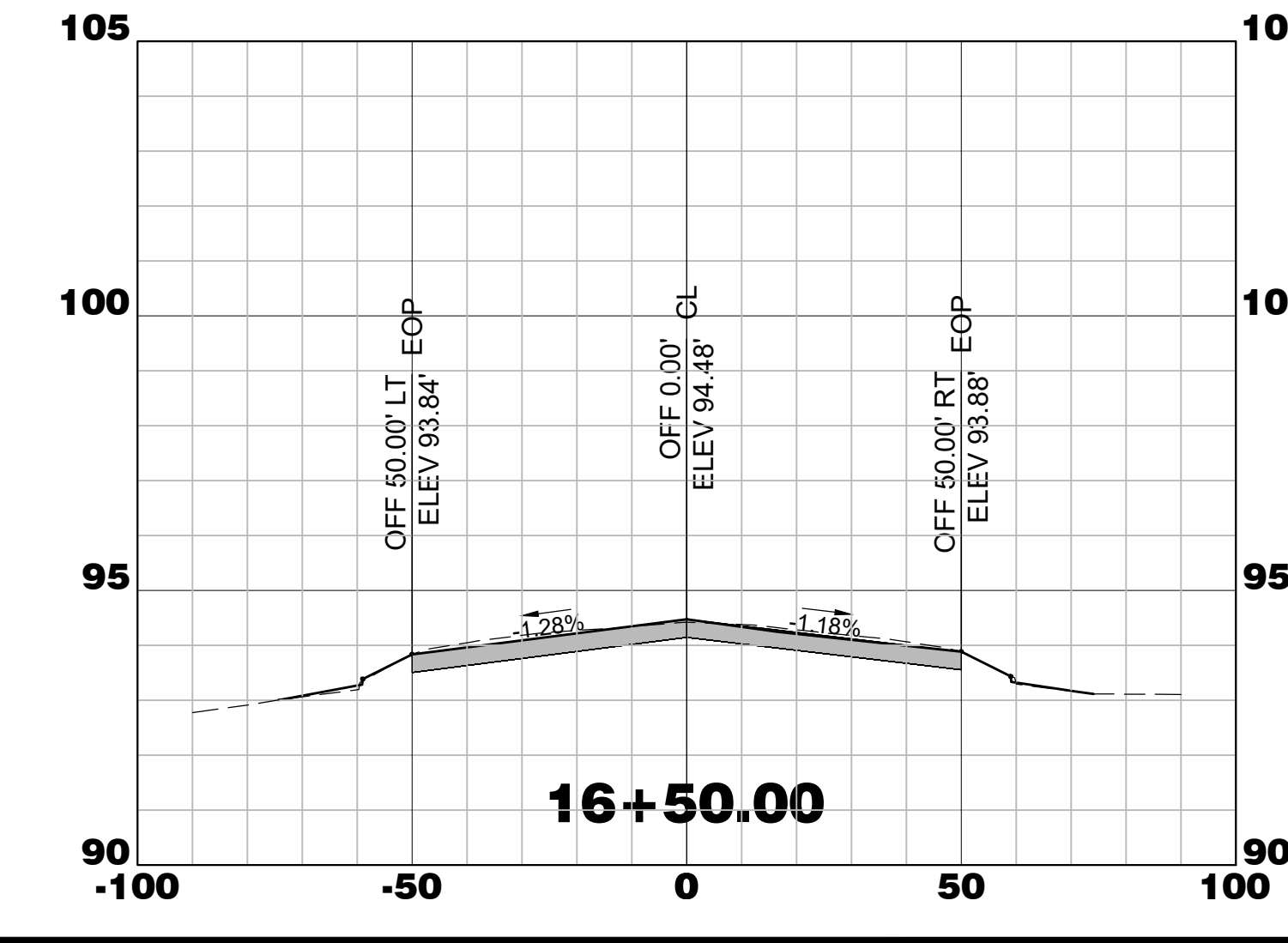
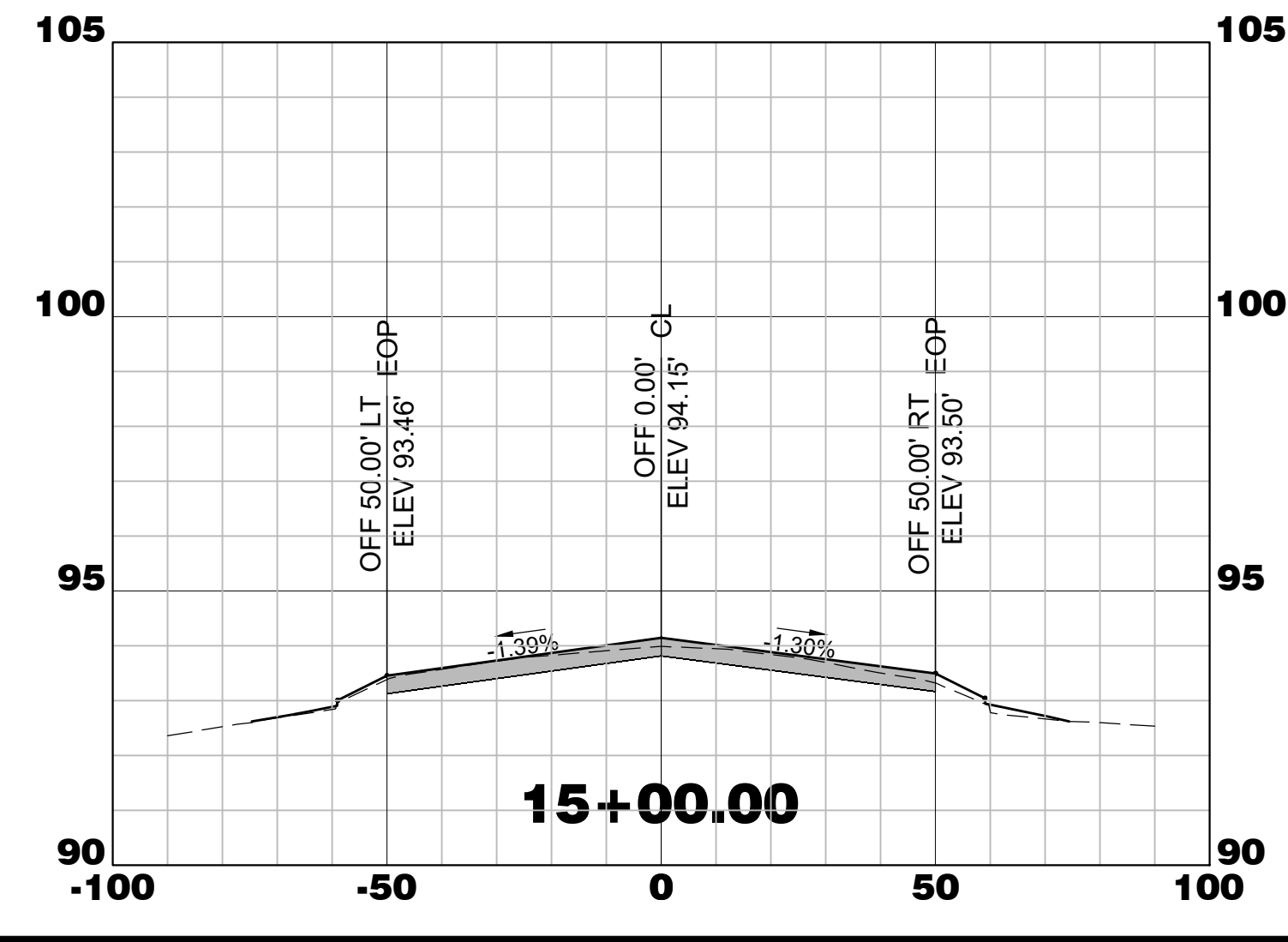
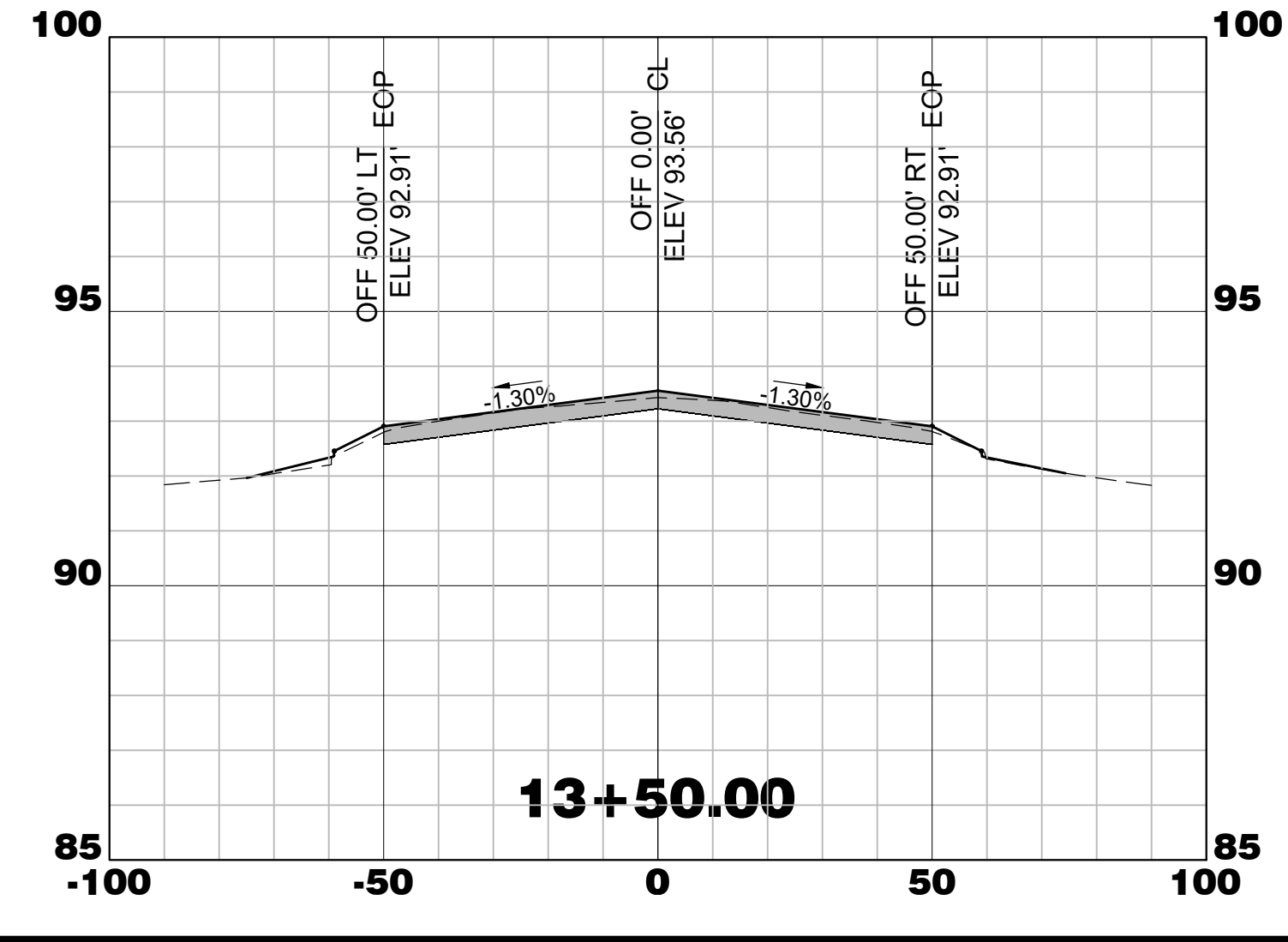
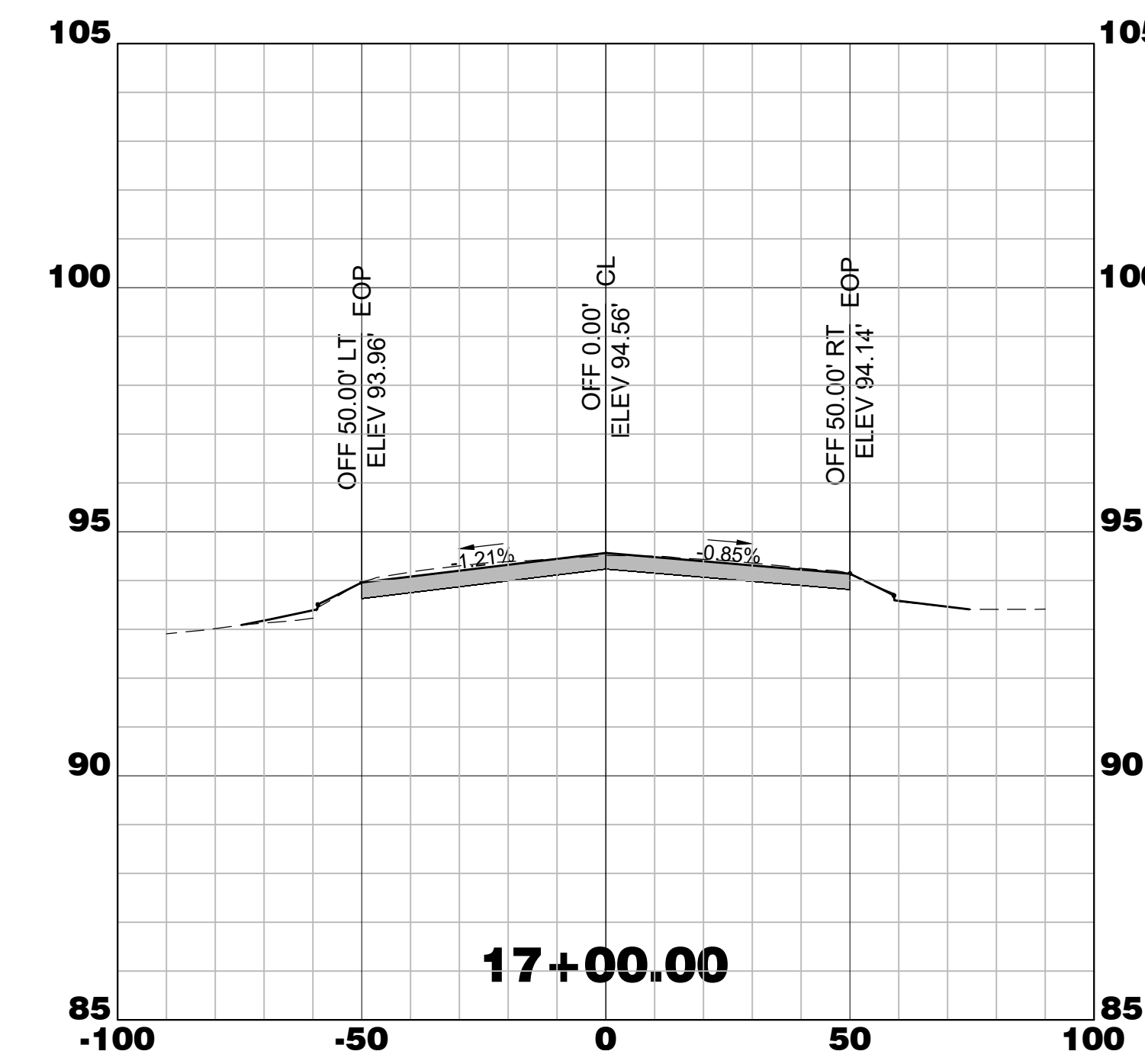
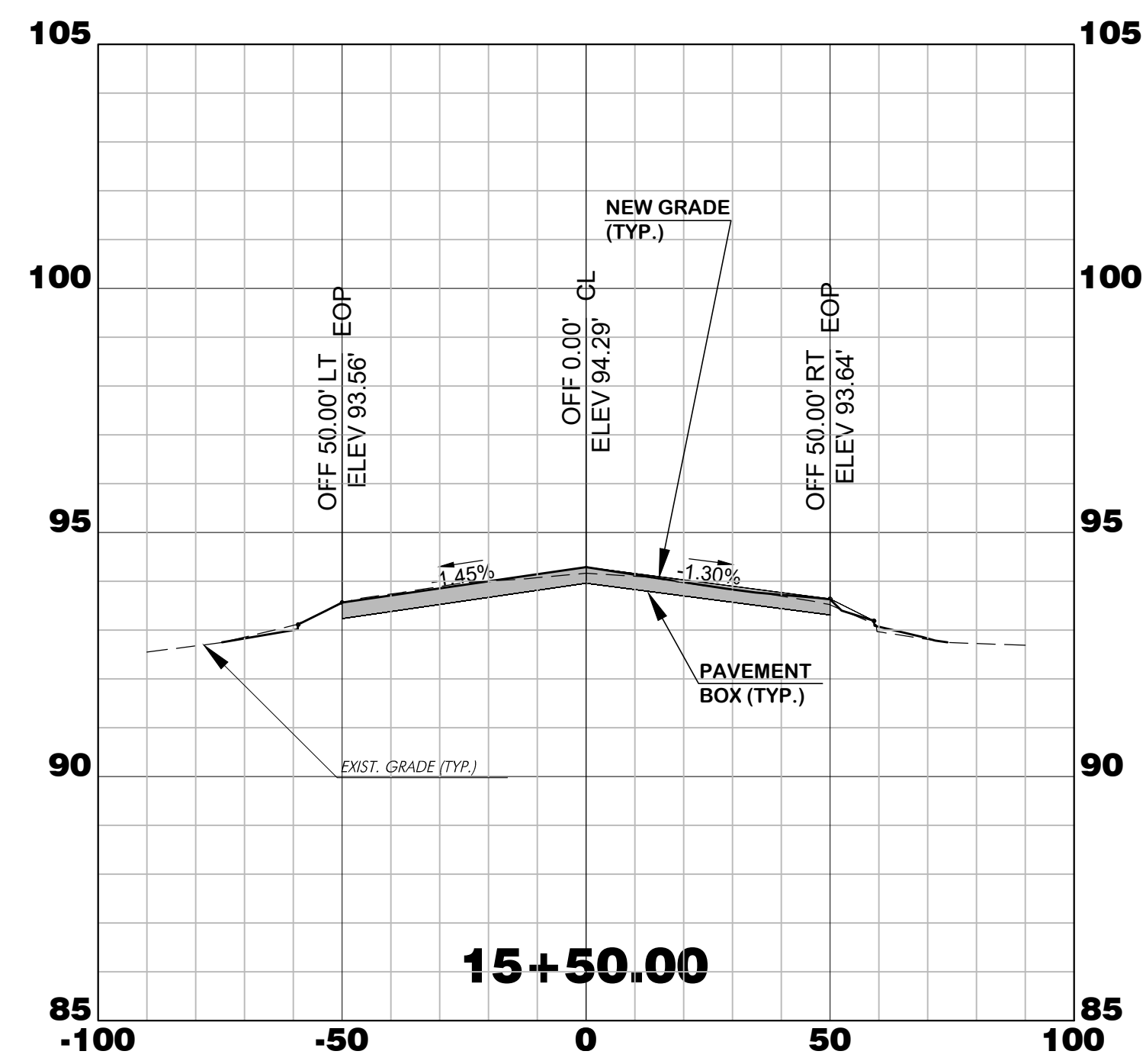
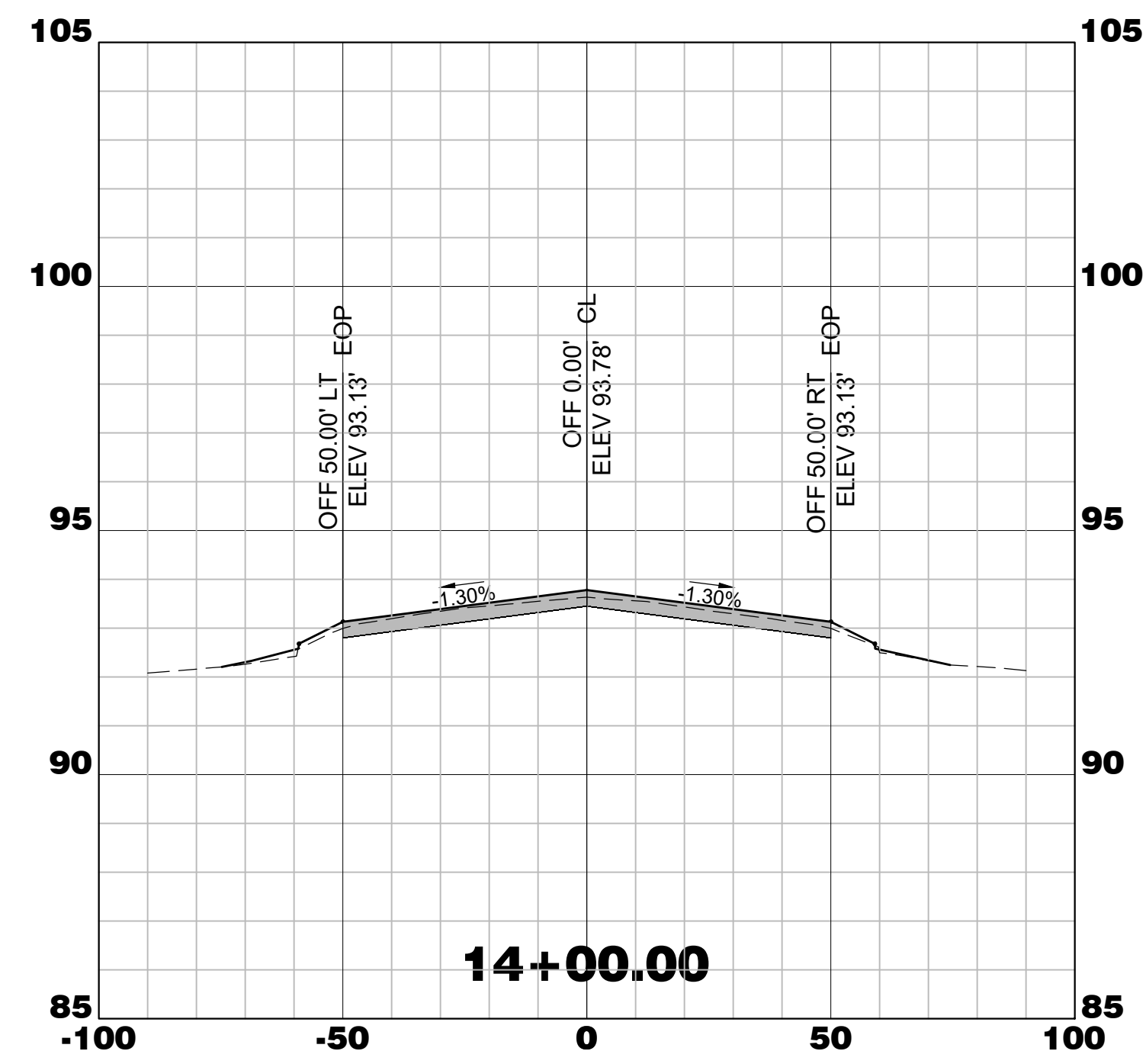
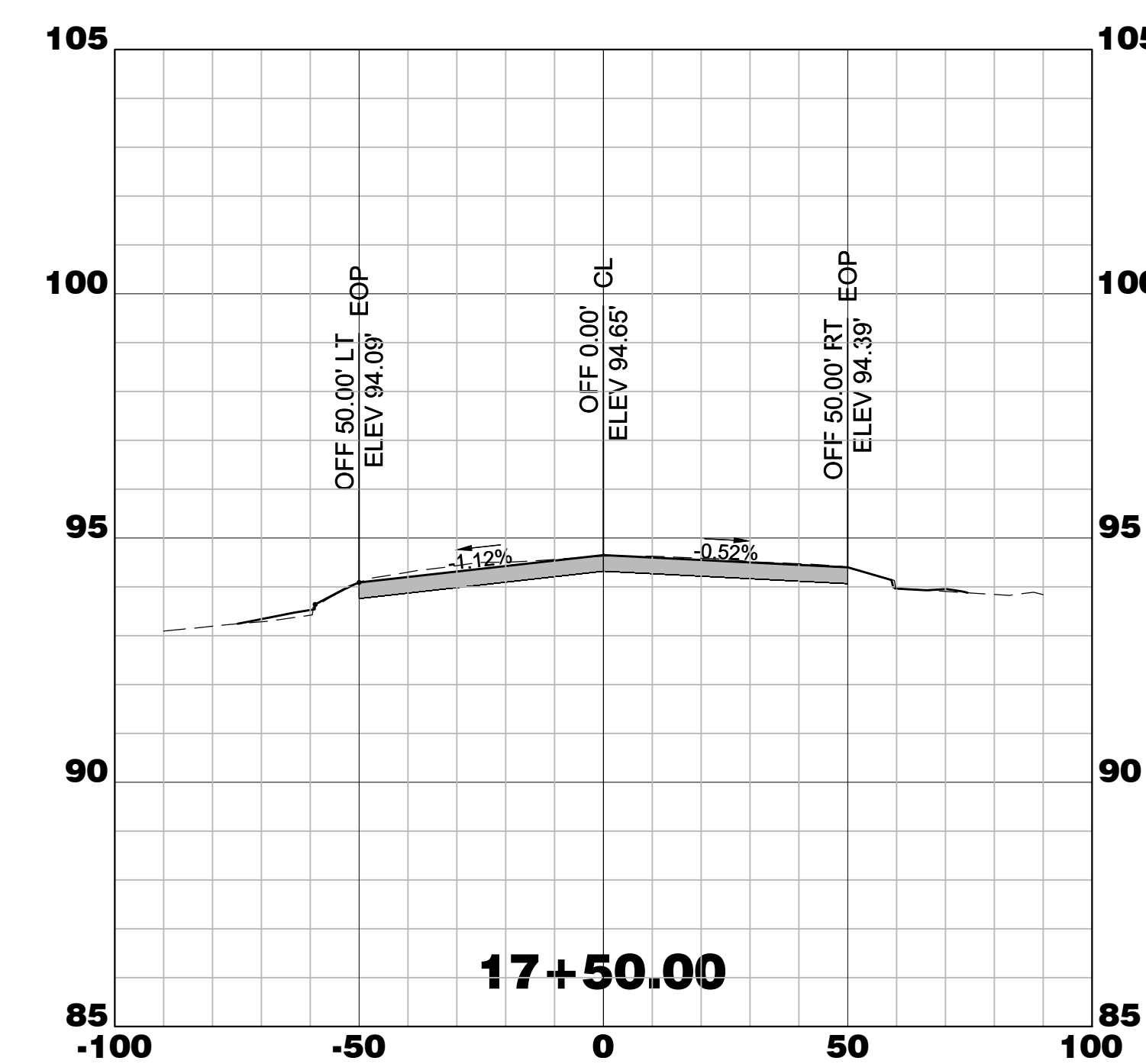
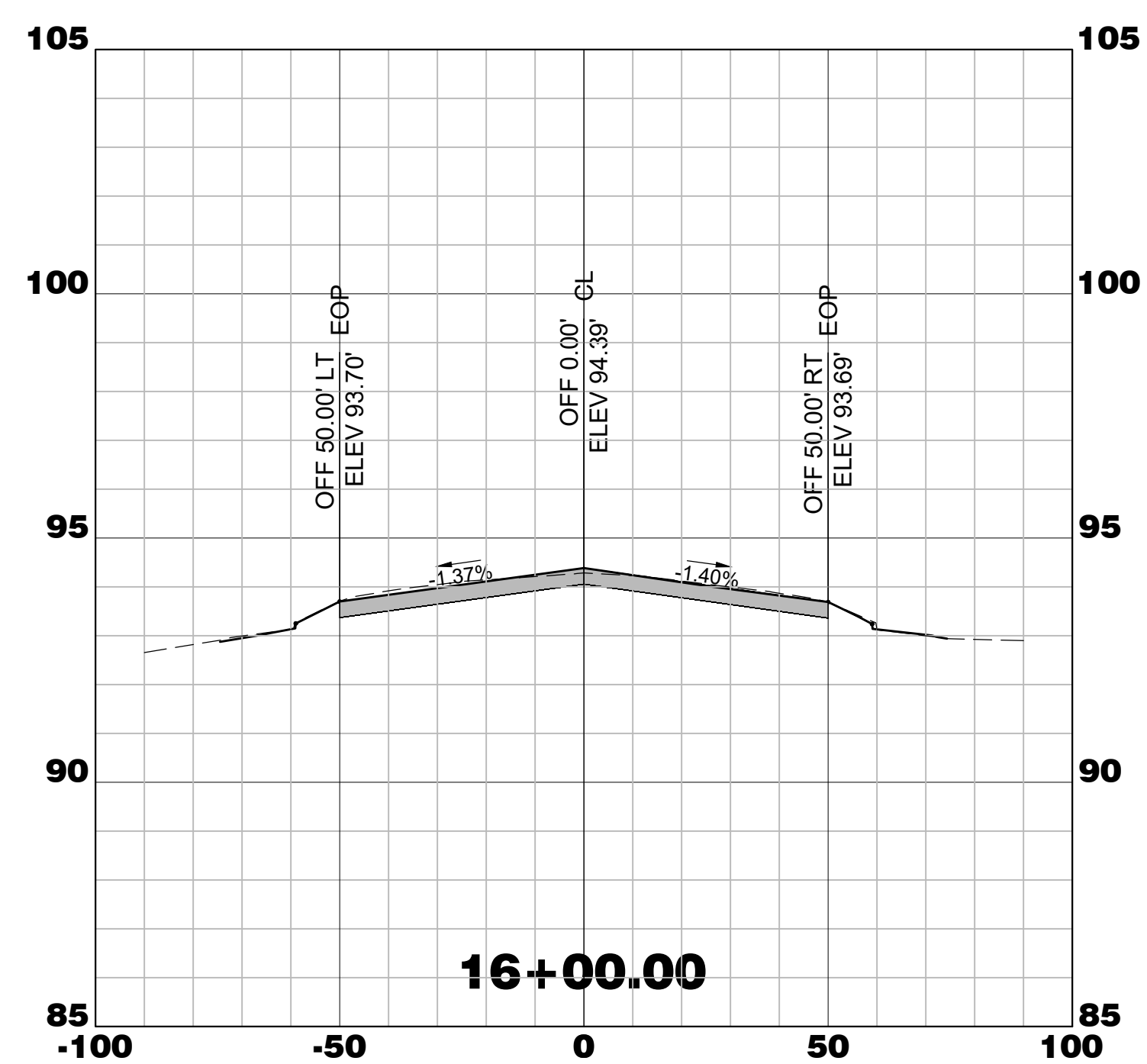
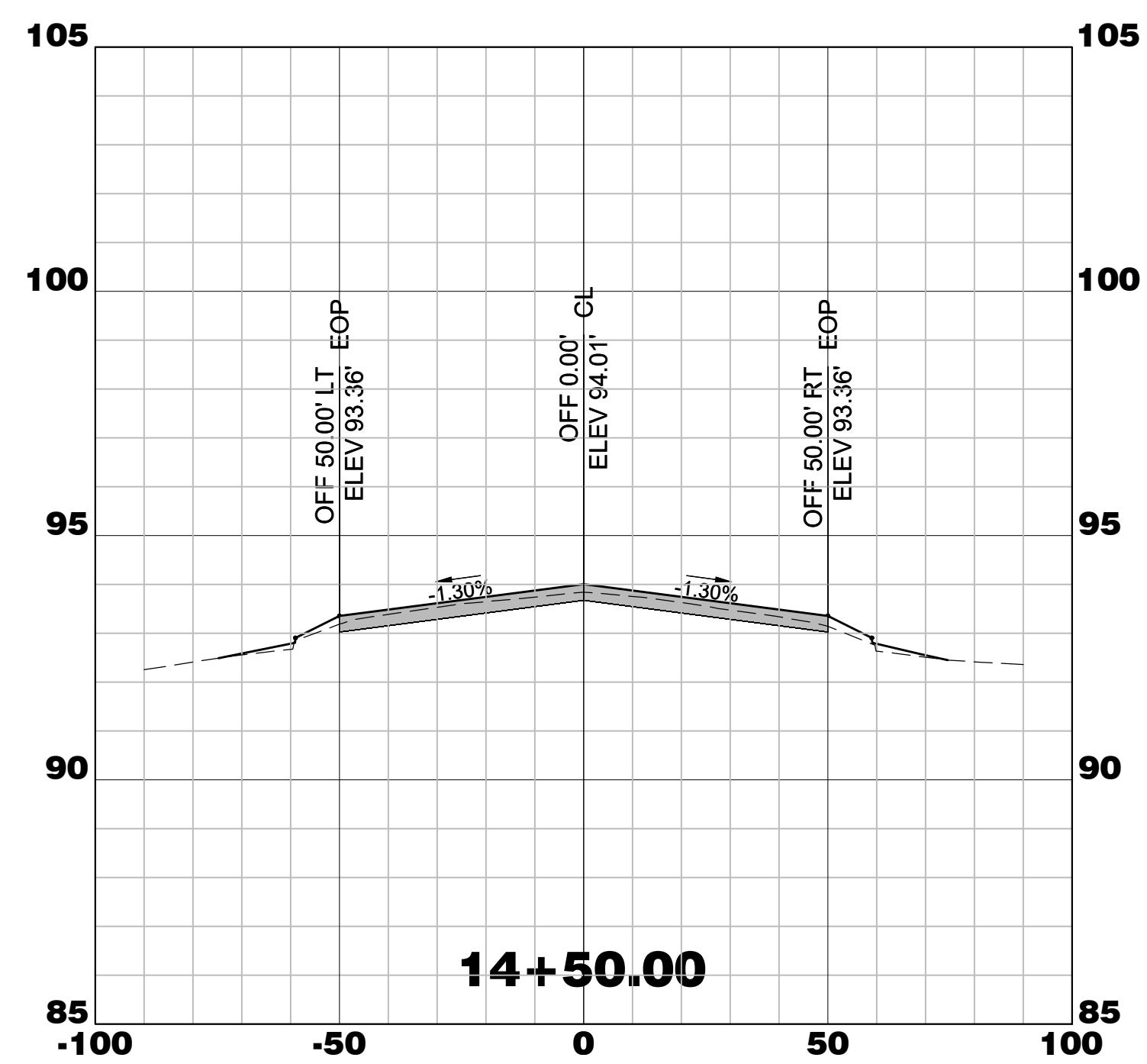
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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**RUNWAY 5-23
CROSS SECTIONS
STA. 13+50 TO 17+50**

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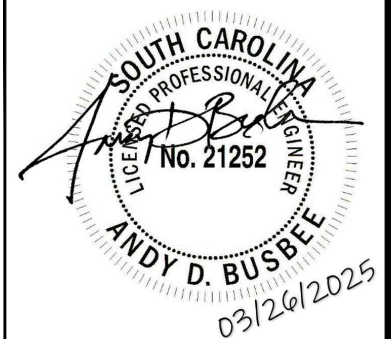


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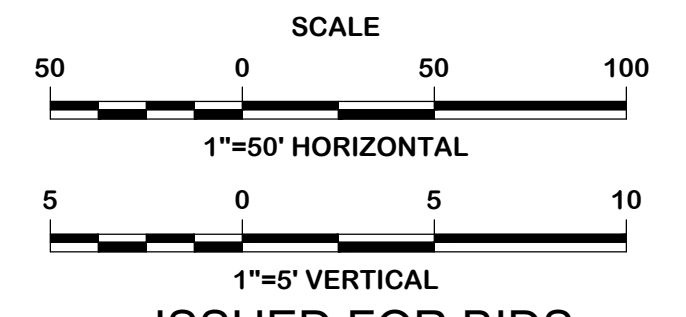
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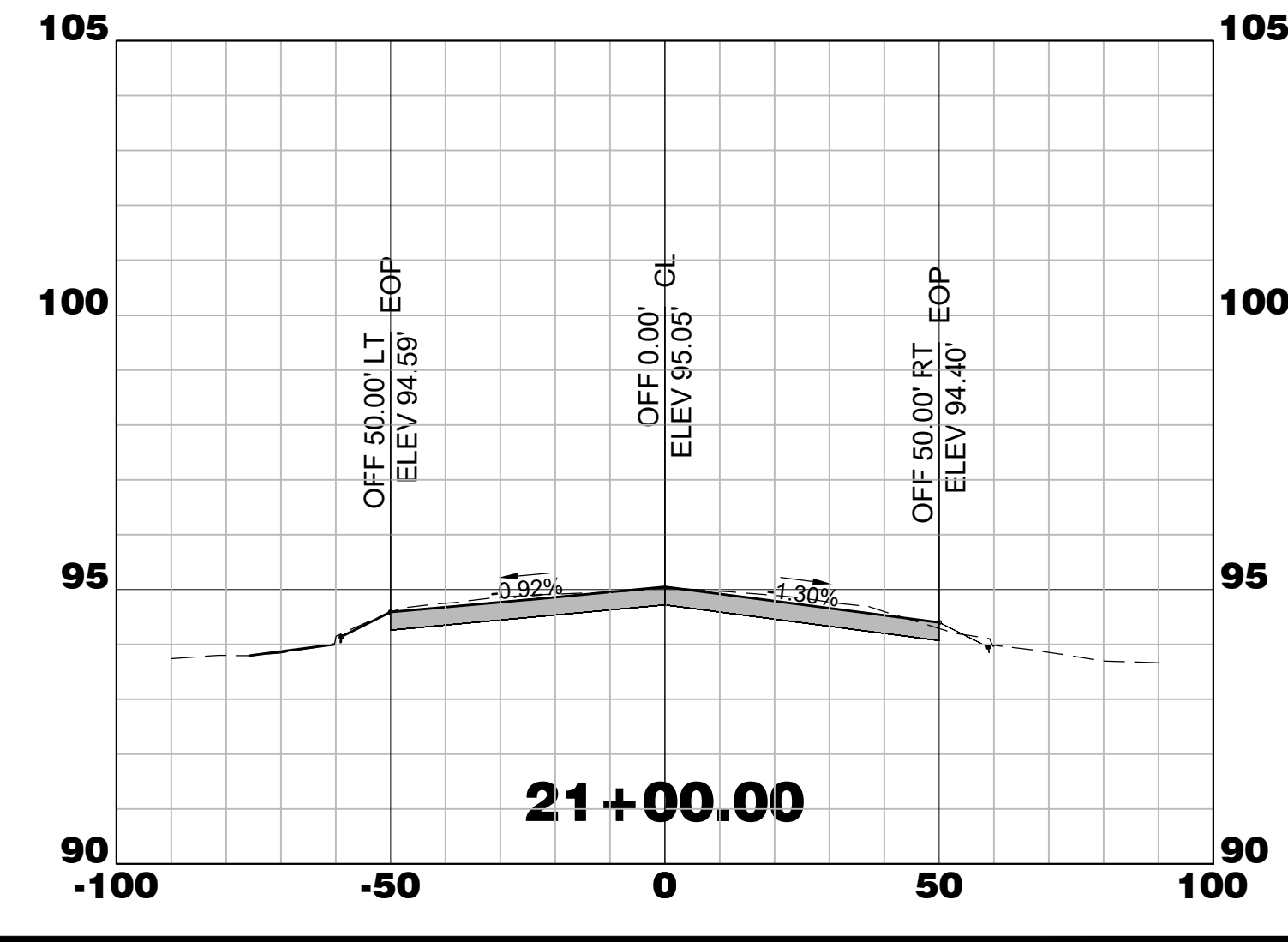
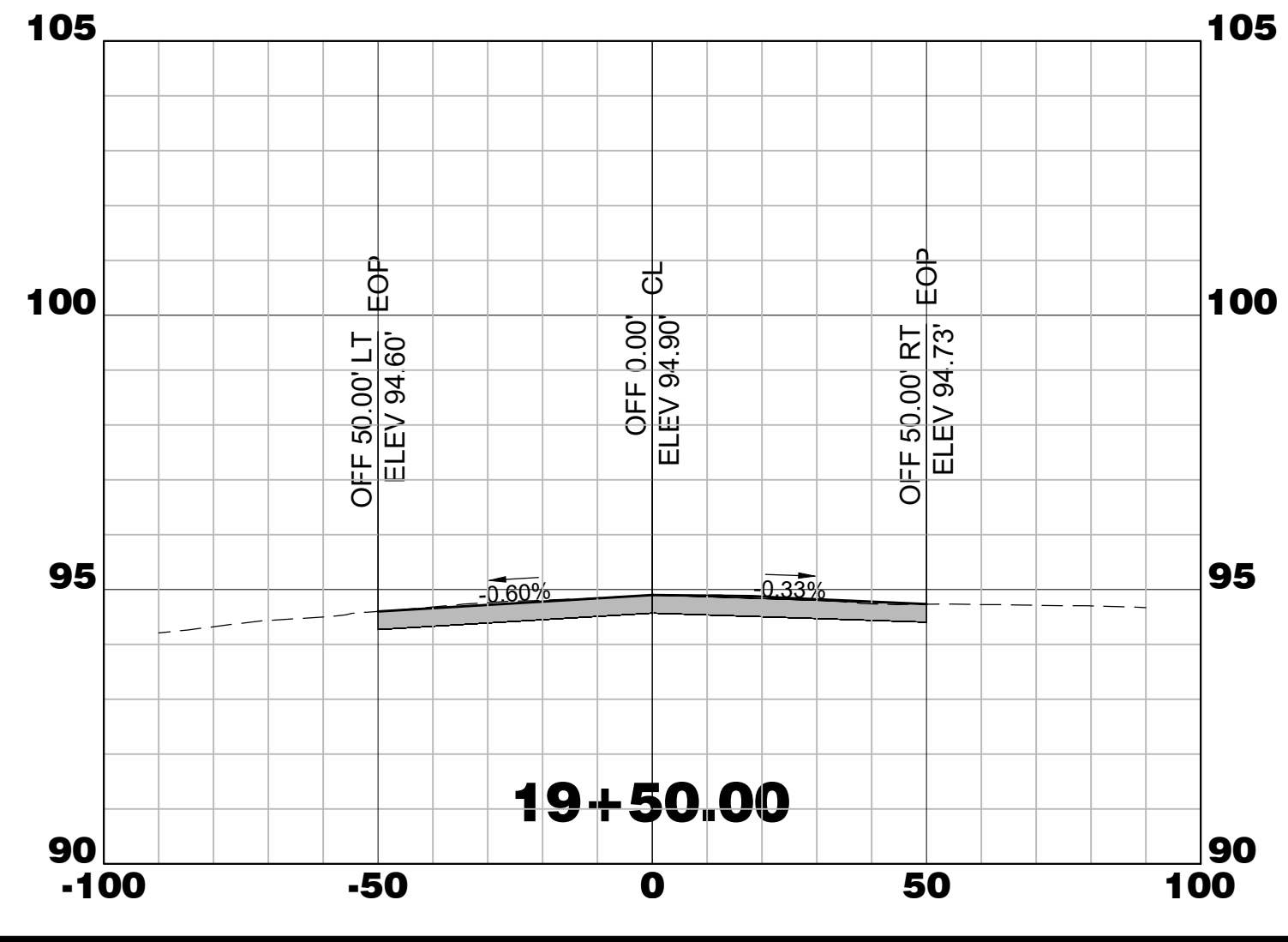
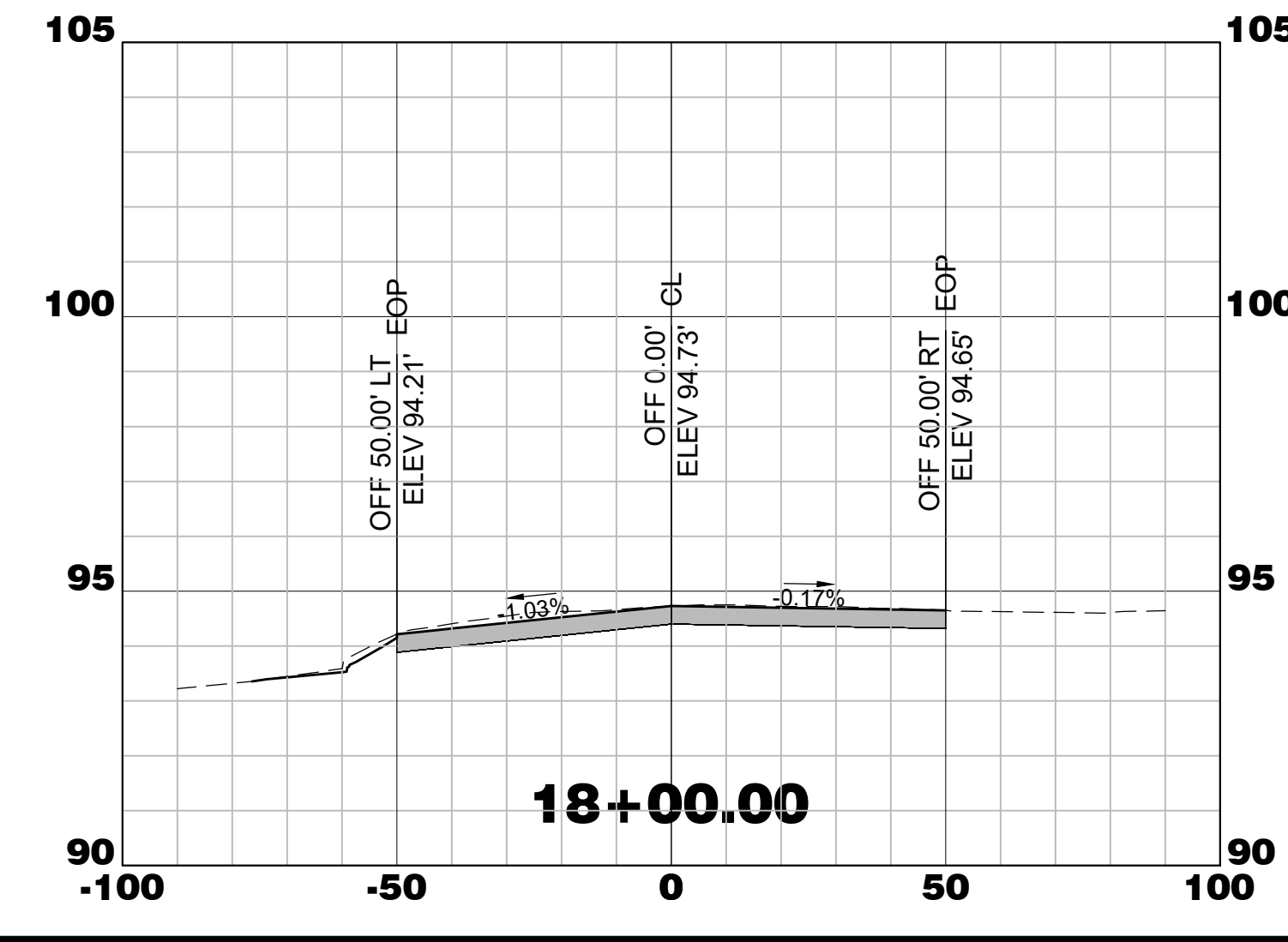
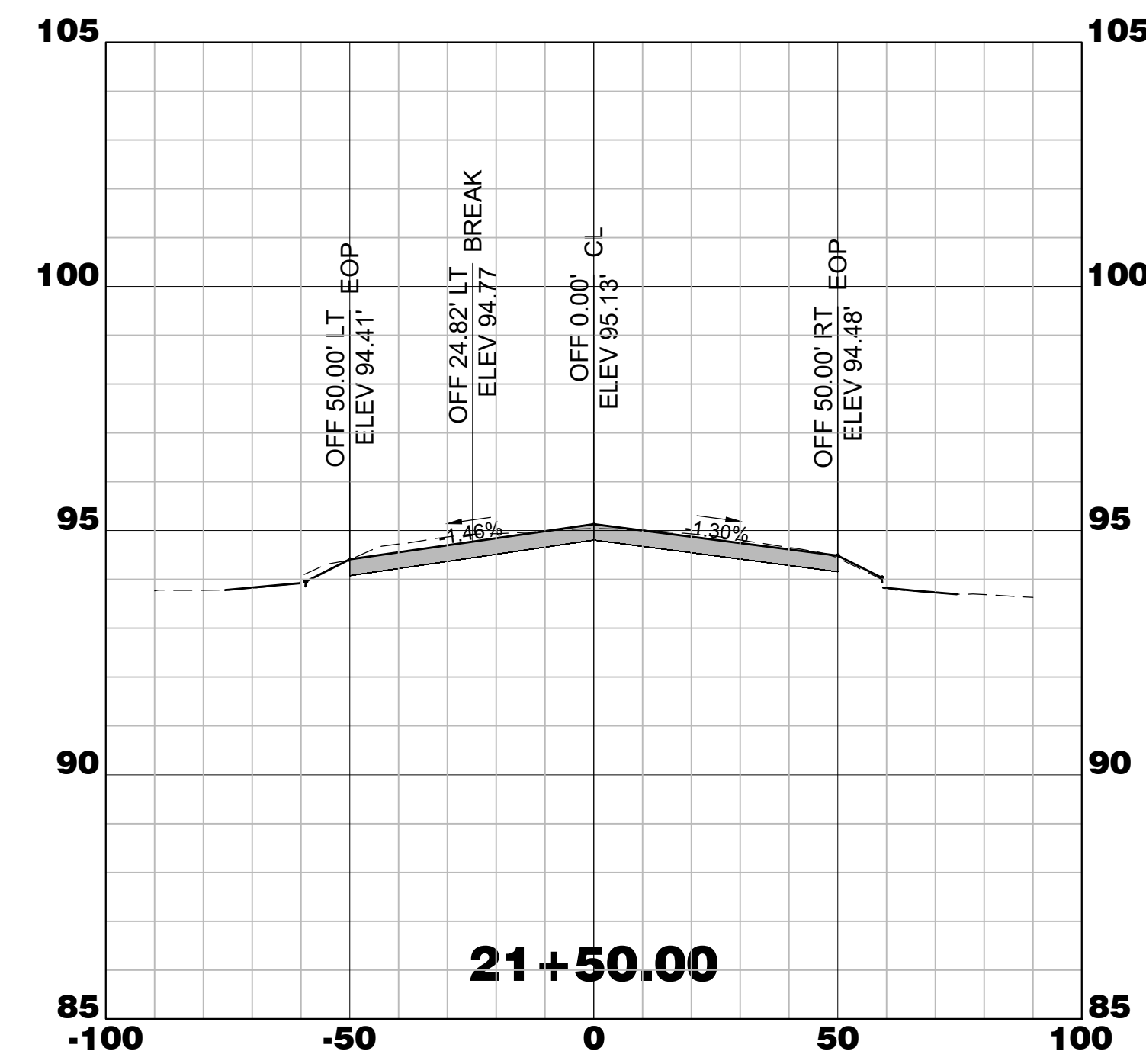
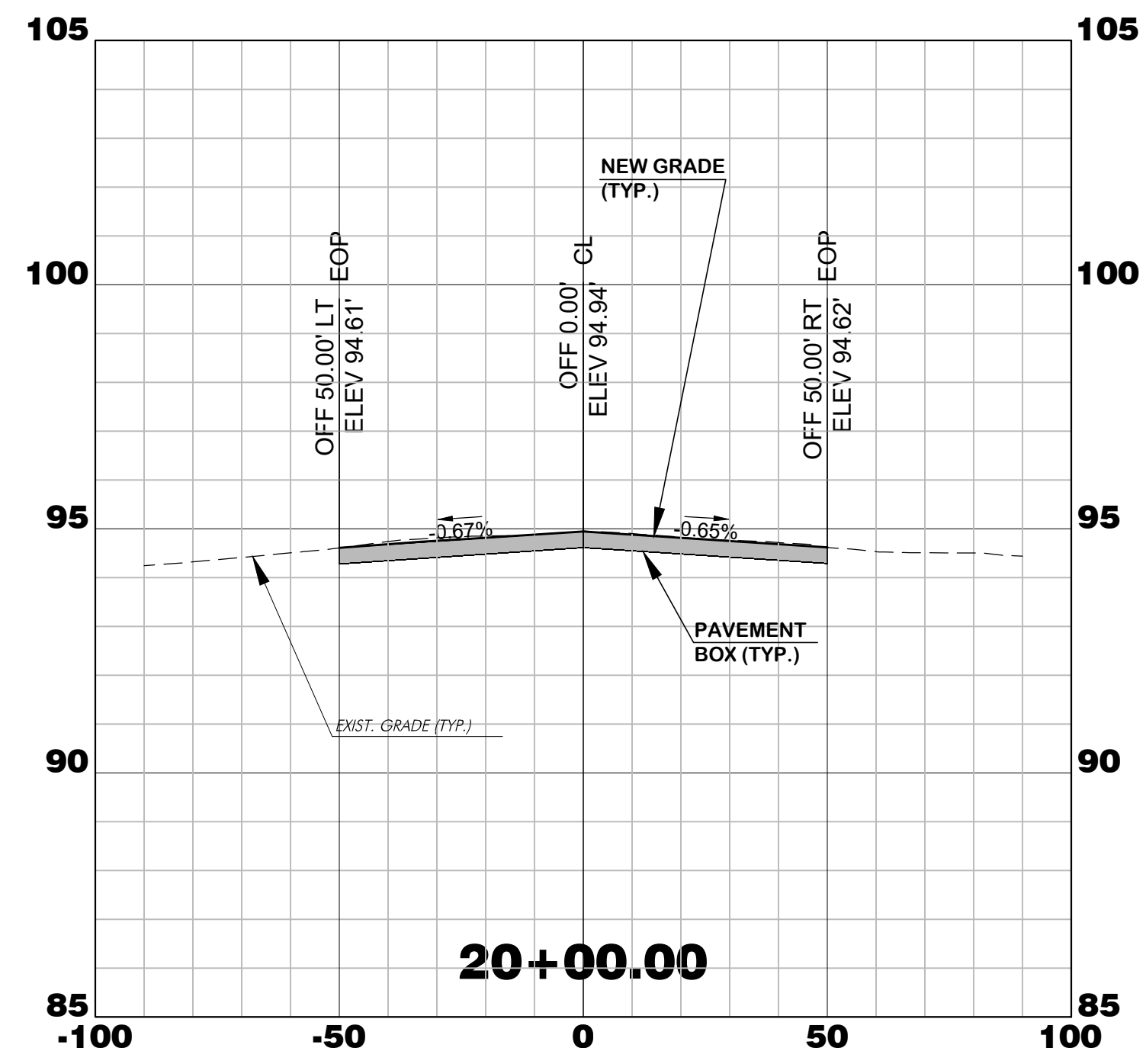
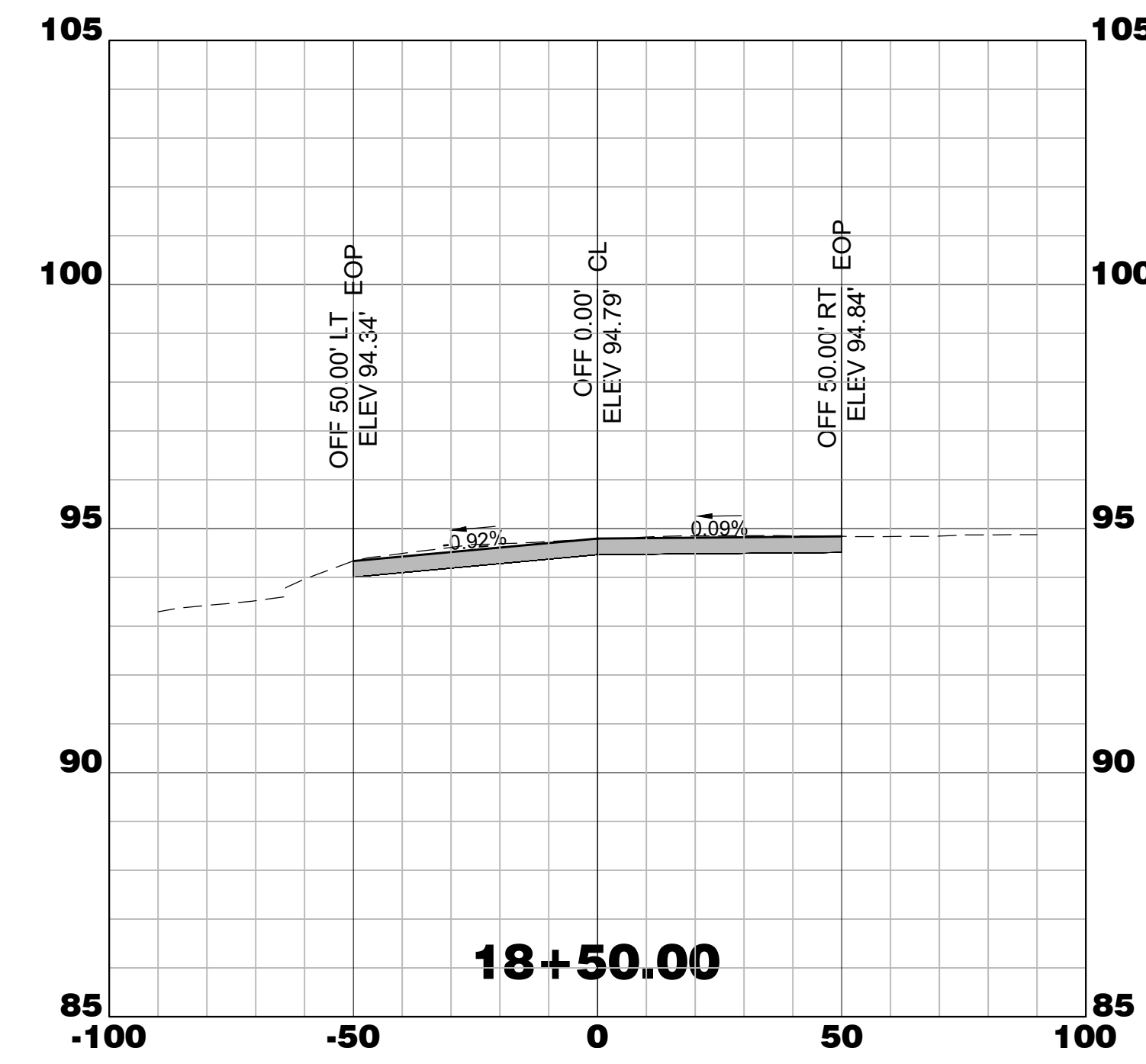
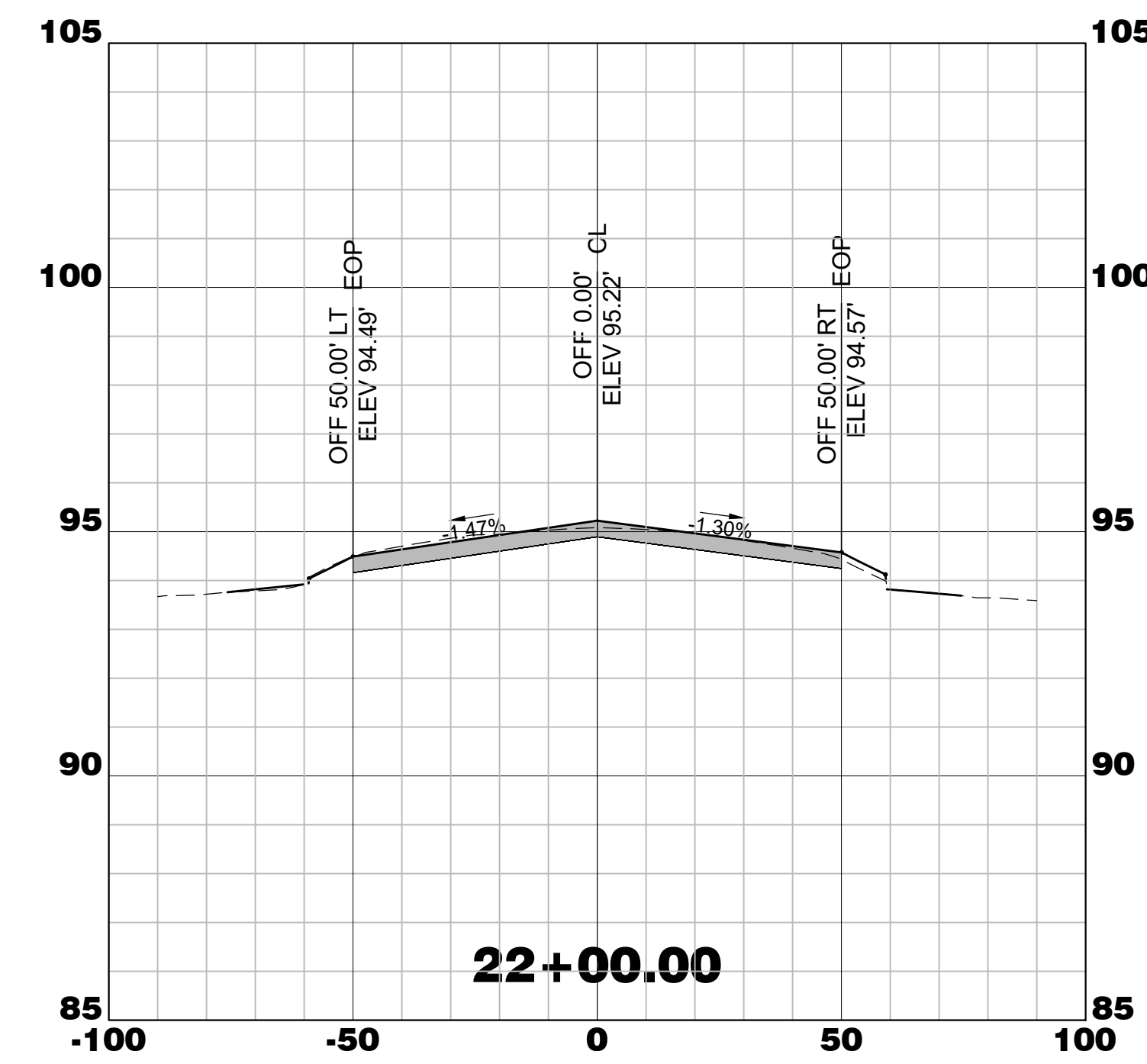
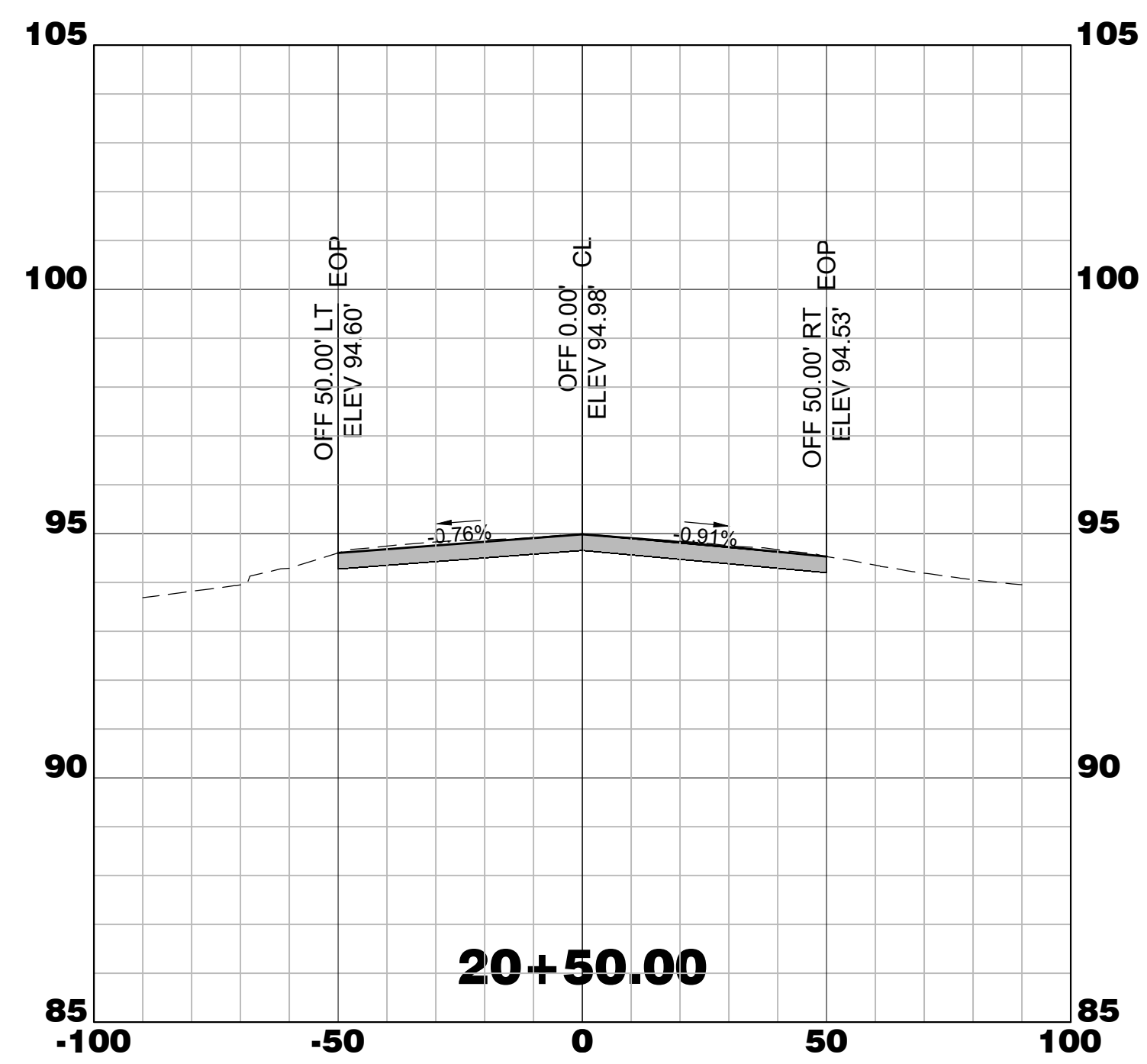
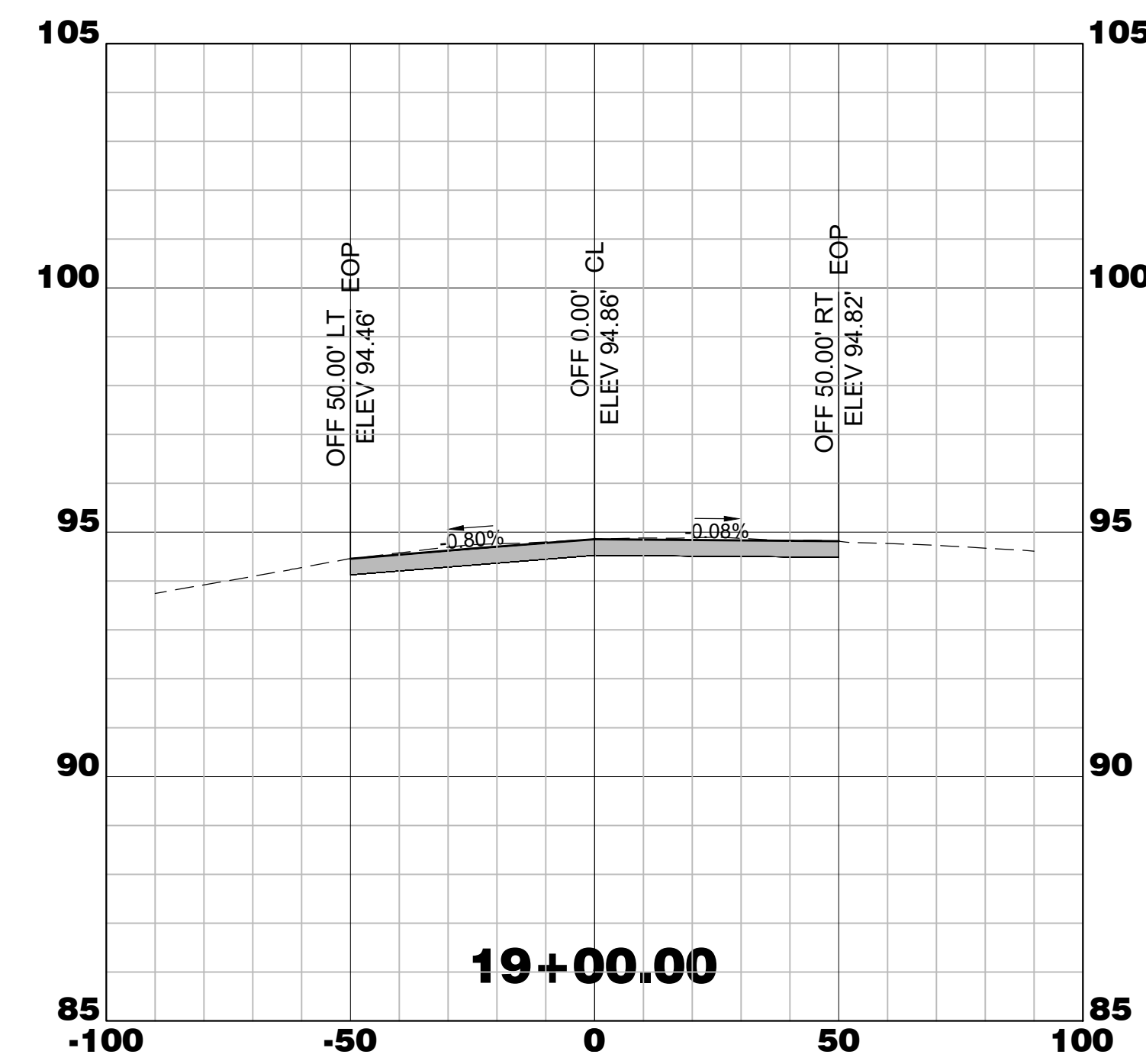
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**RUNWAY 5-23
CROSS SECTIONS
STA. 19+00 TO 22+00**

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ. 1"=5' VERT.	Sheet Number: X-4



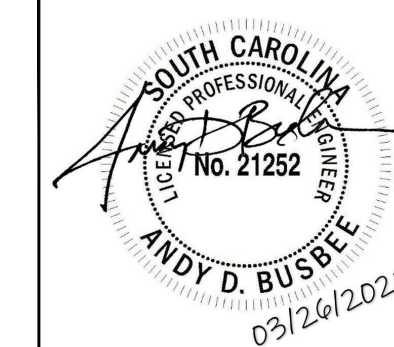
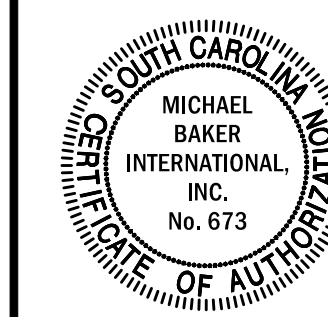
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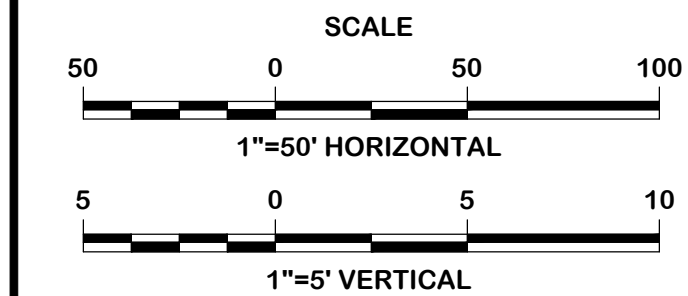
WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

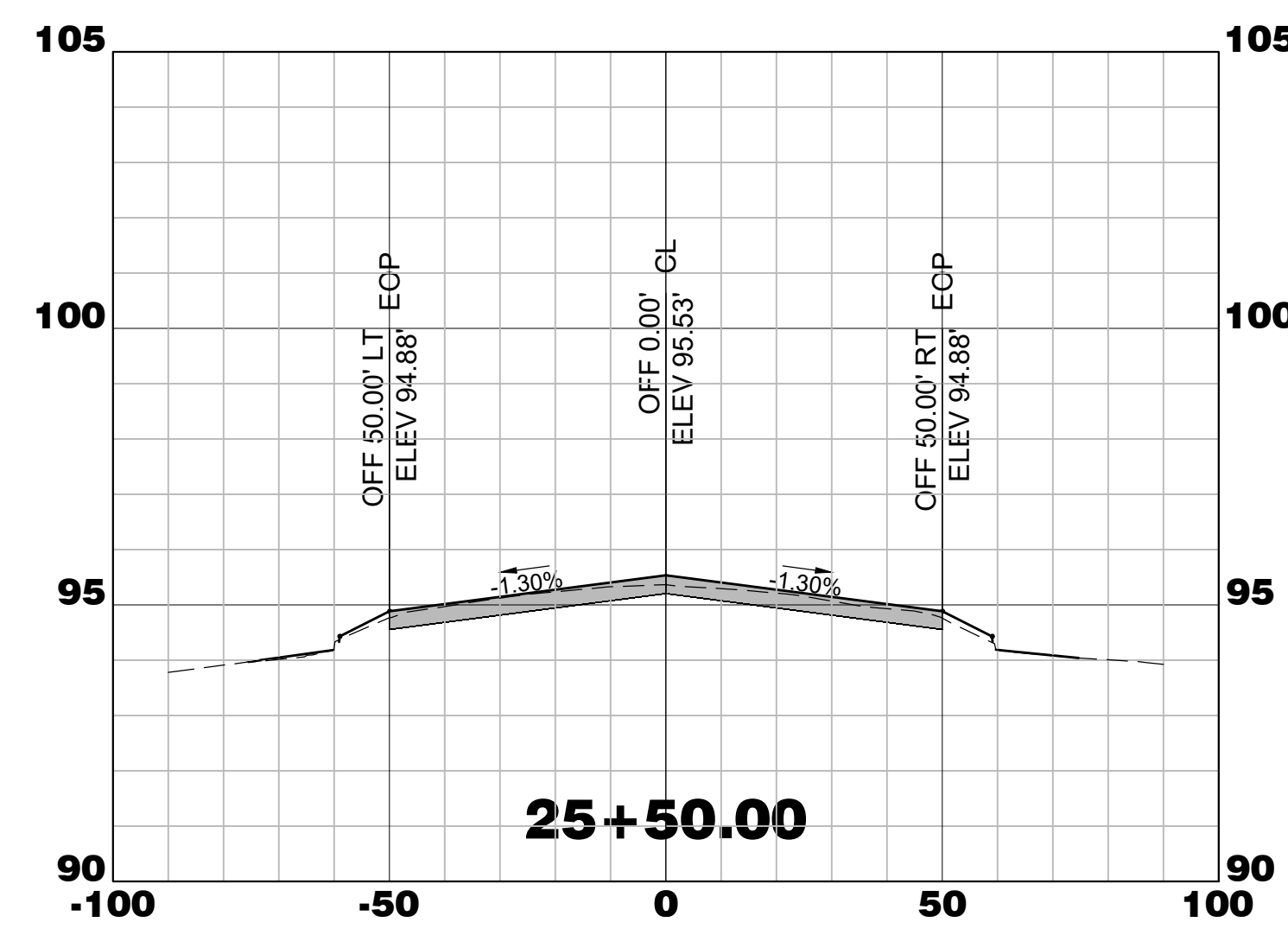
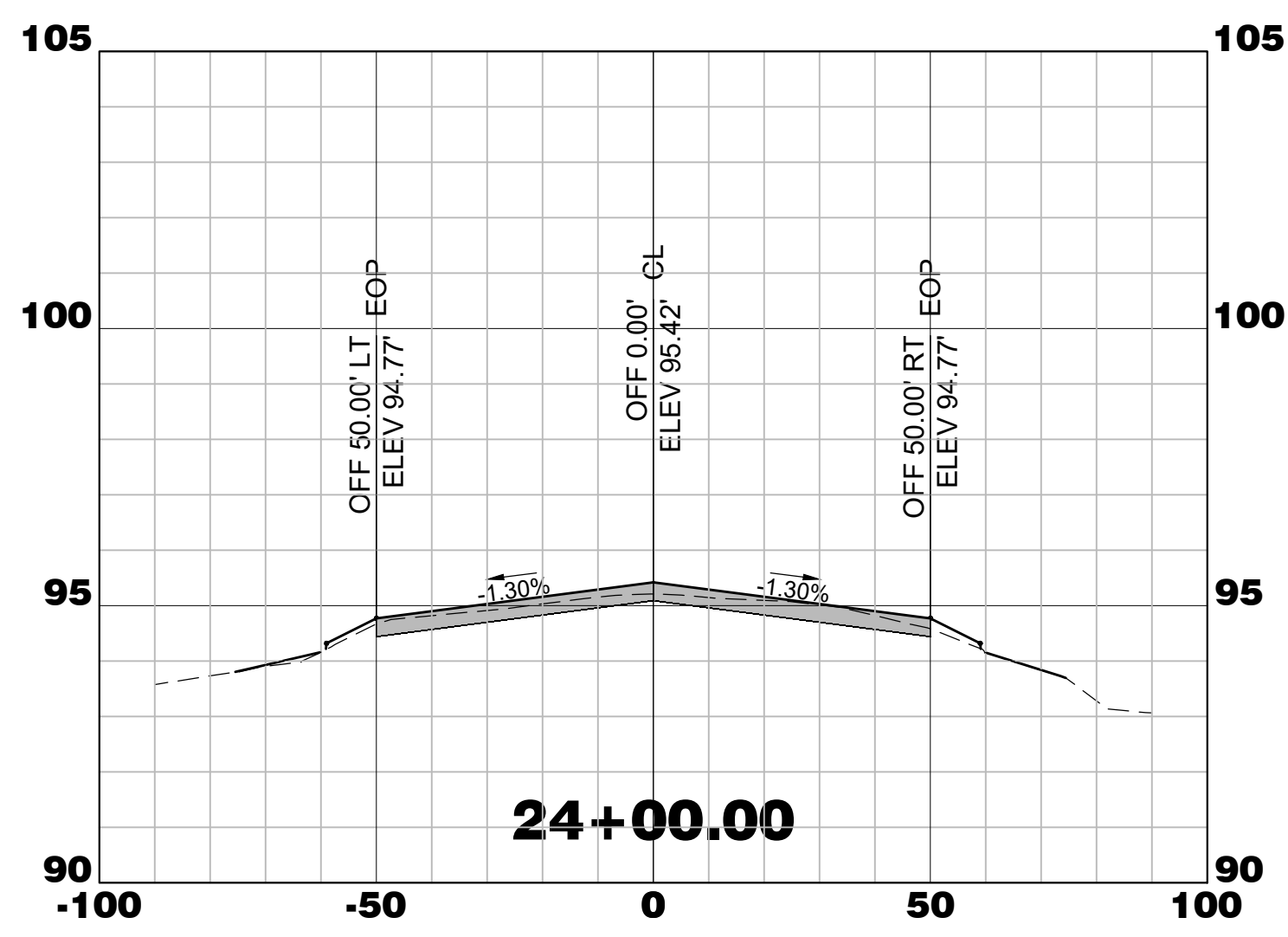
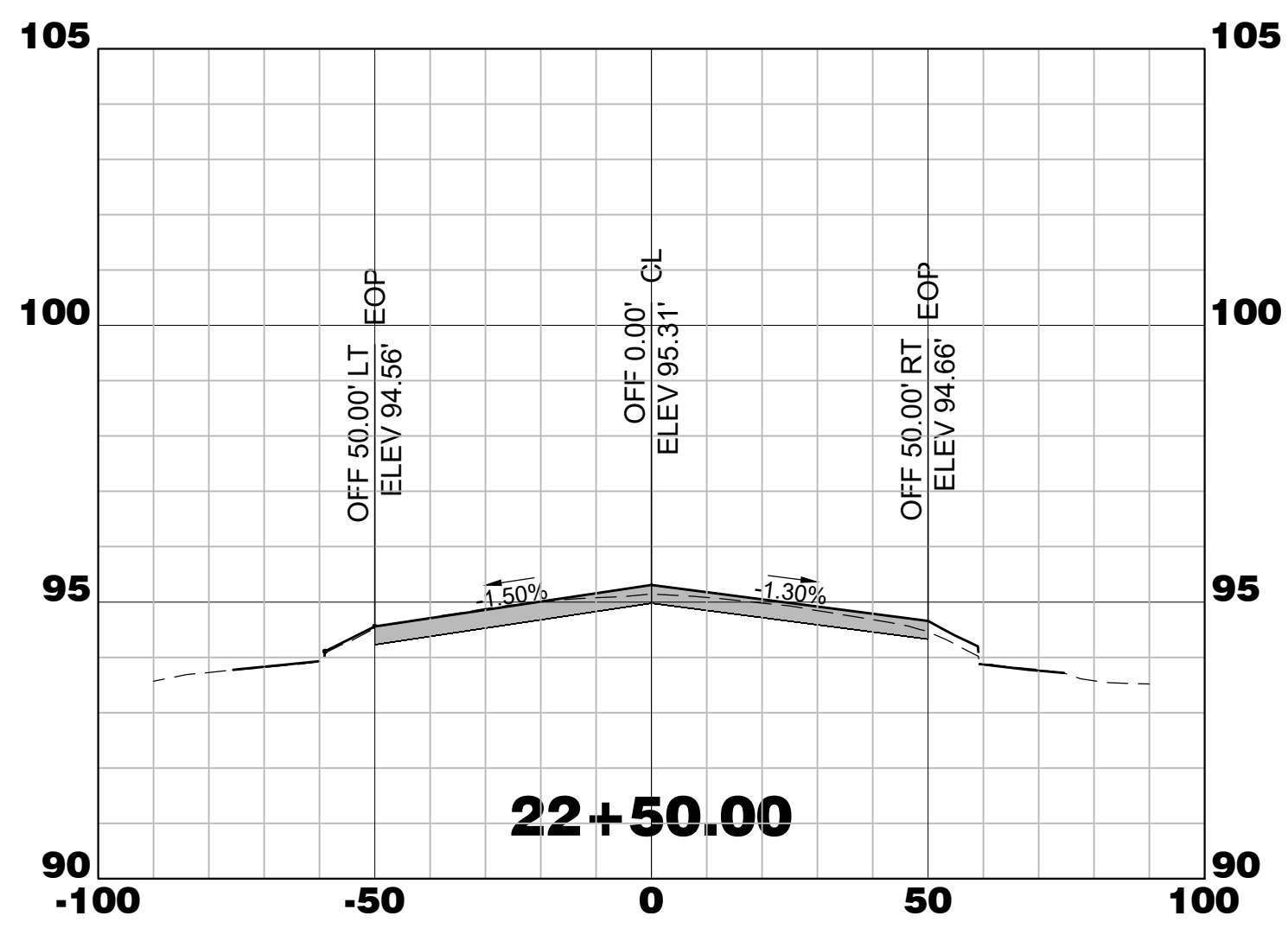
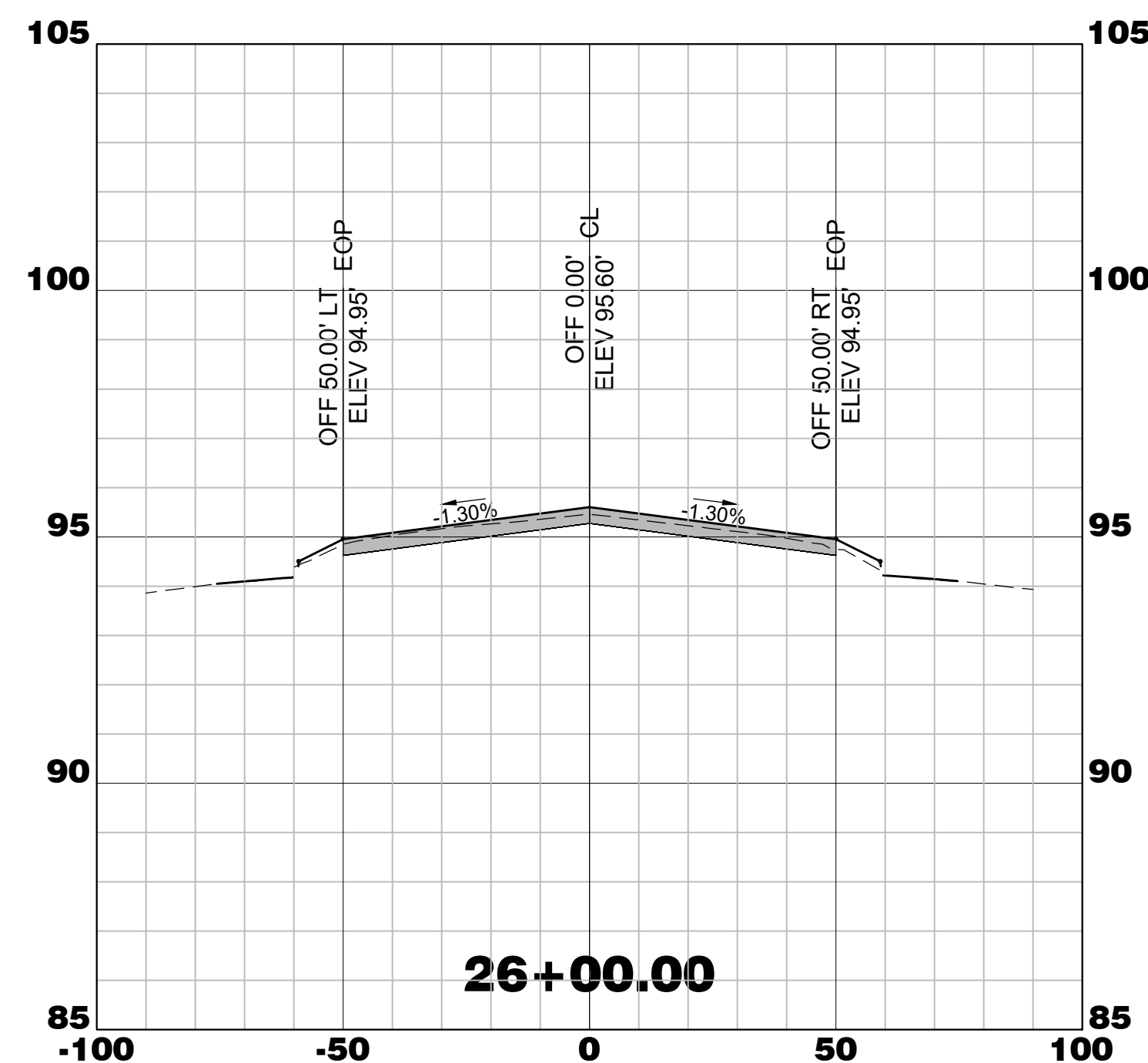
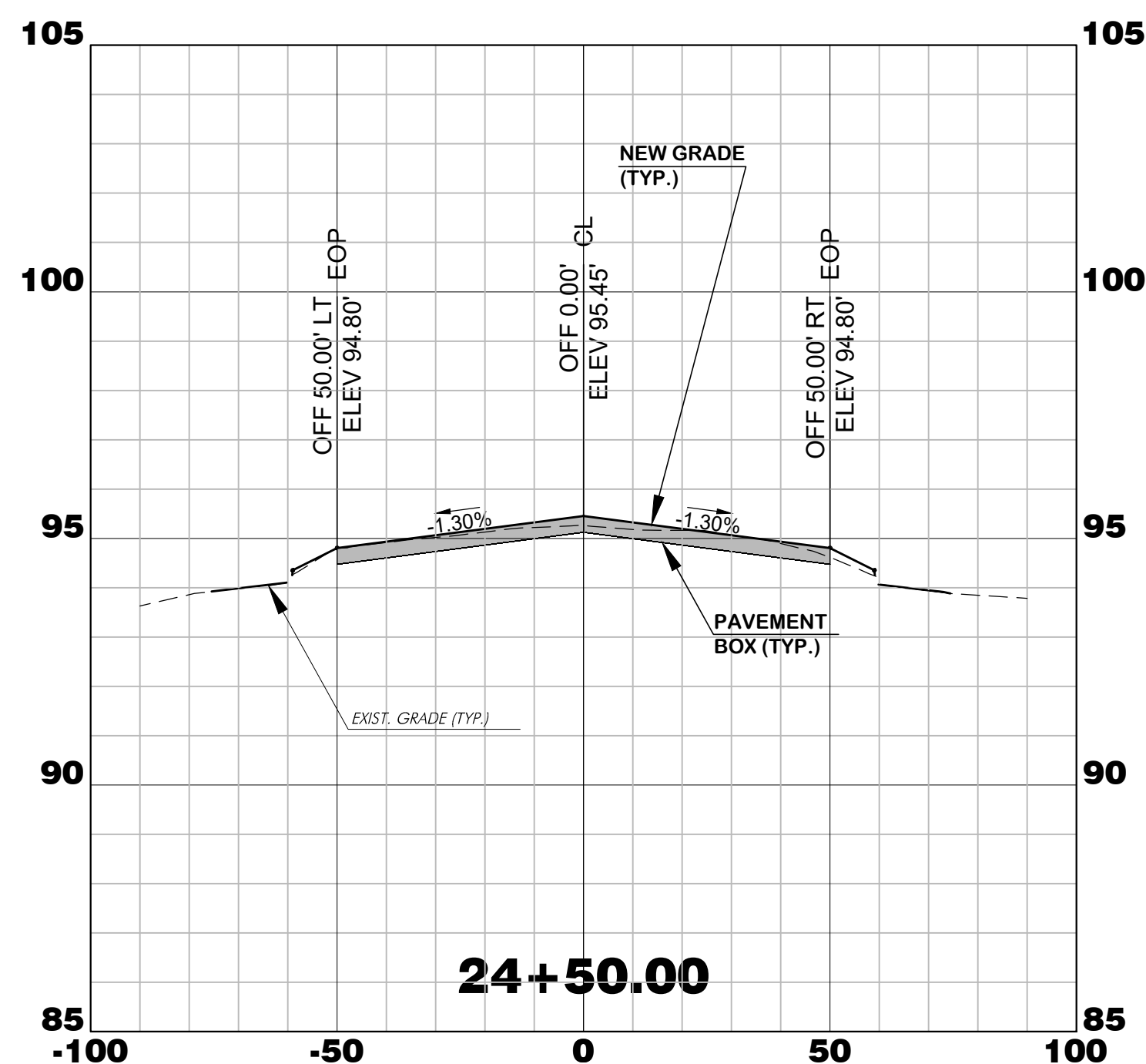
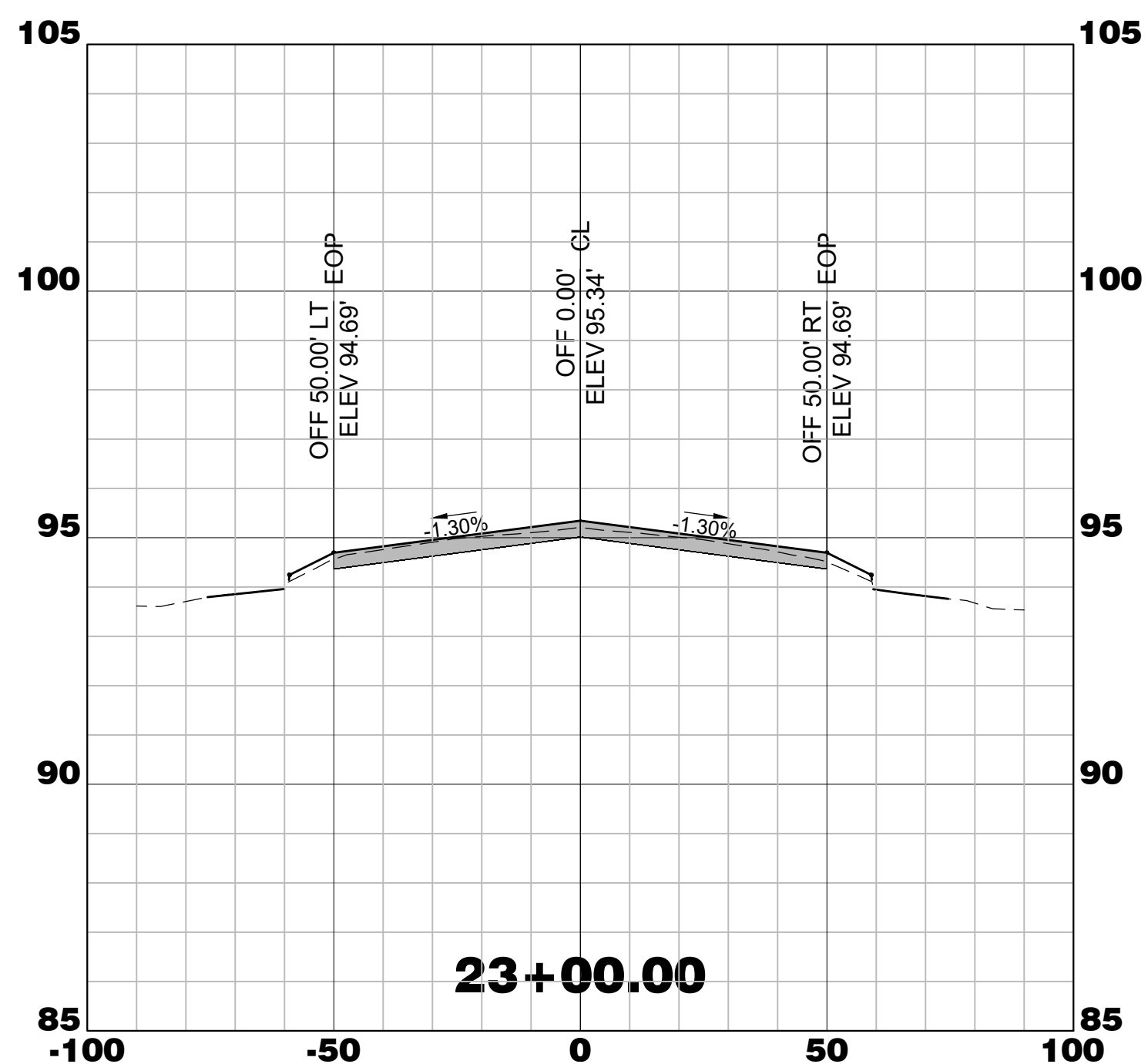
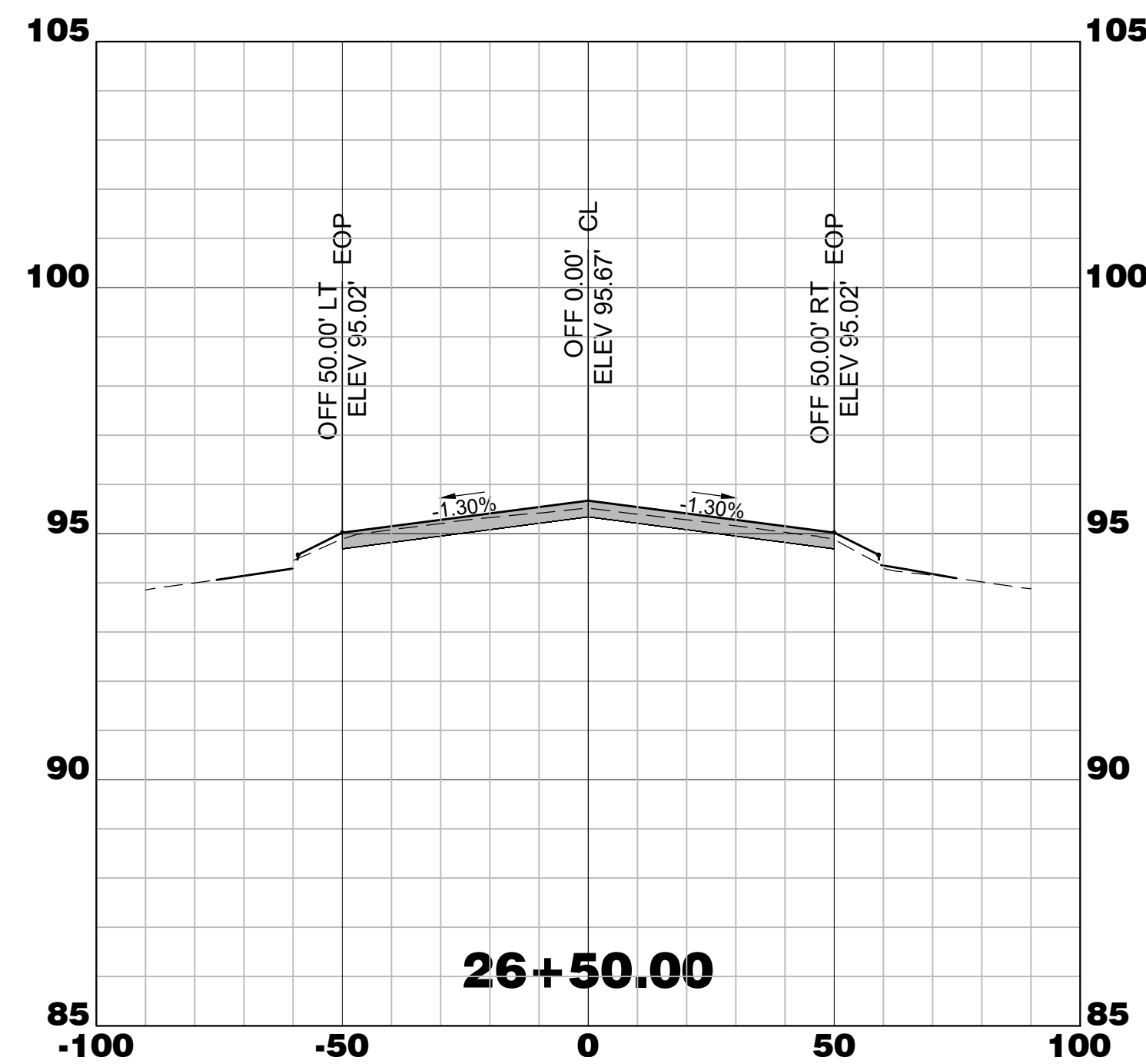
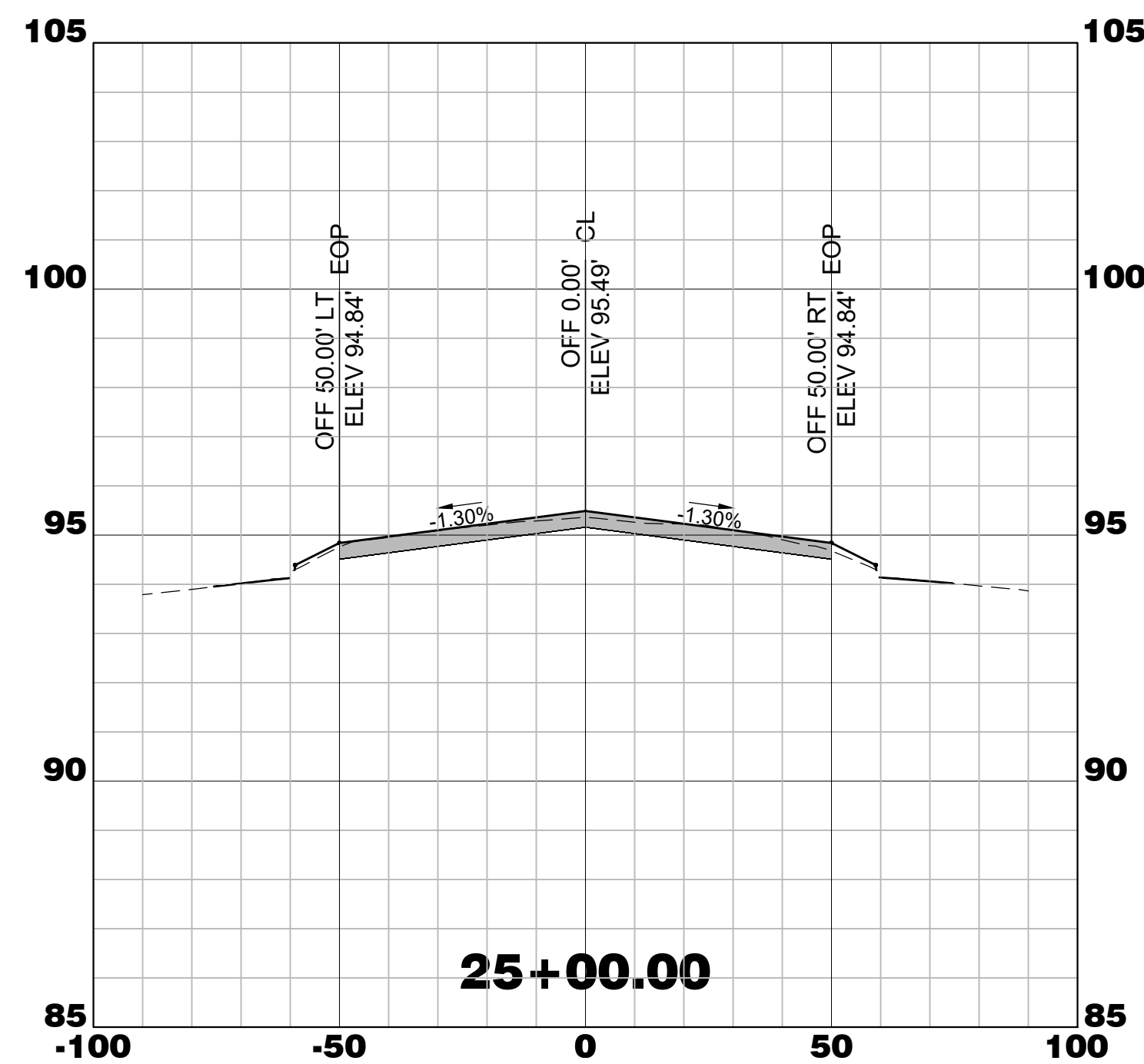
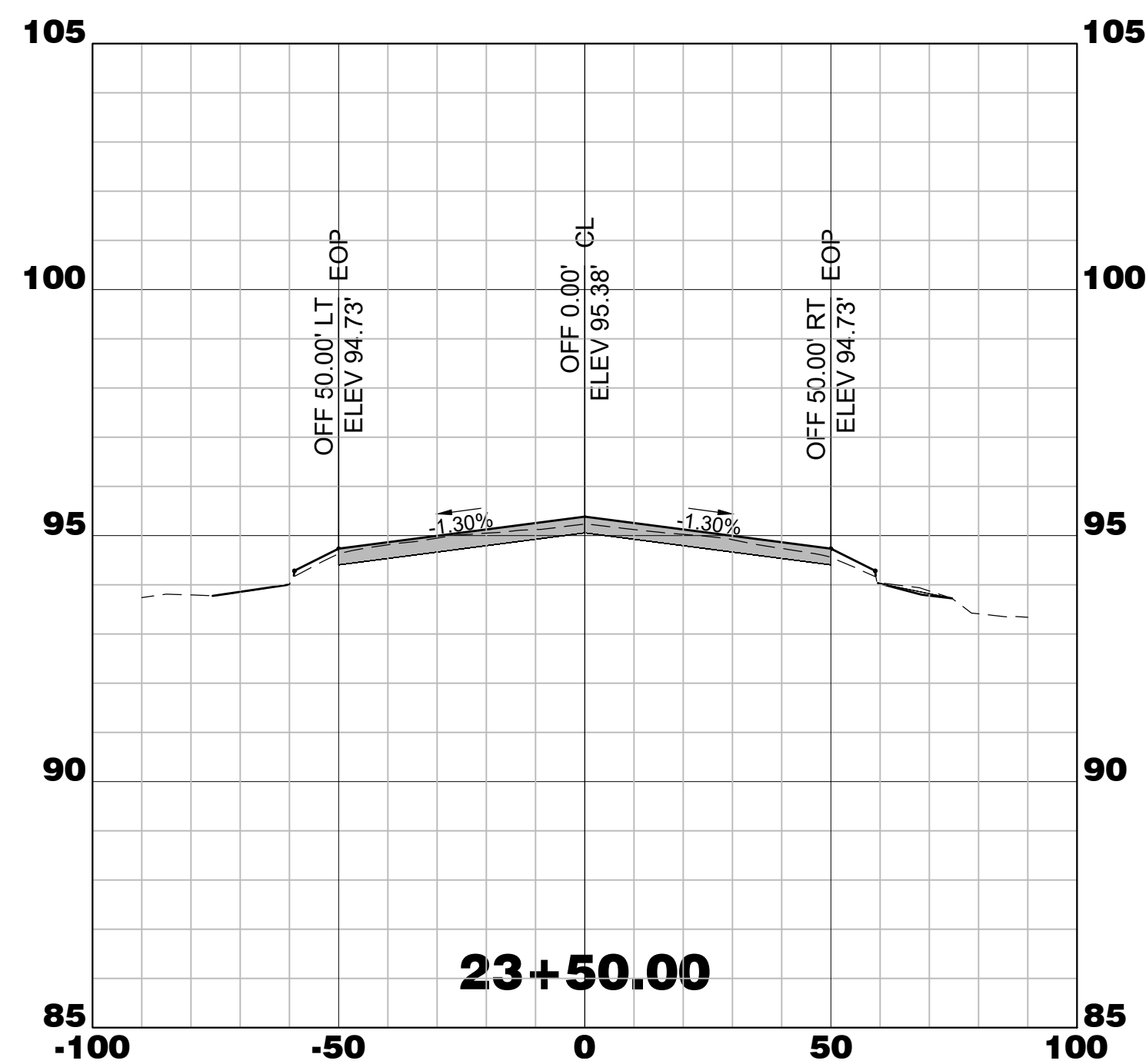
Drawing Name:
**RUNWAY 5-23
CROSS SECTIONS
STA. 22+20 TO 26+50**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: AIRPORTS
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Scale: 1"=50 HORIZ.	Sheet Number:
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Scale: 1"=5' VERT.	Drawing Number: X-5
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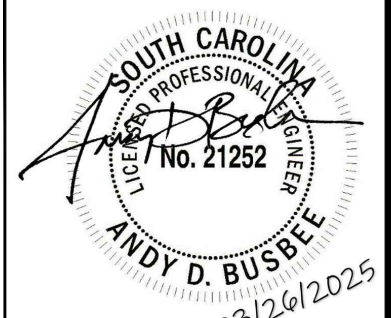


WALTERBORO, SOUTH CAROLINA

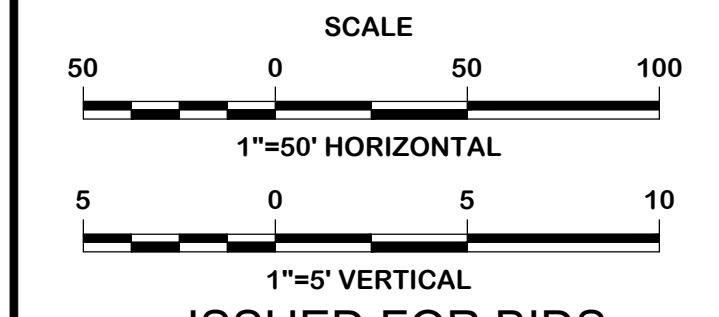
Michael Baker

INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

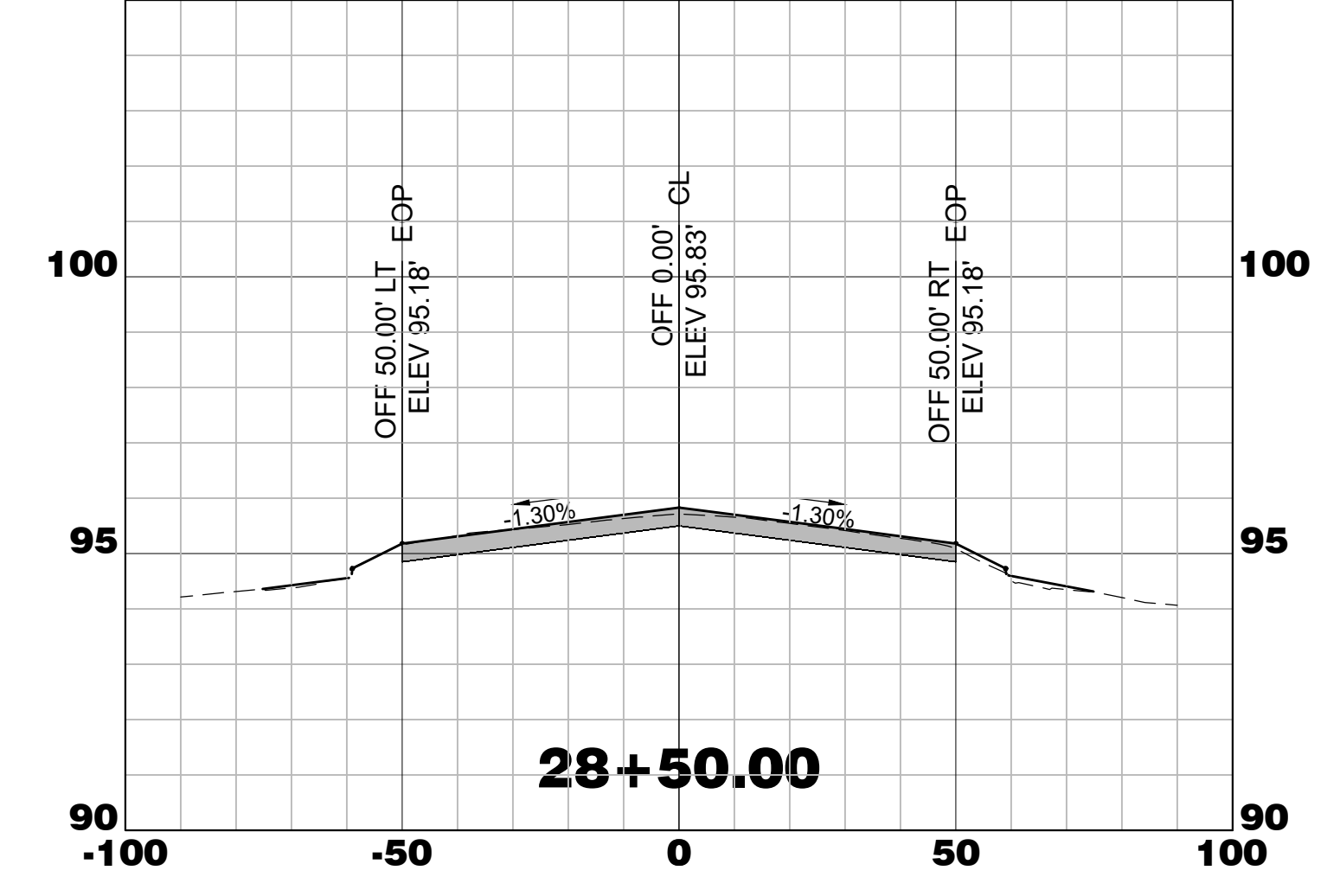
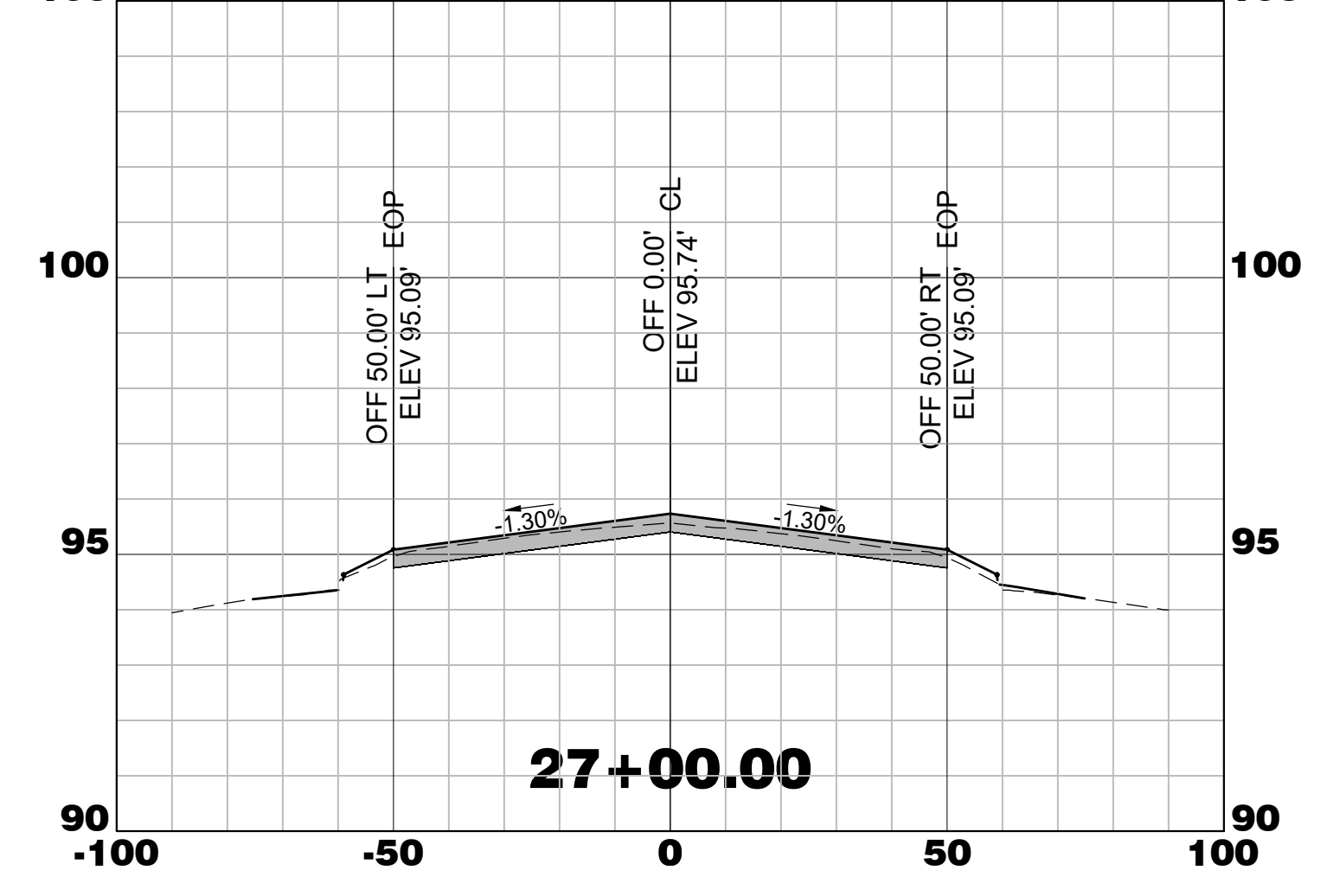
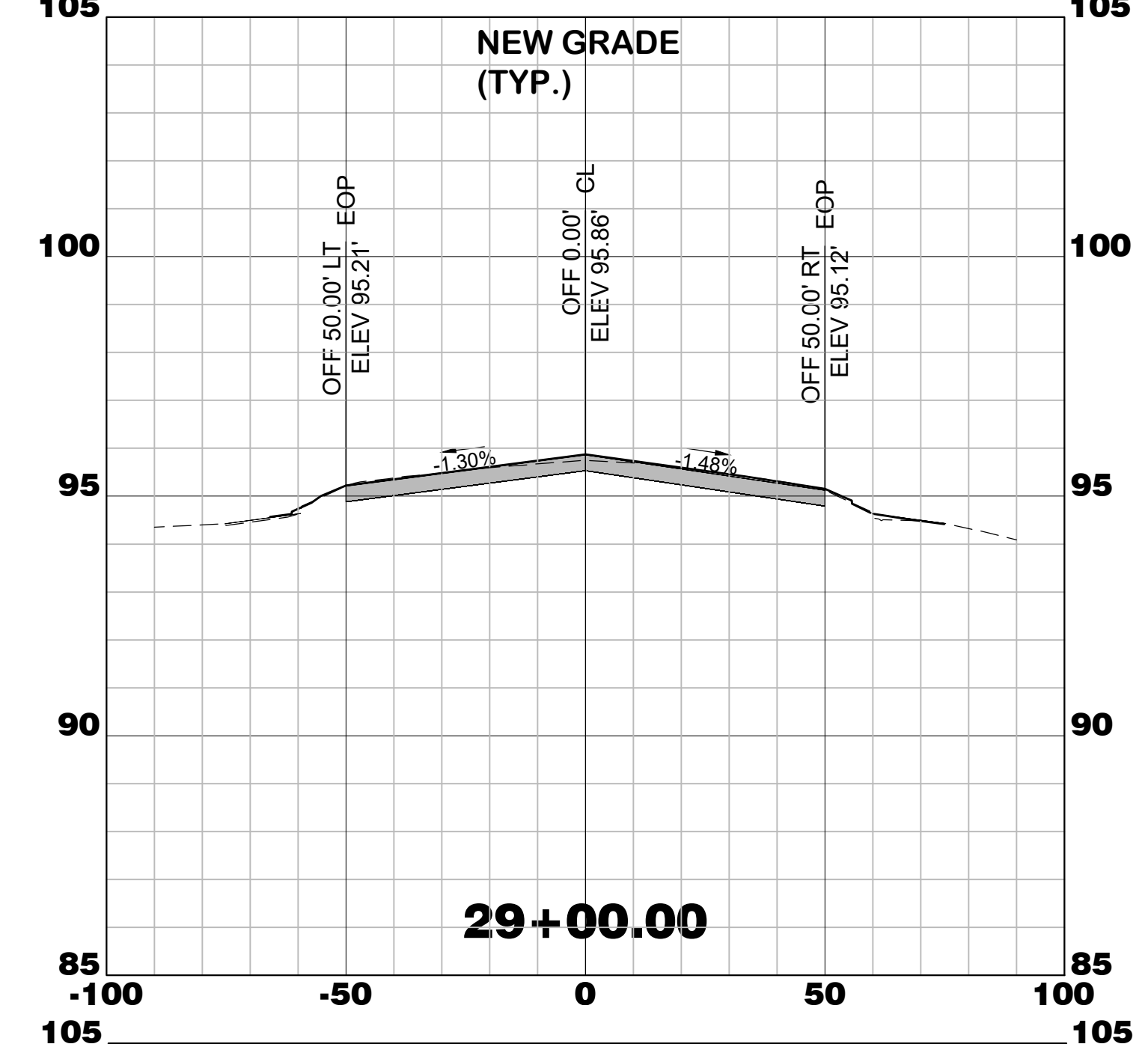
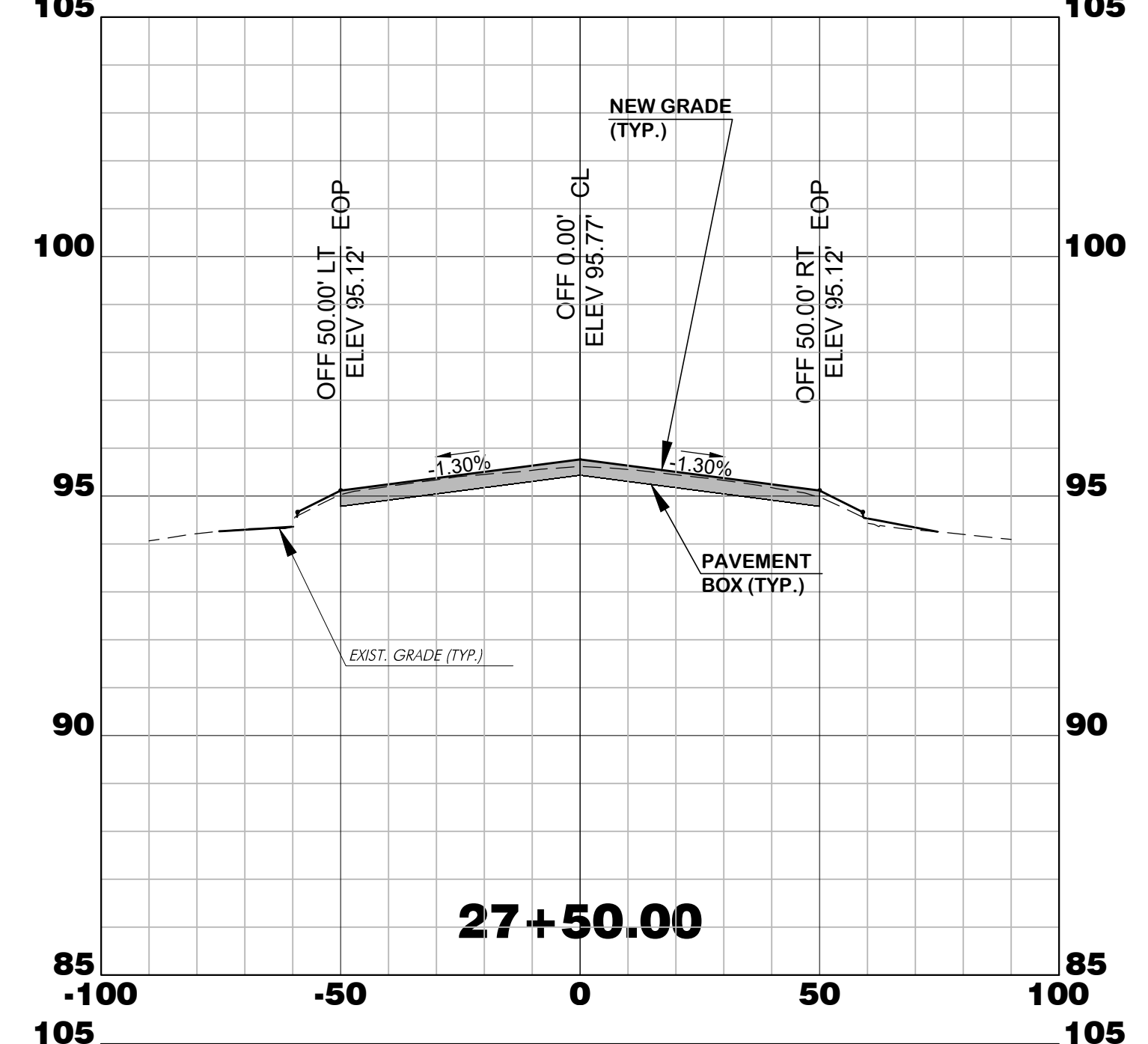
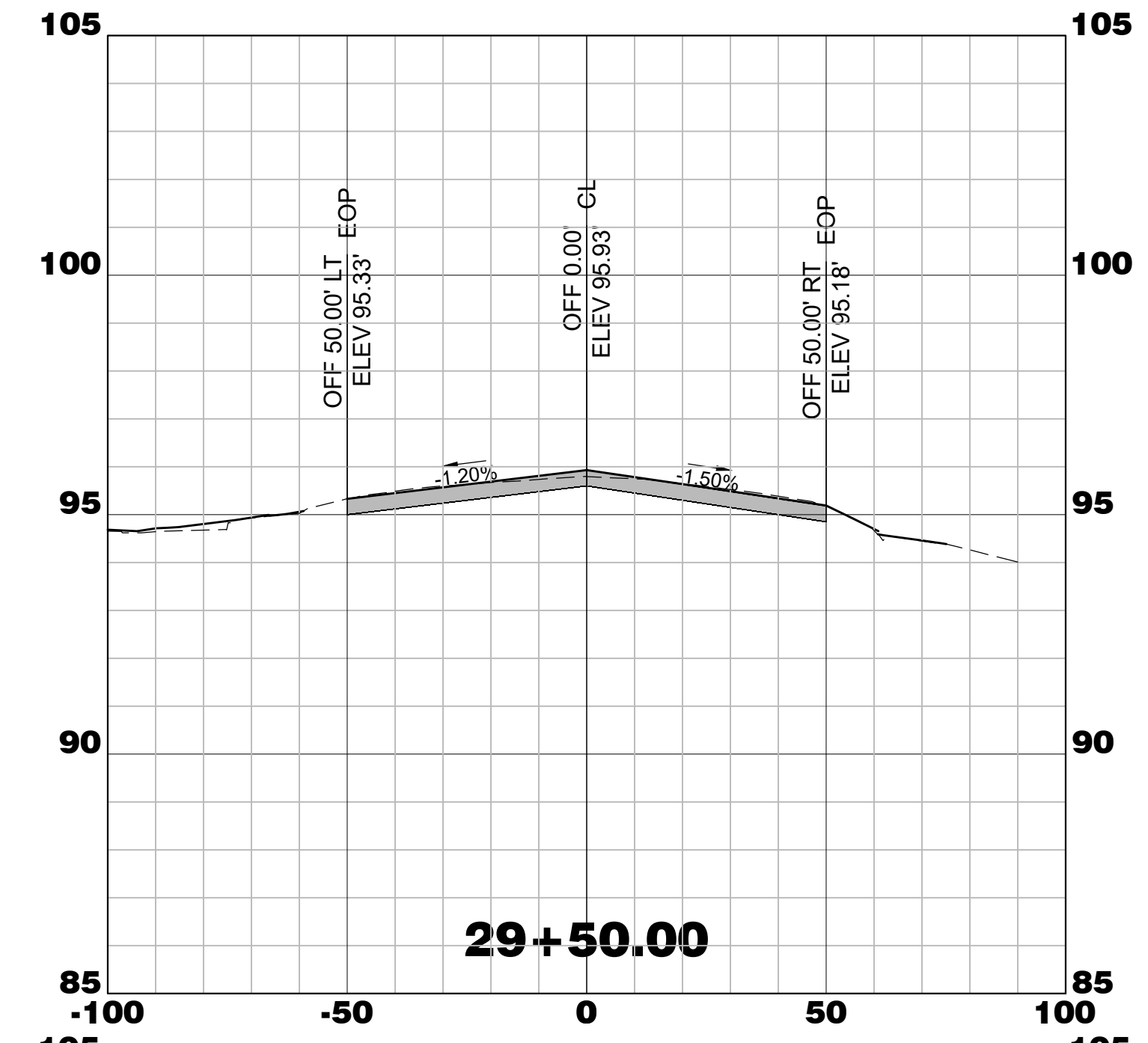
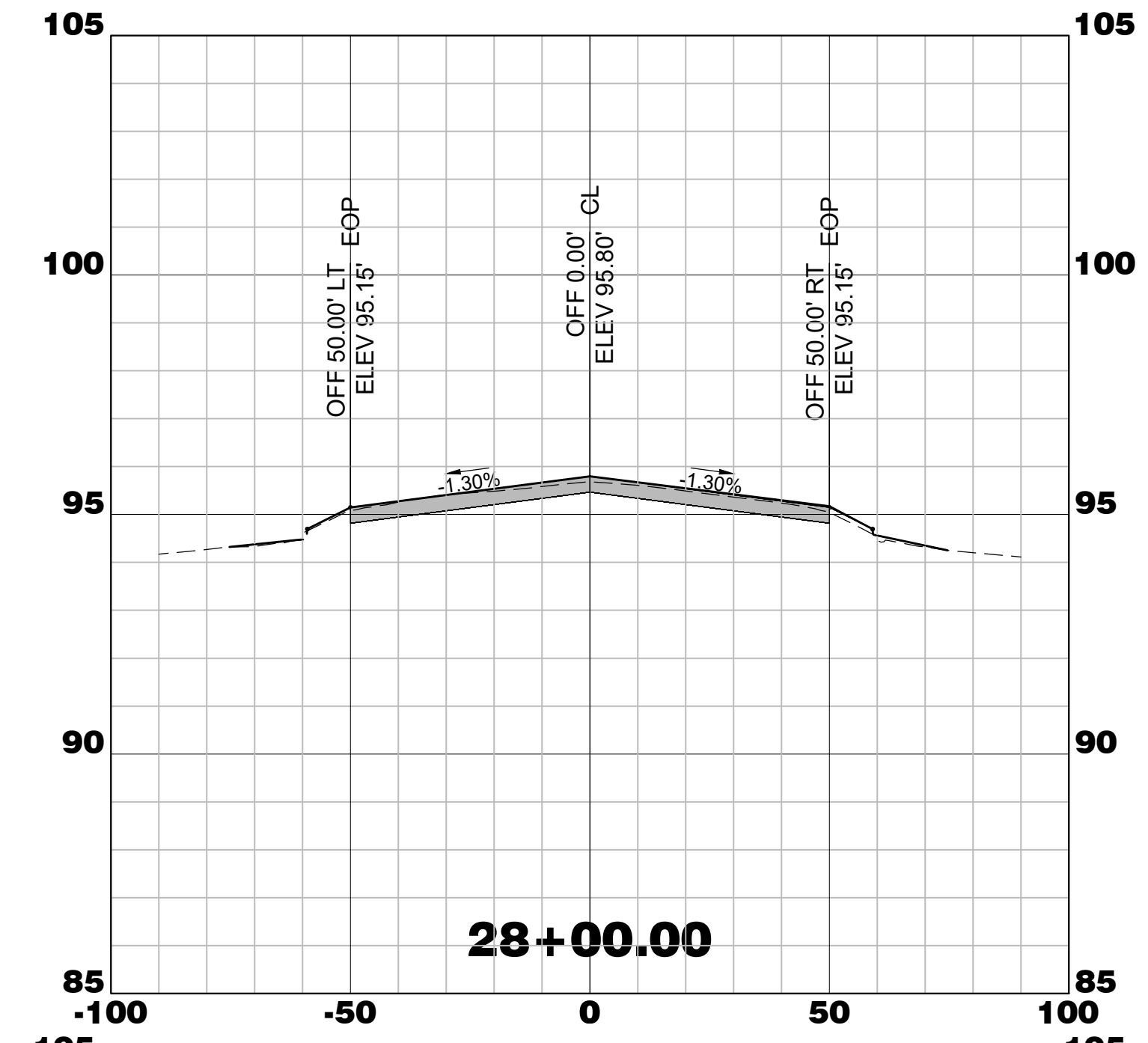
Drawing Name:
RUNWAY 5-23 CROSS SECTIONS STA. 27+00 TO 29+50

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

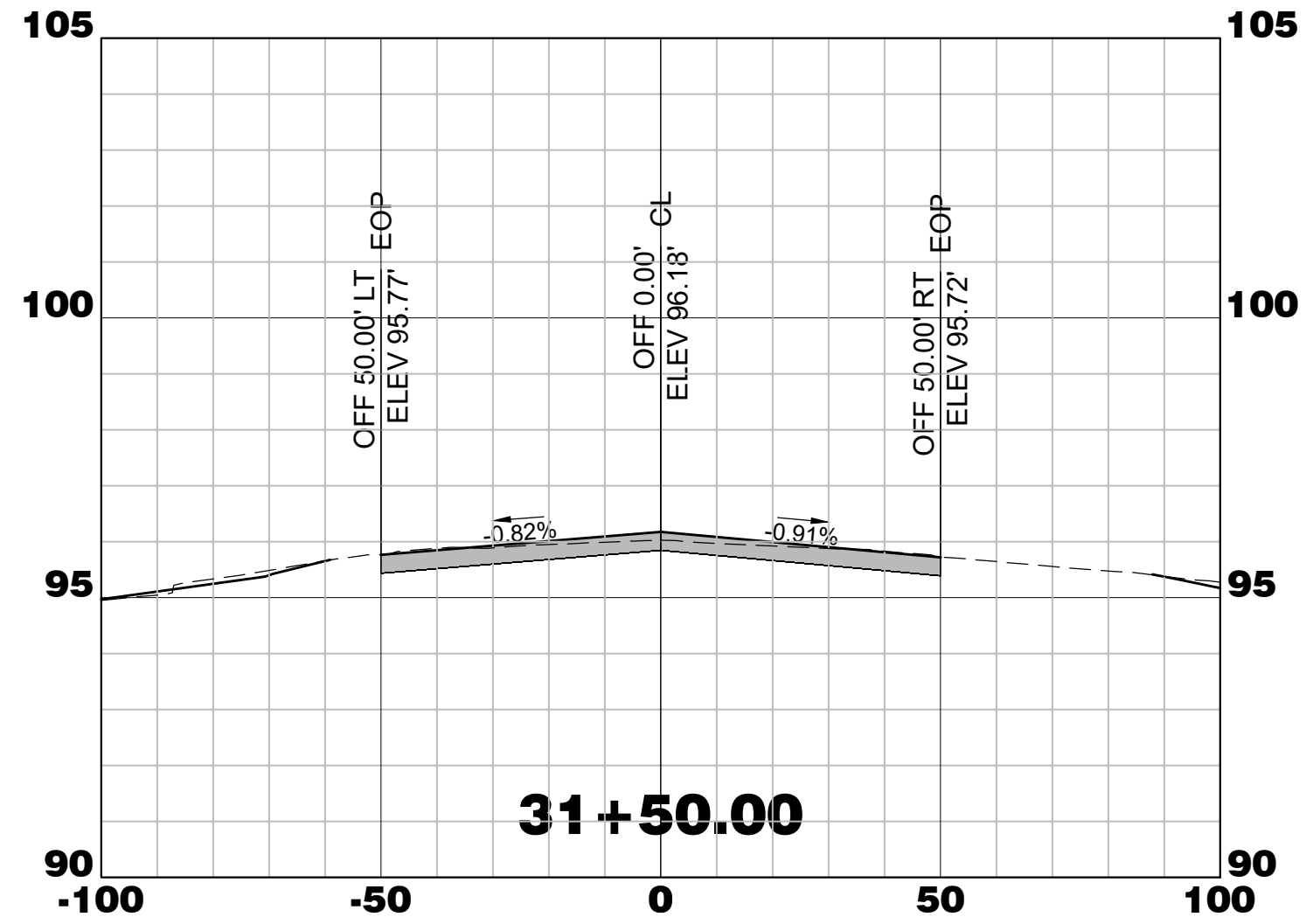
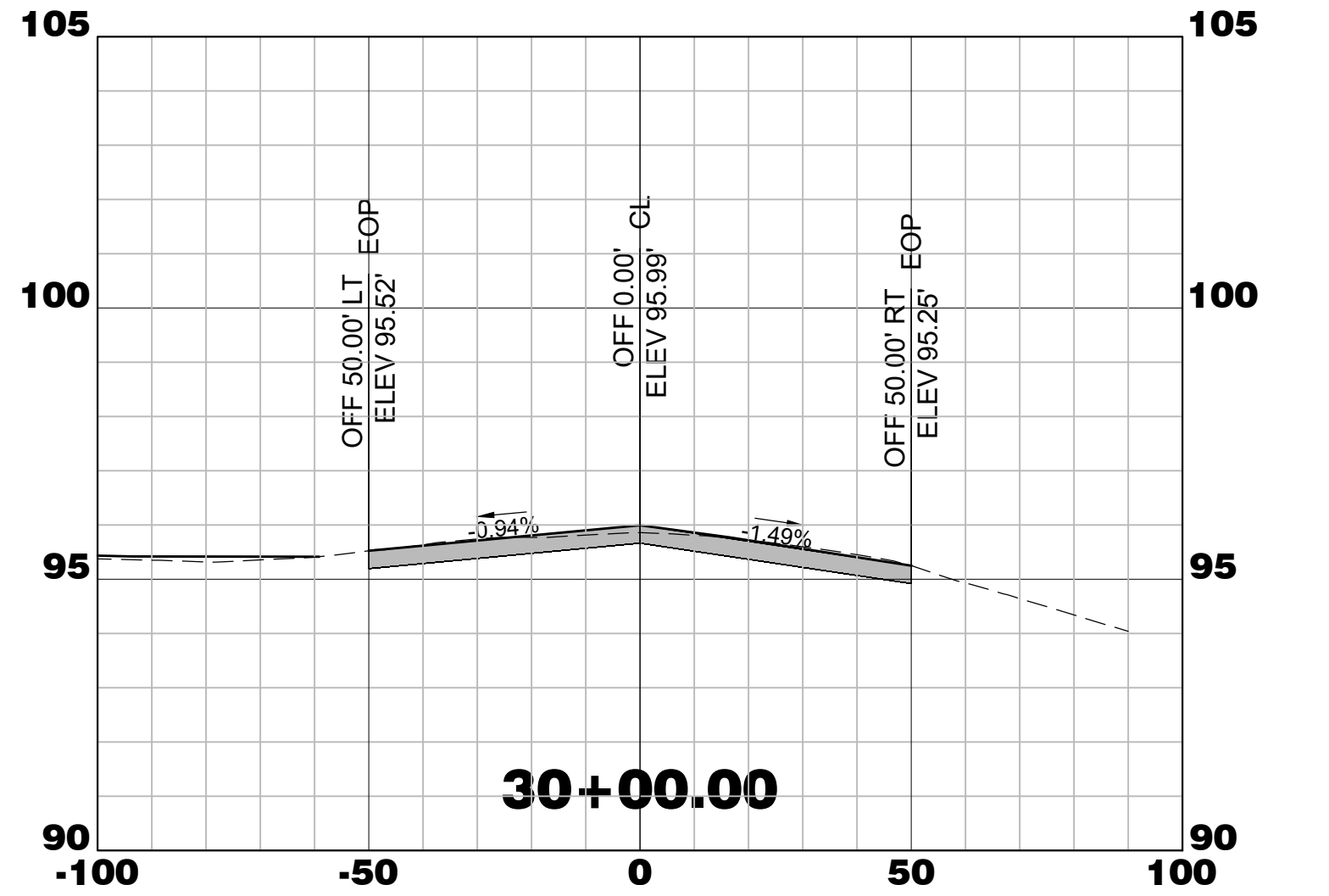
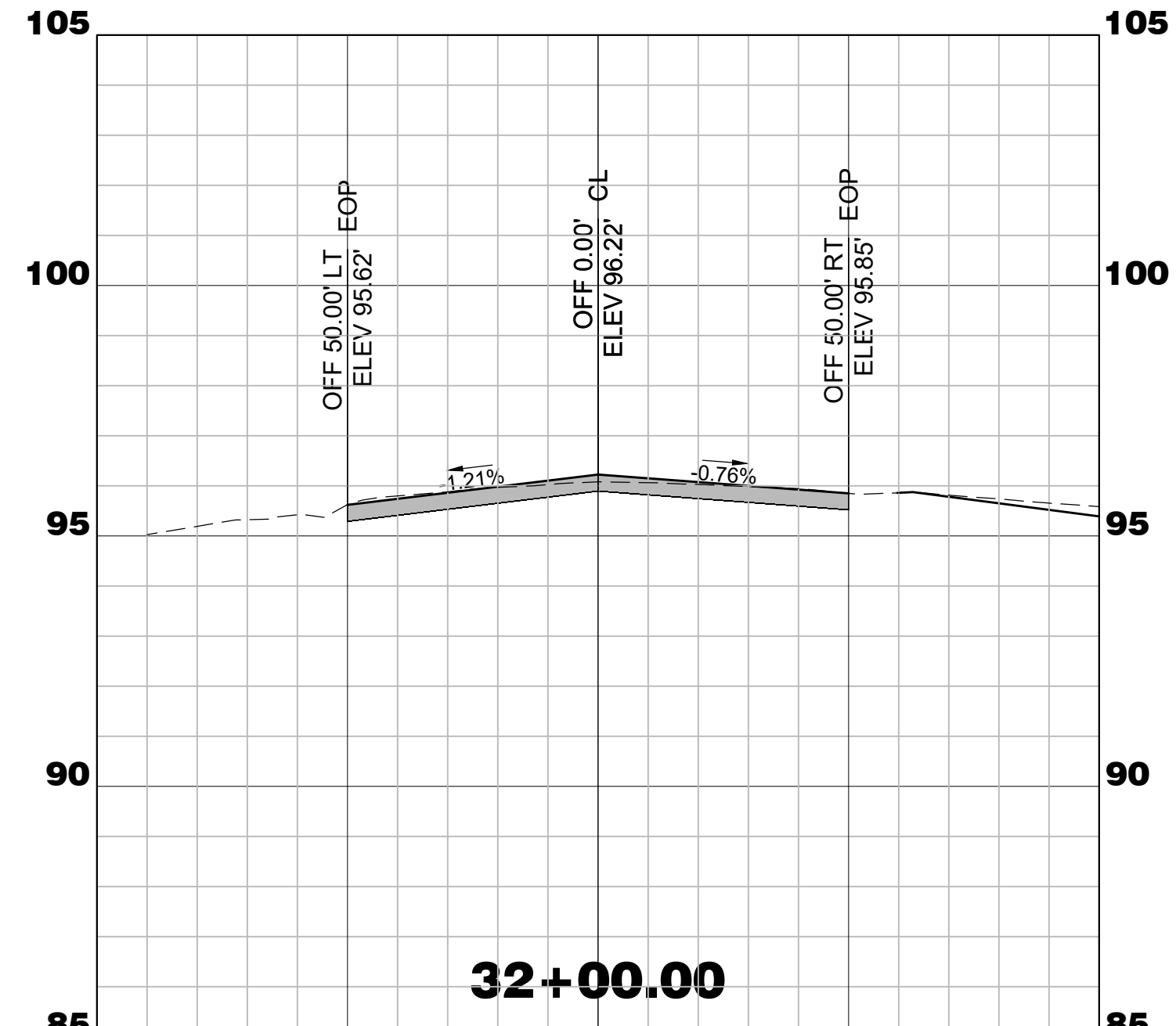
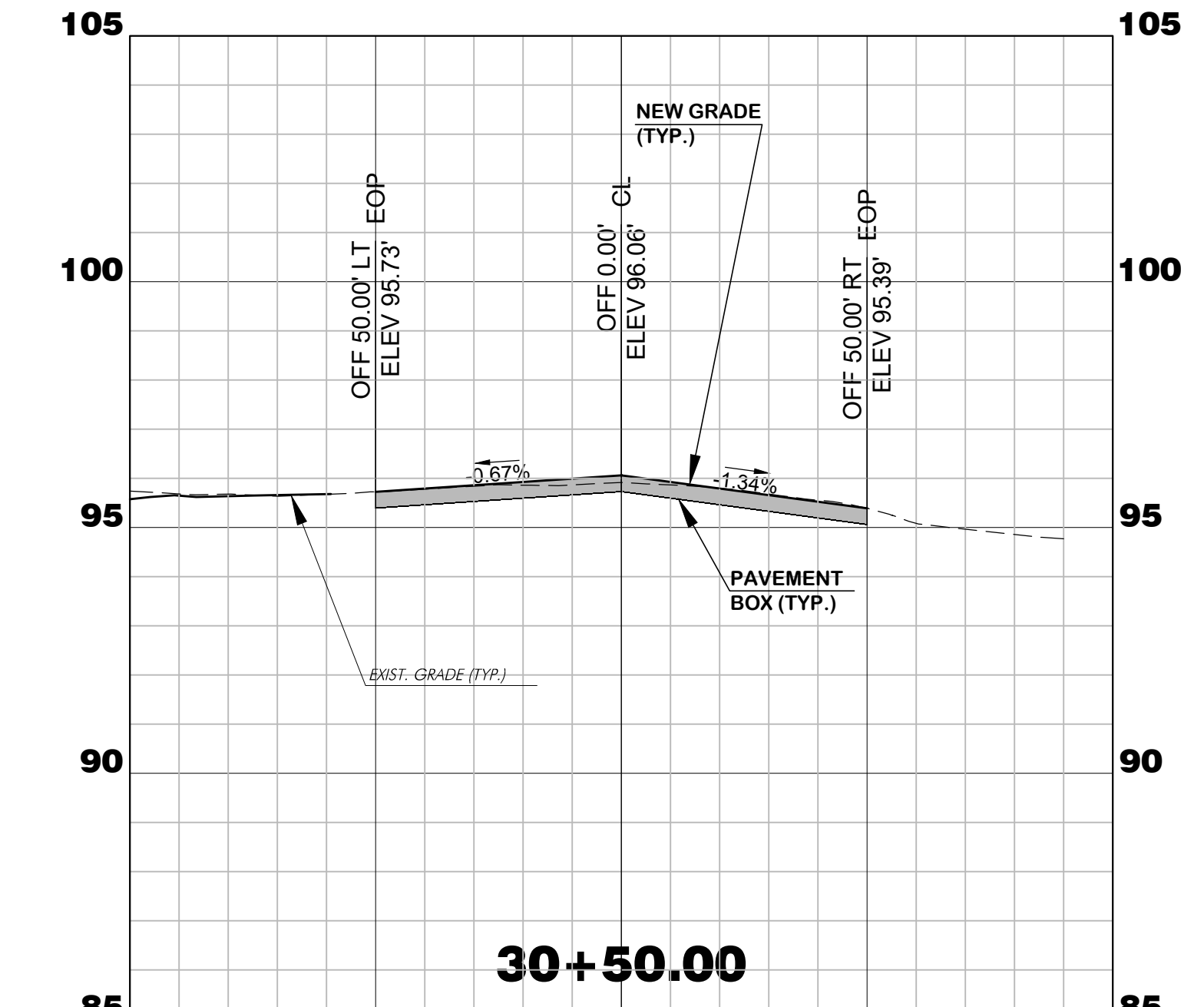
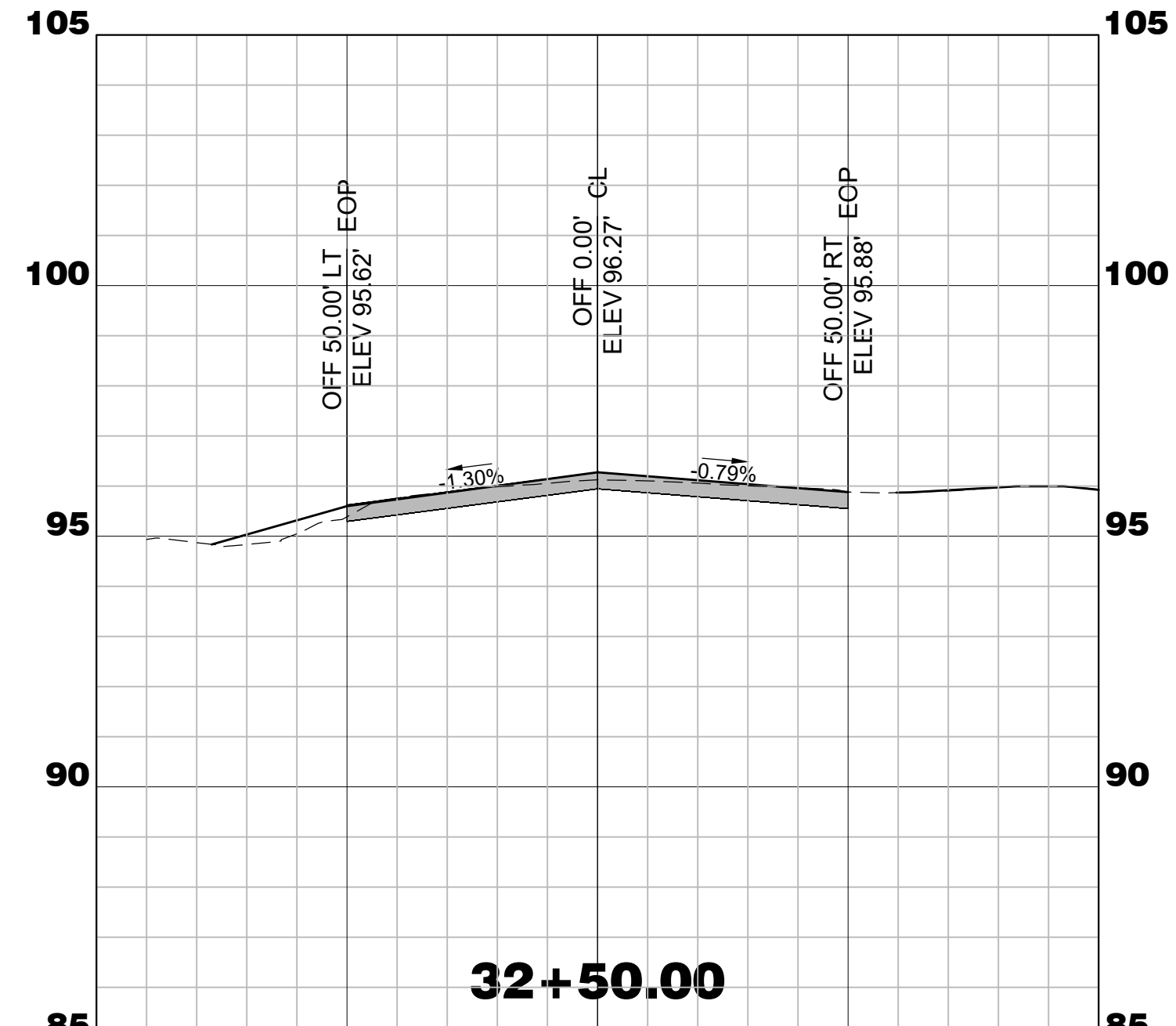
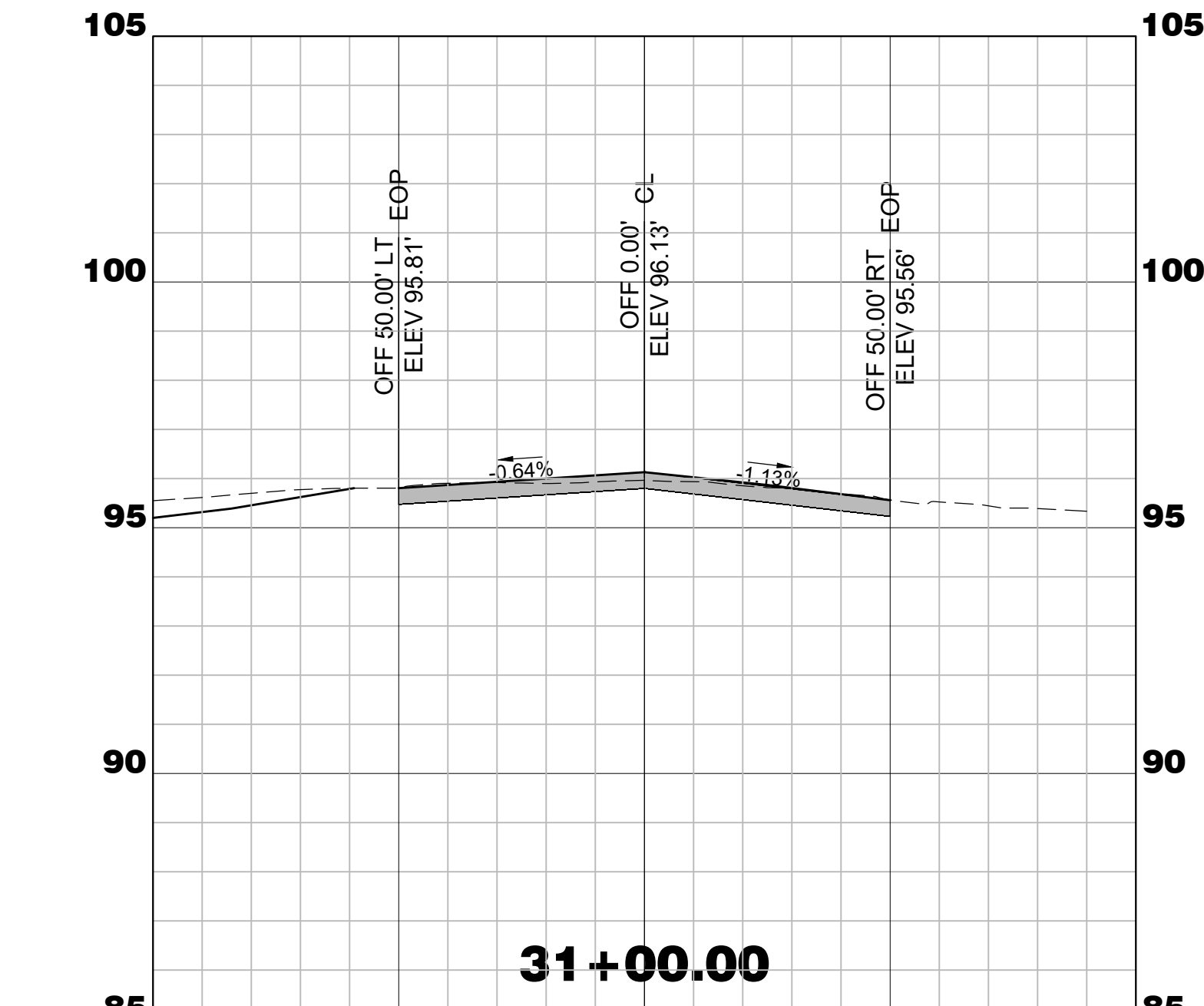
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1"=5' VERT. Drawing Number:
X-6



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Z:\Aviation\CLIENTS\walterboro lowcountry\gn02025 rv 5-23 rehab\rehab02-drawings\Civil\3D DESIGN\img Modified: Mar 26, 2025 - 4:15pm Plotted: Mar 26, 2025 - 5:00pm Lauren Watson



WALTERBORO, SOUTH CAROLINA

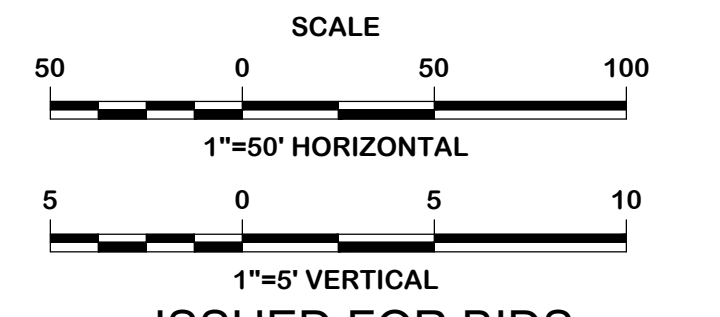
Michael Baker

INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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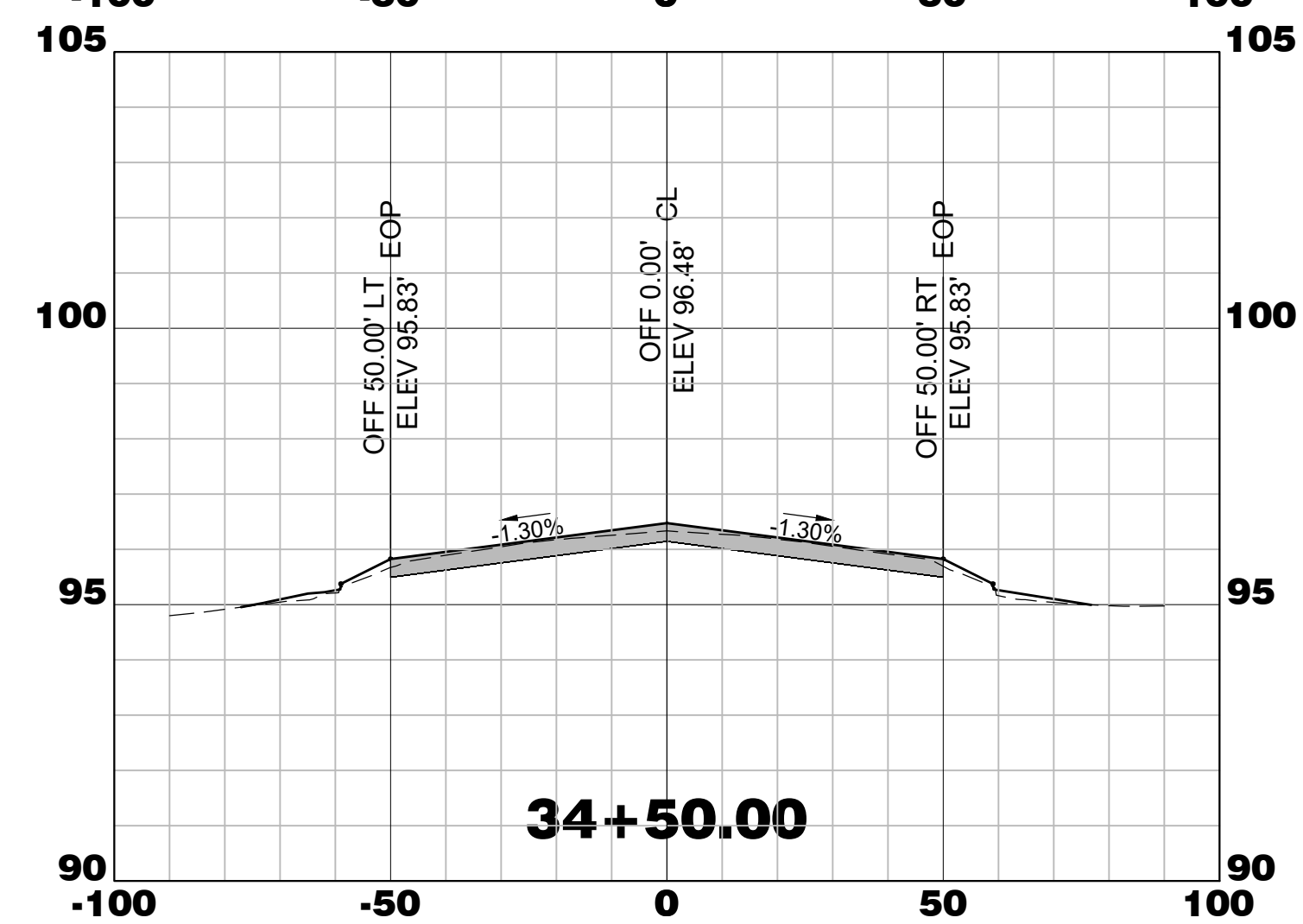
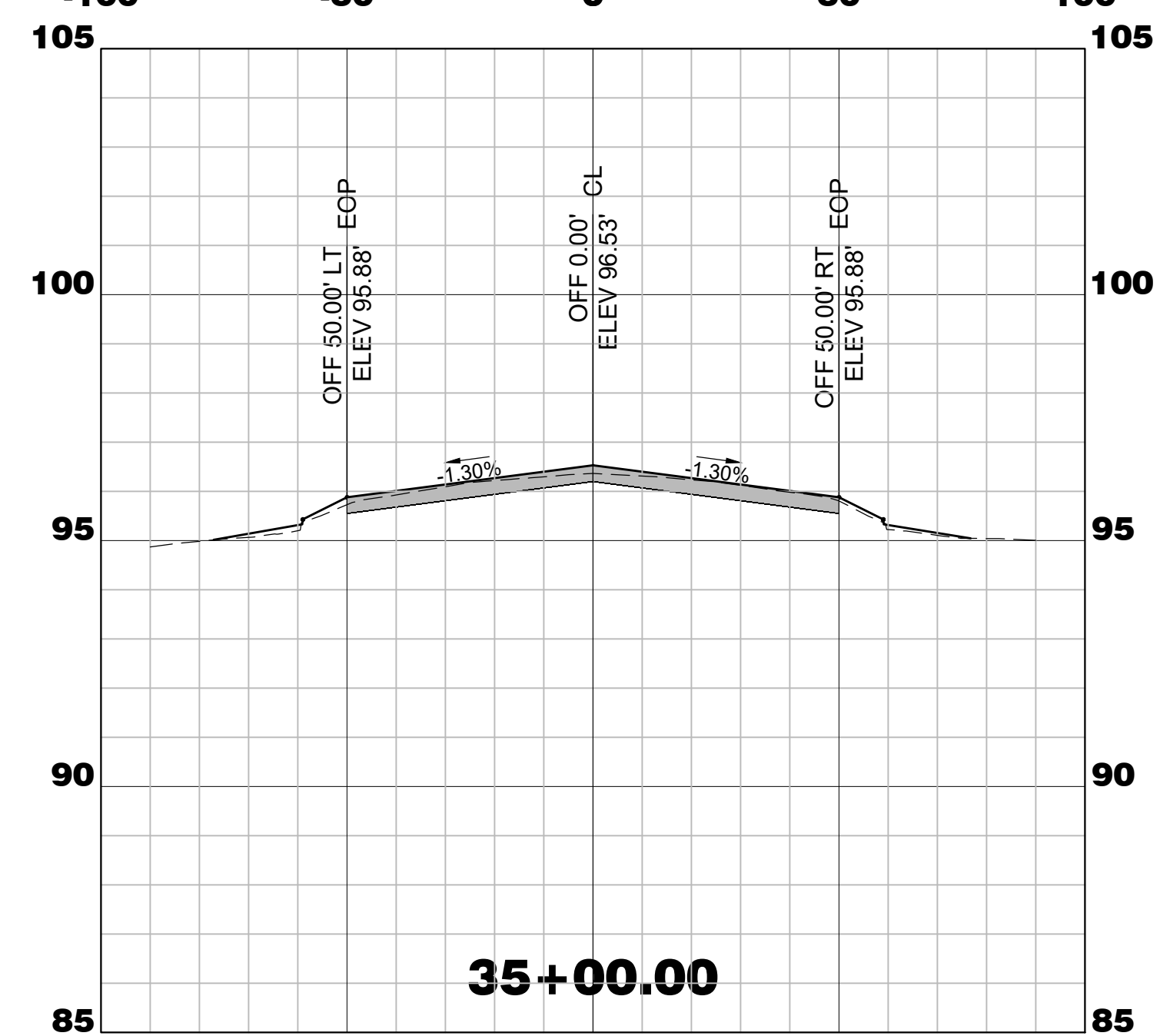
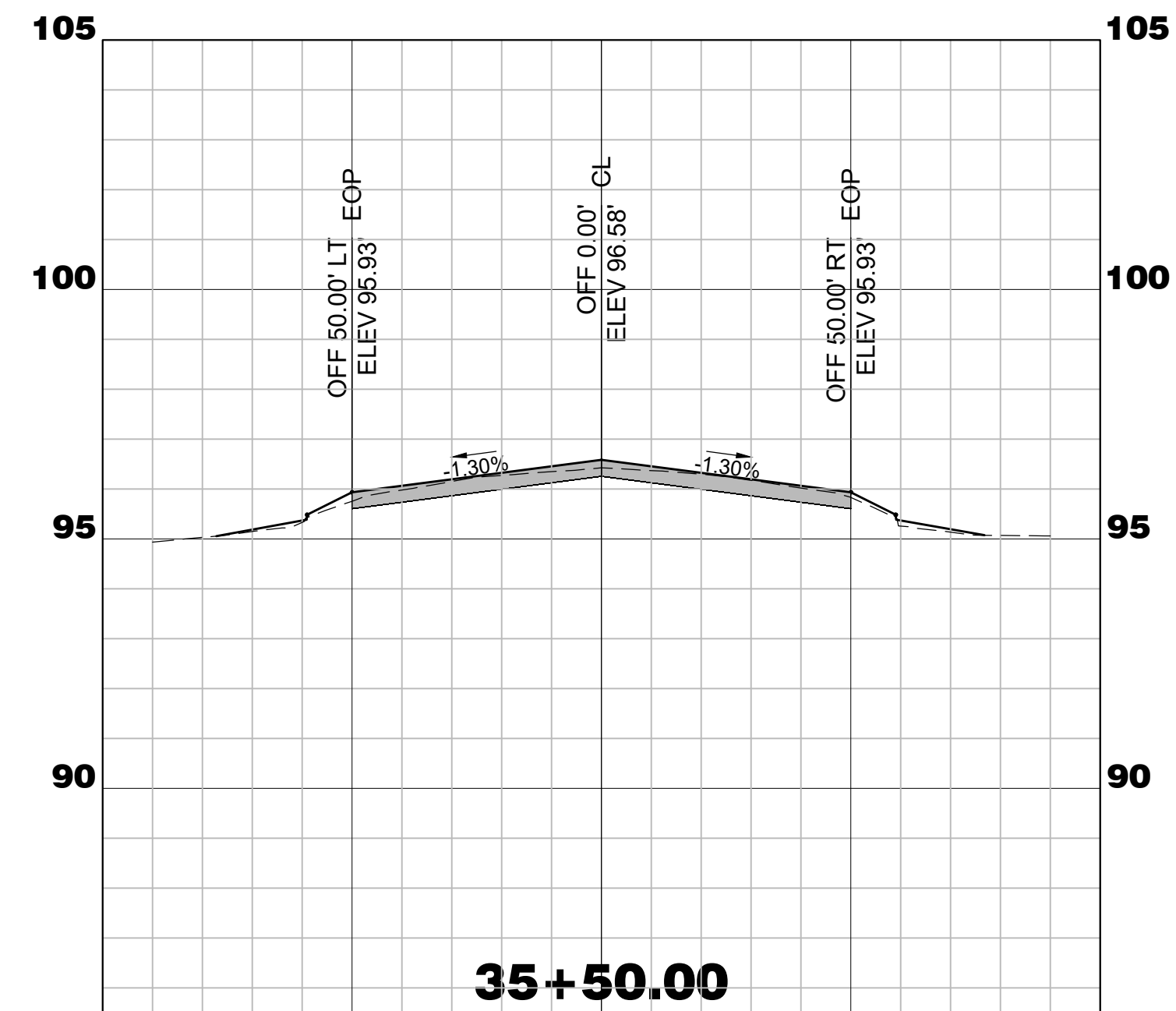
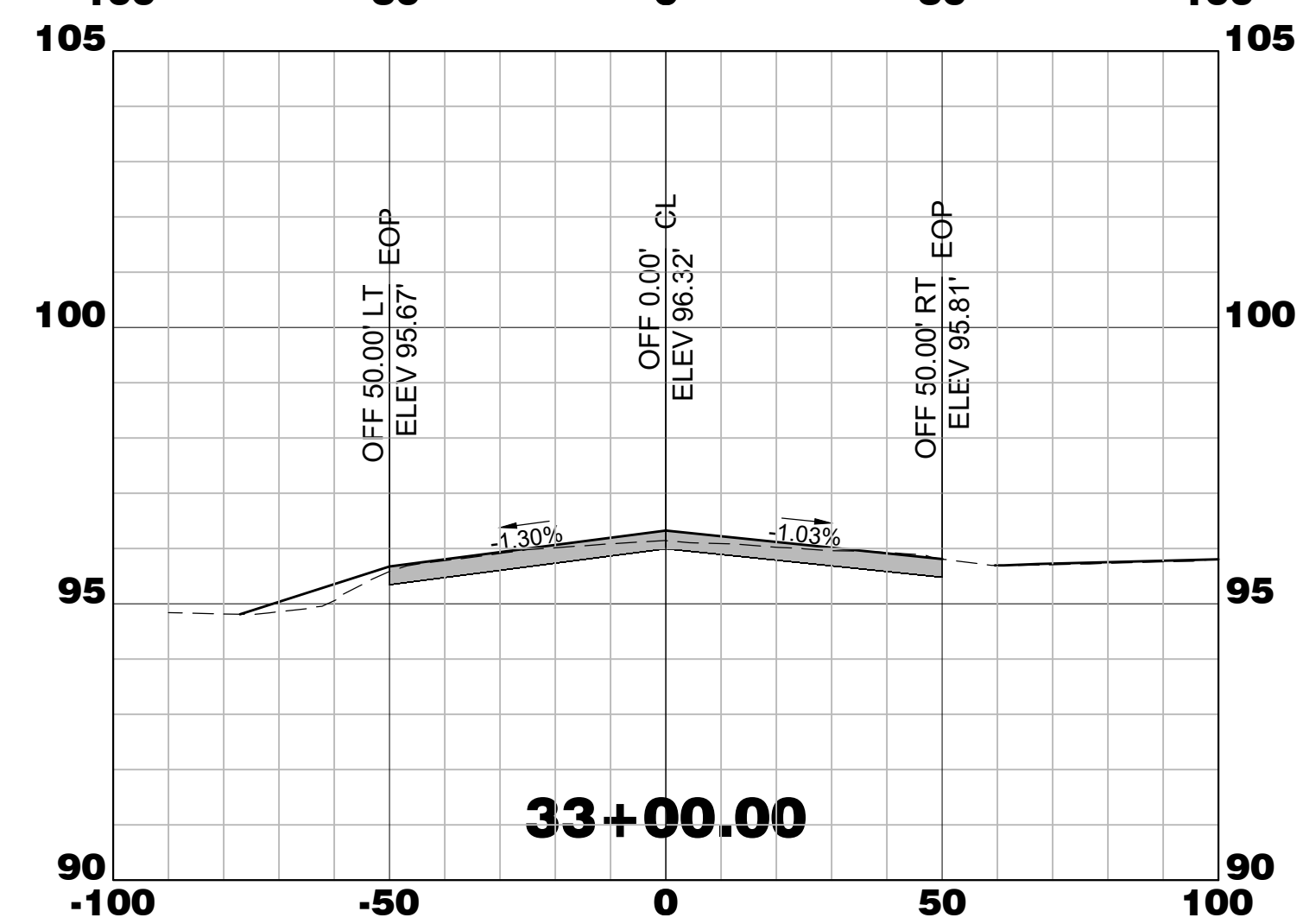
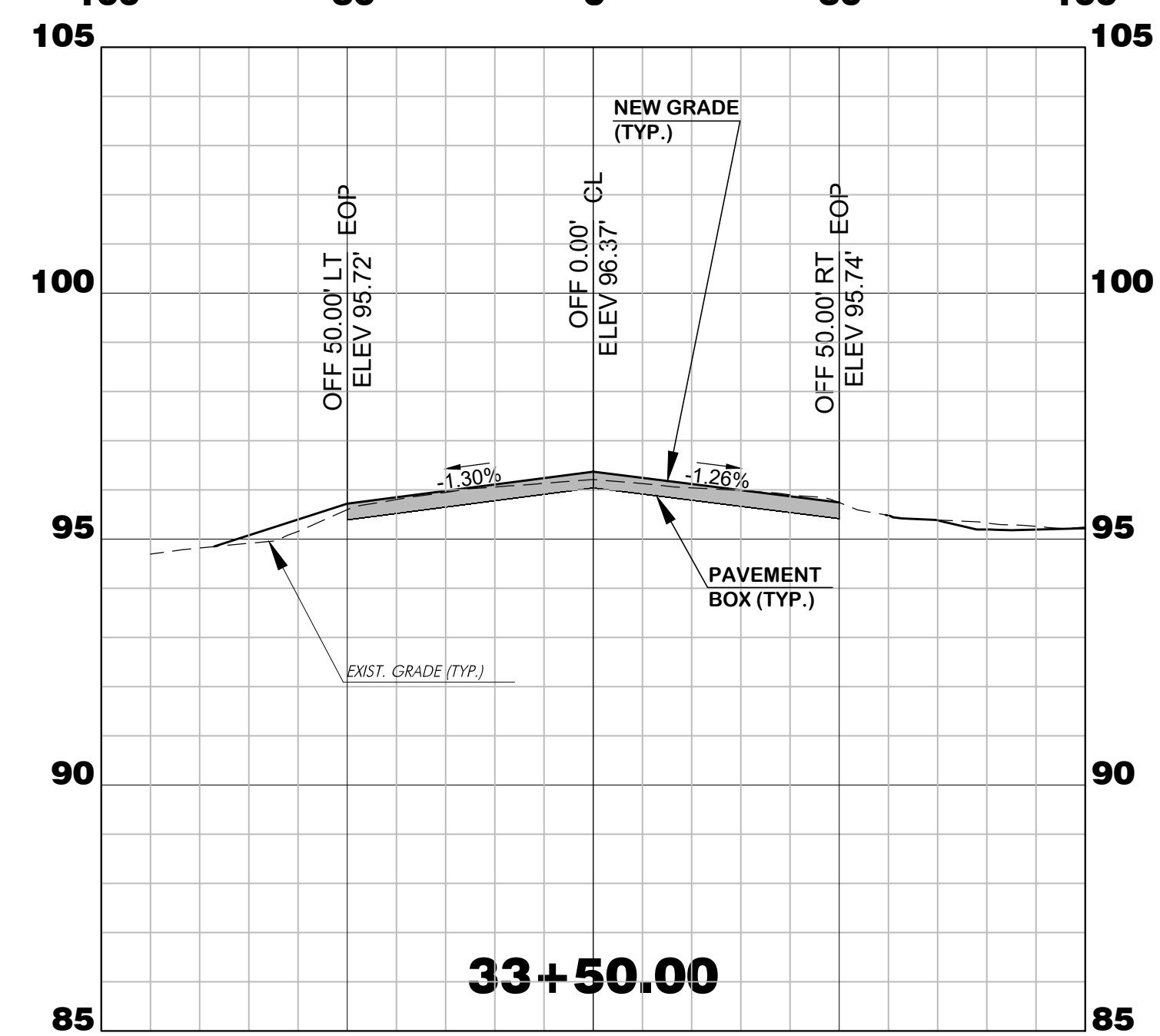
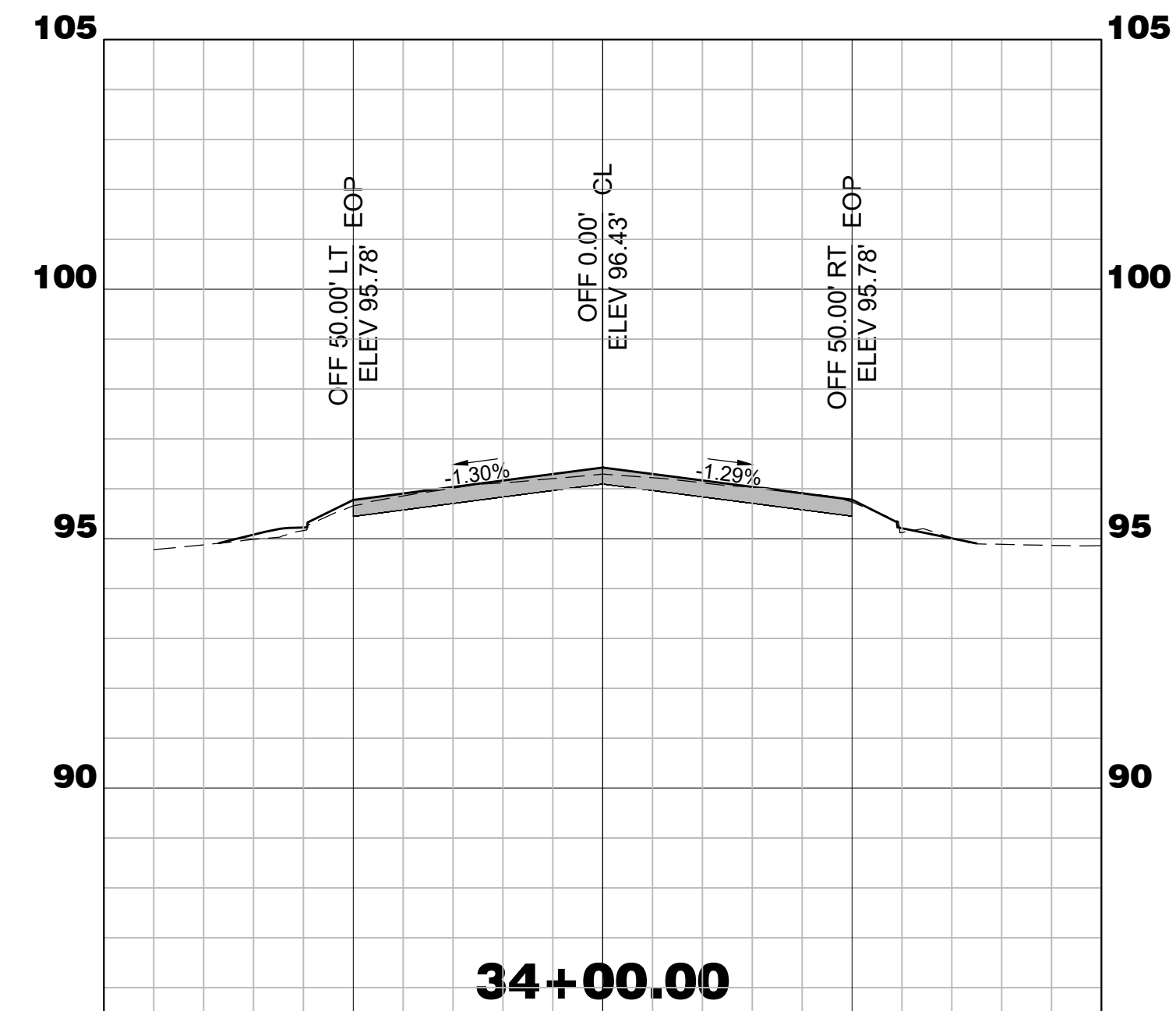
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 CROSS SECTIONS STA. 30+00 TO 32+50

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ.	Sheet Number:
1"=5' VERT.	Drawing Number: X-7

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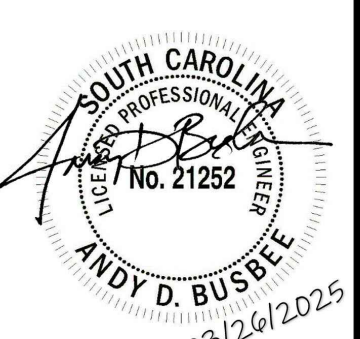


WALTERBORO, SOUTH CAROLINA

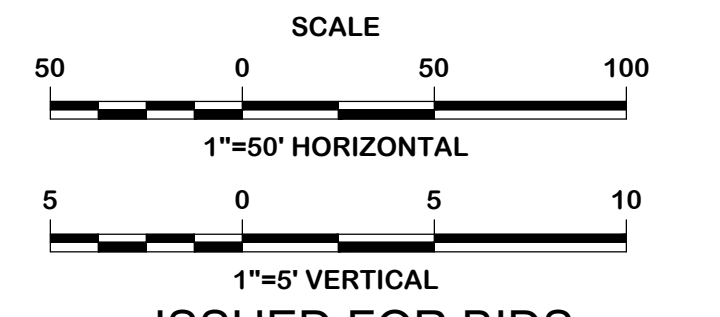
Michael Baker

INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

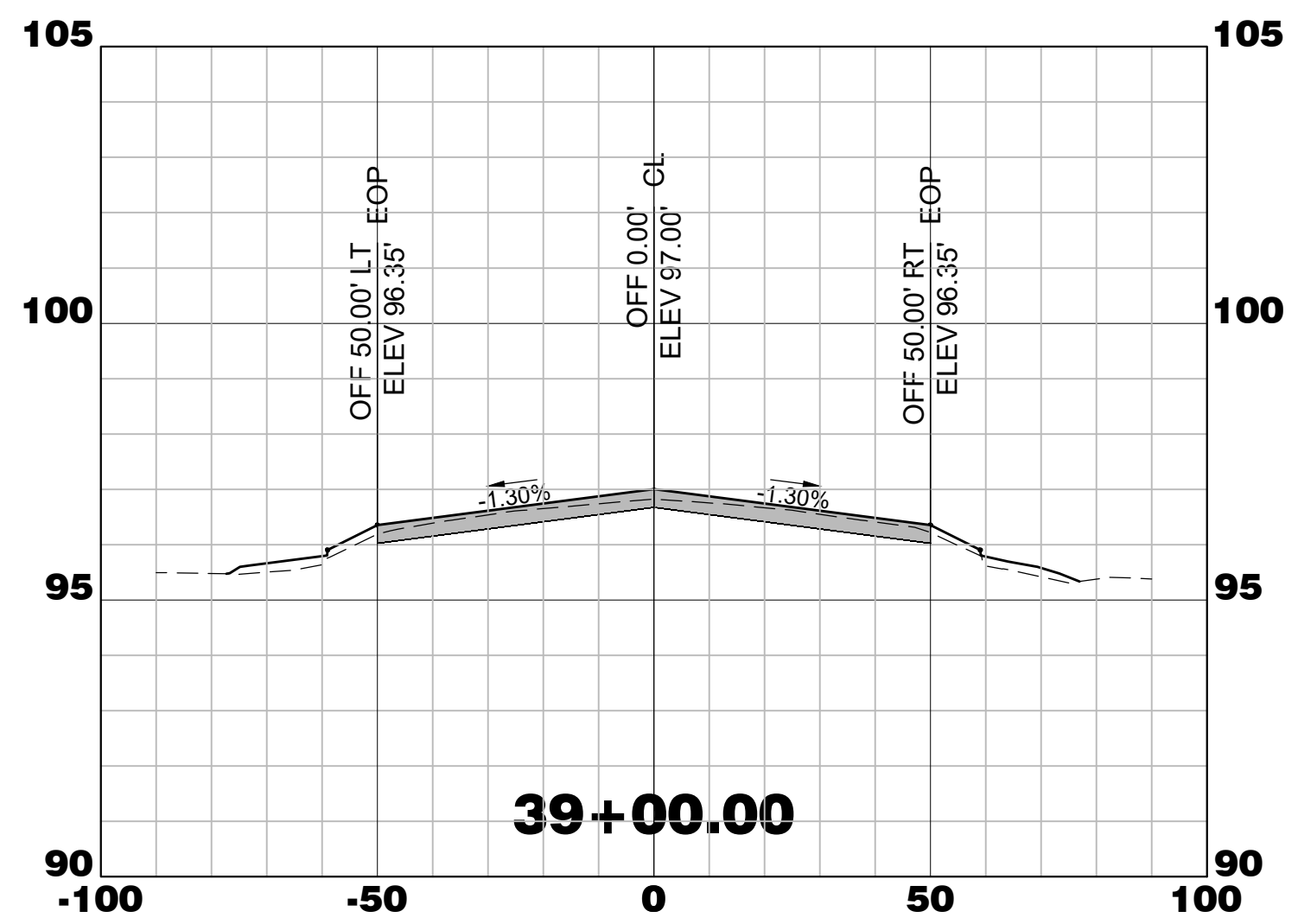
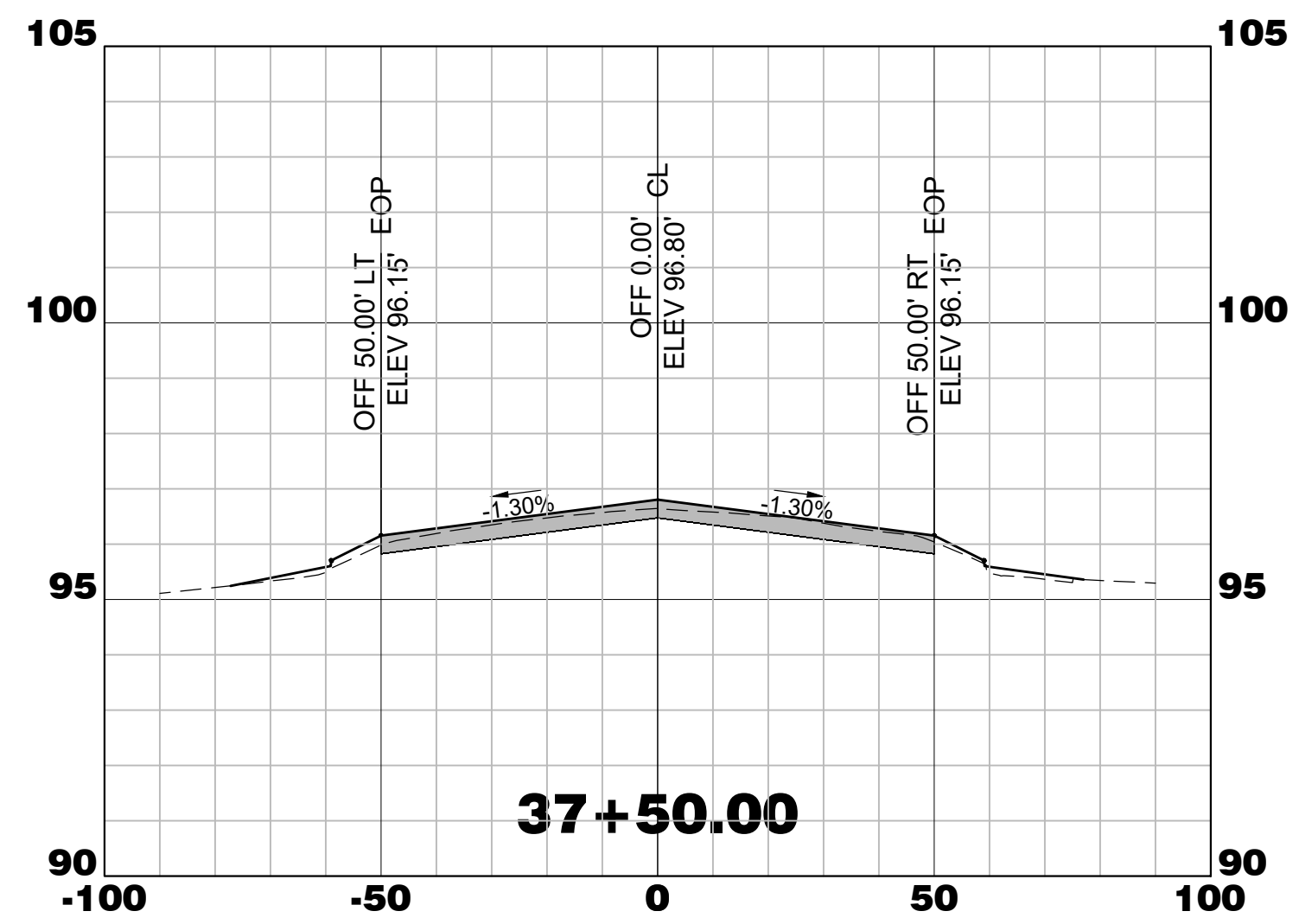
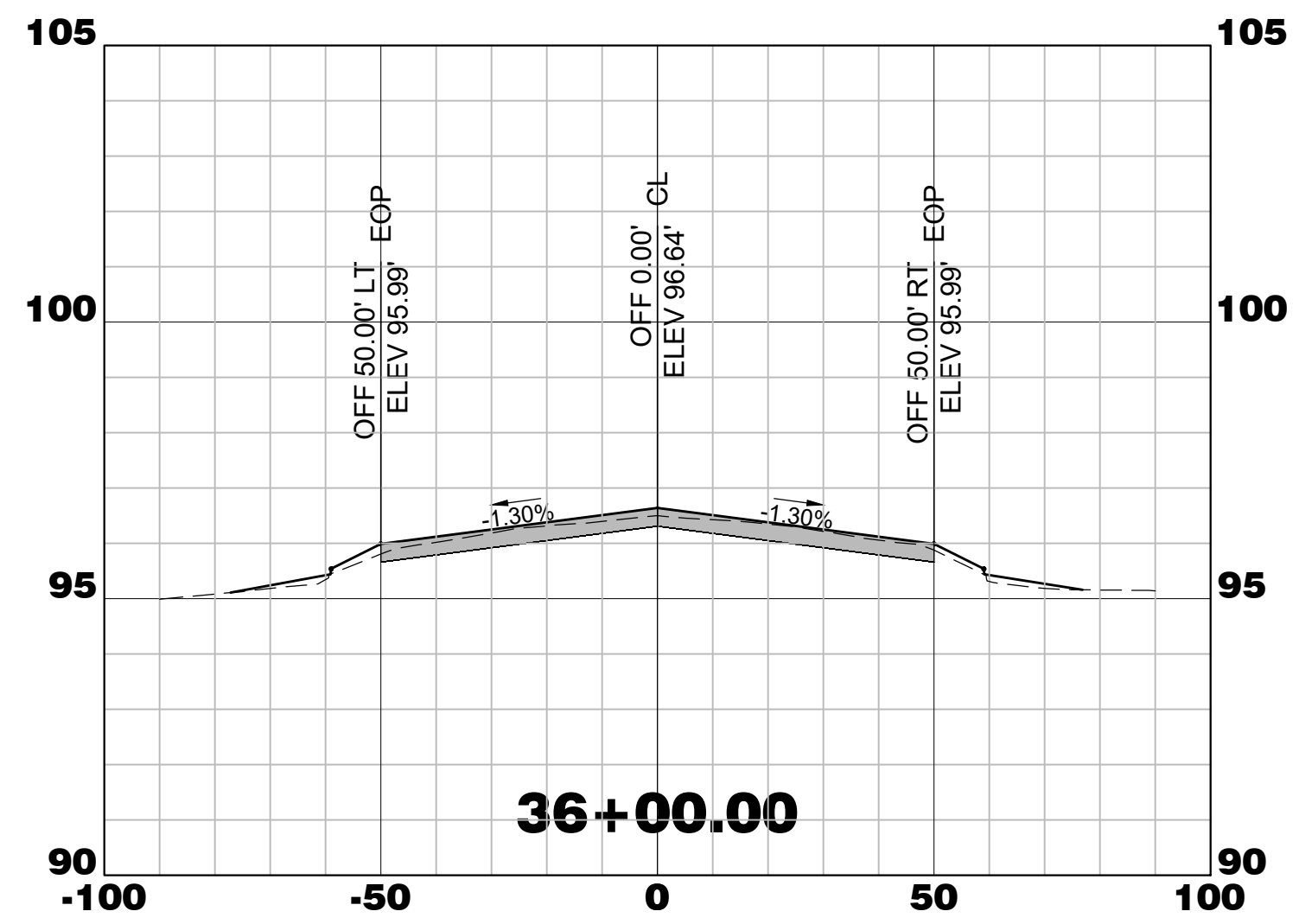
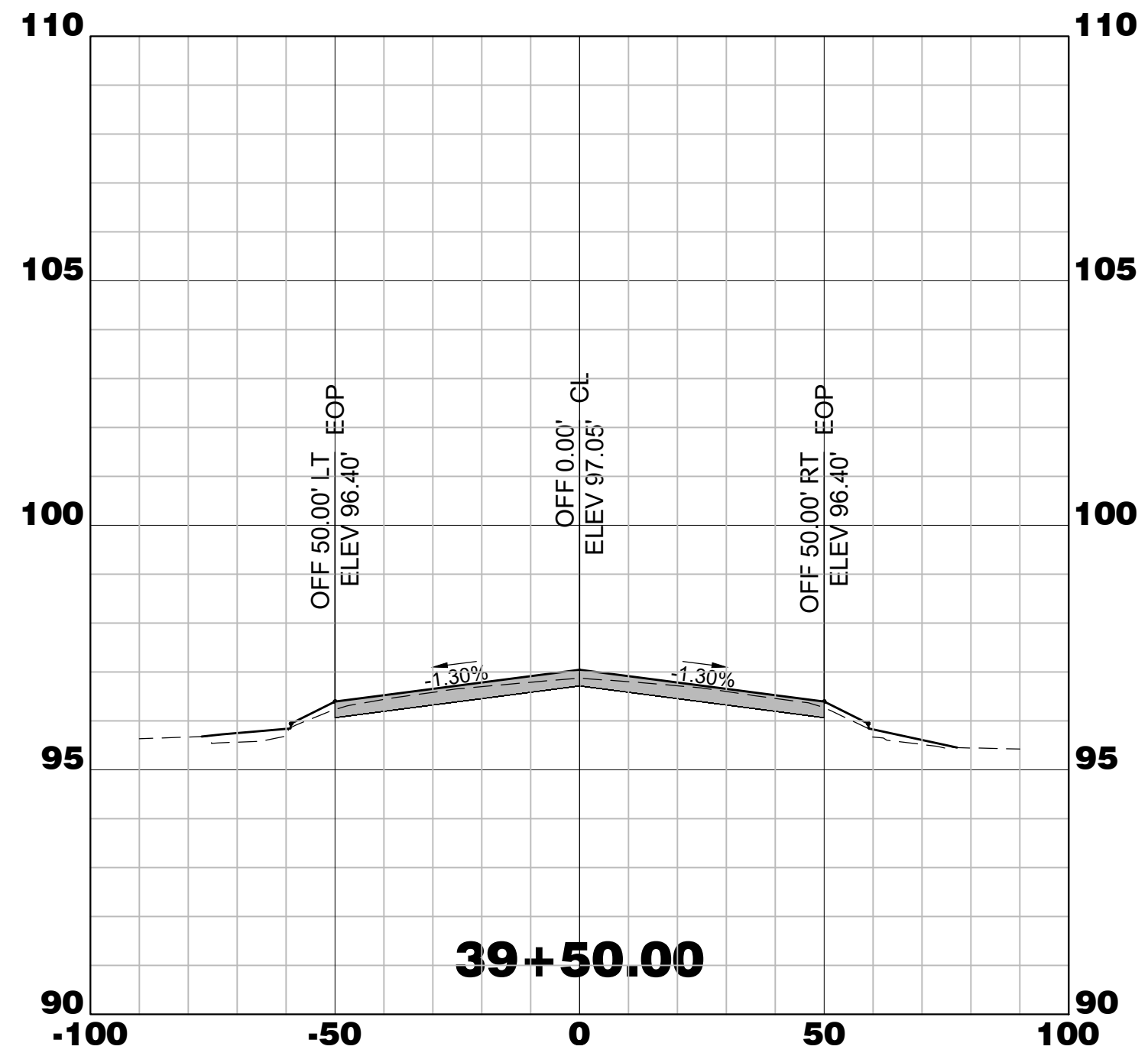
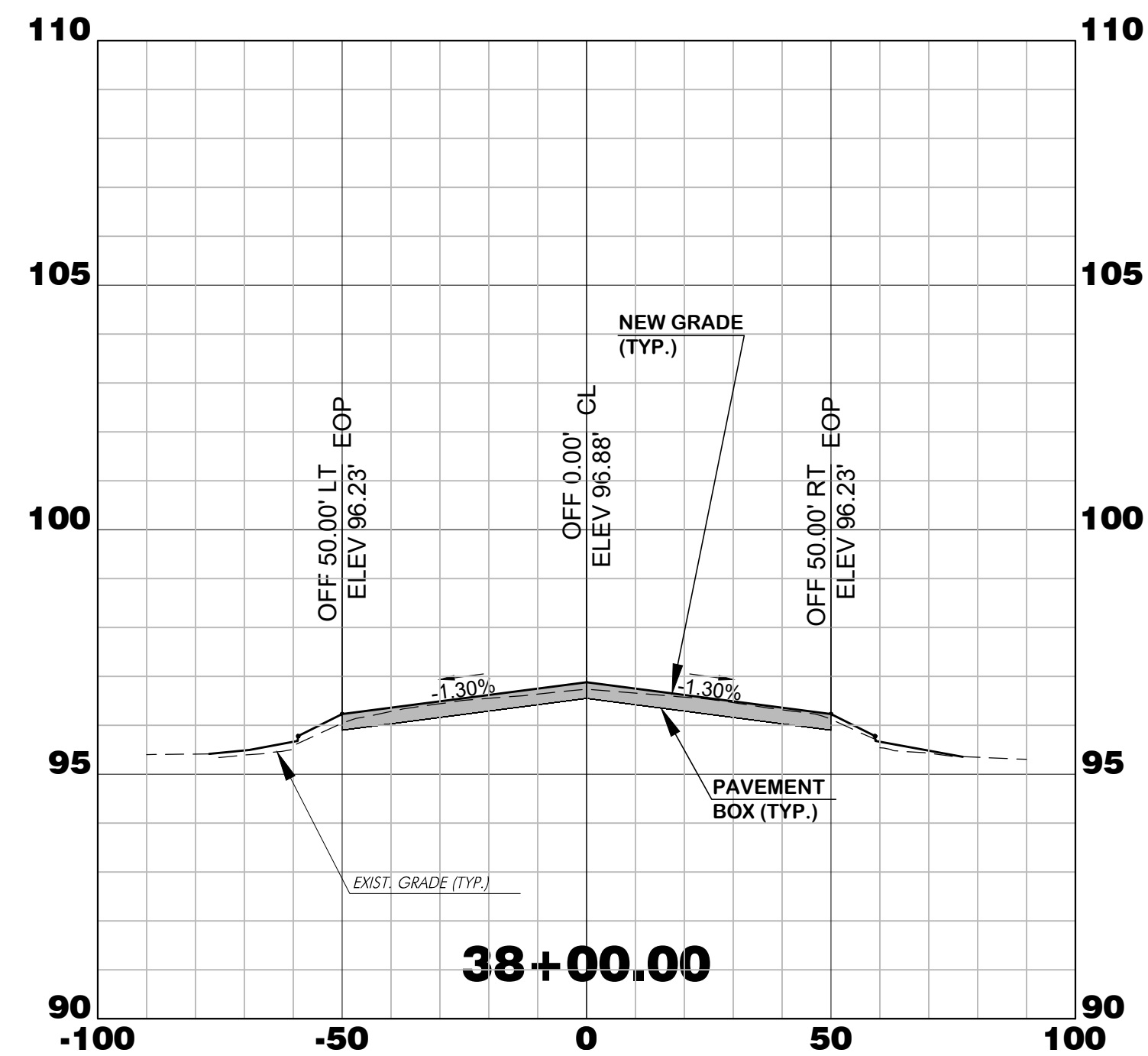
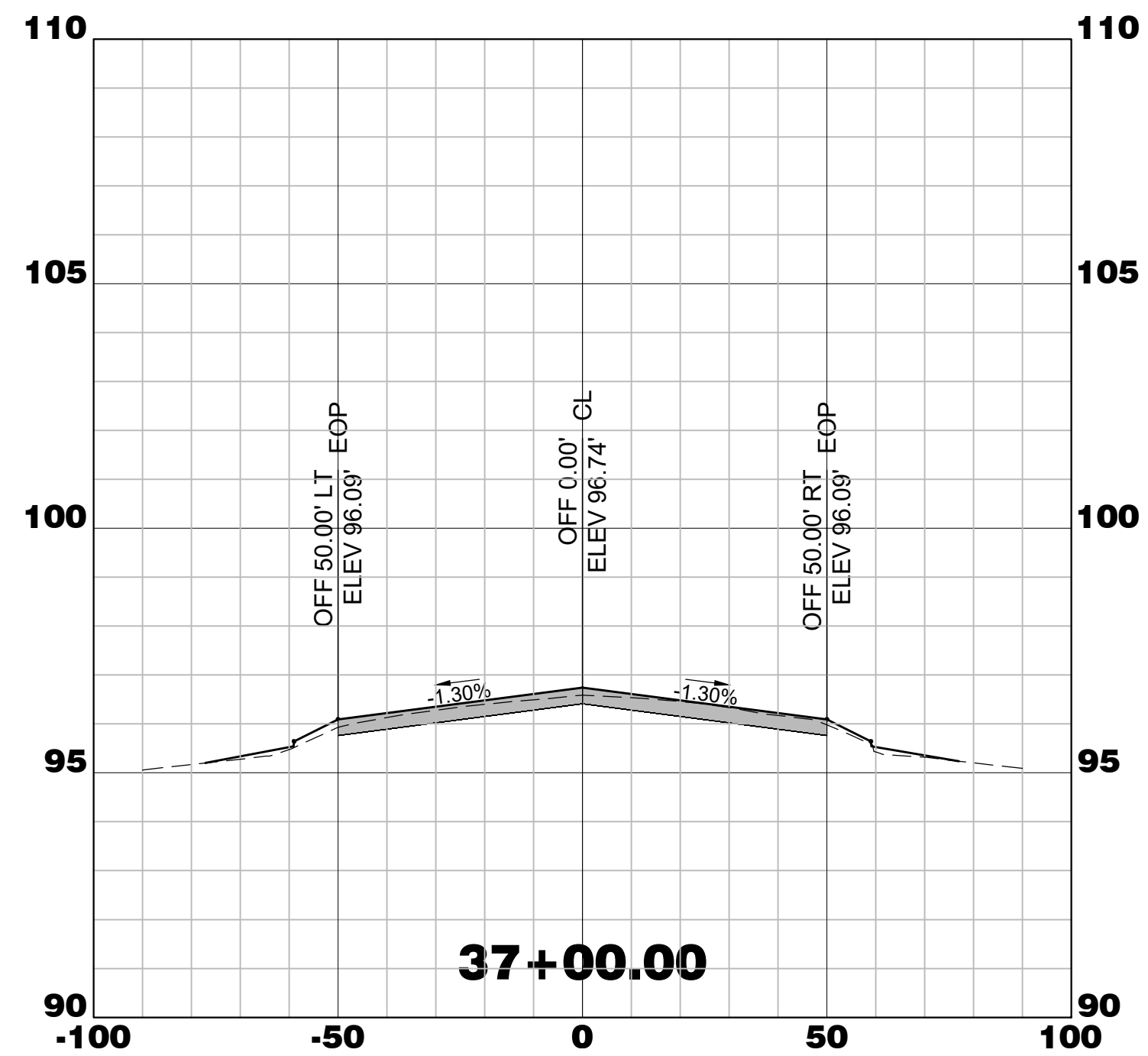
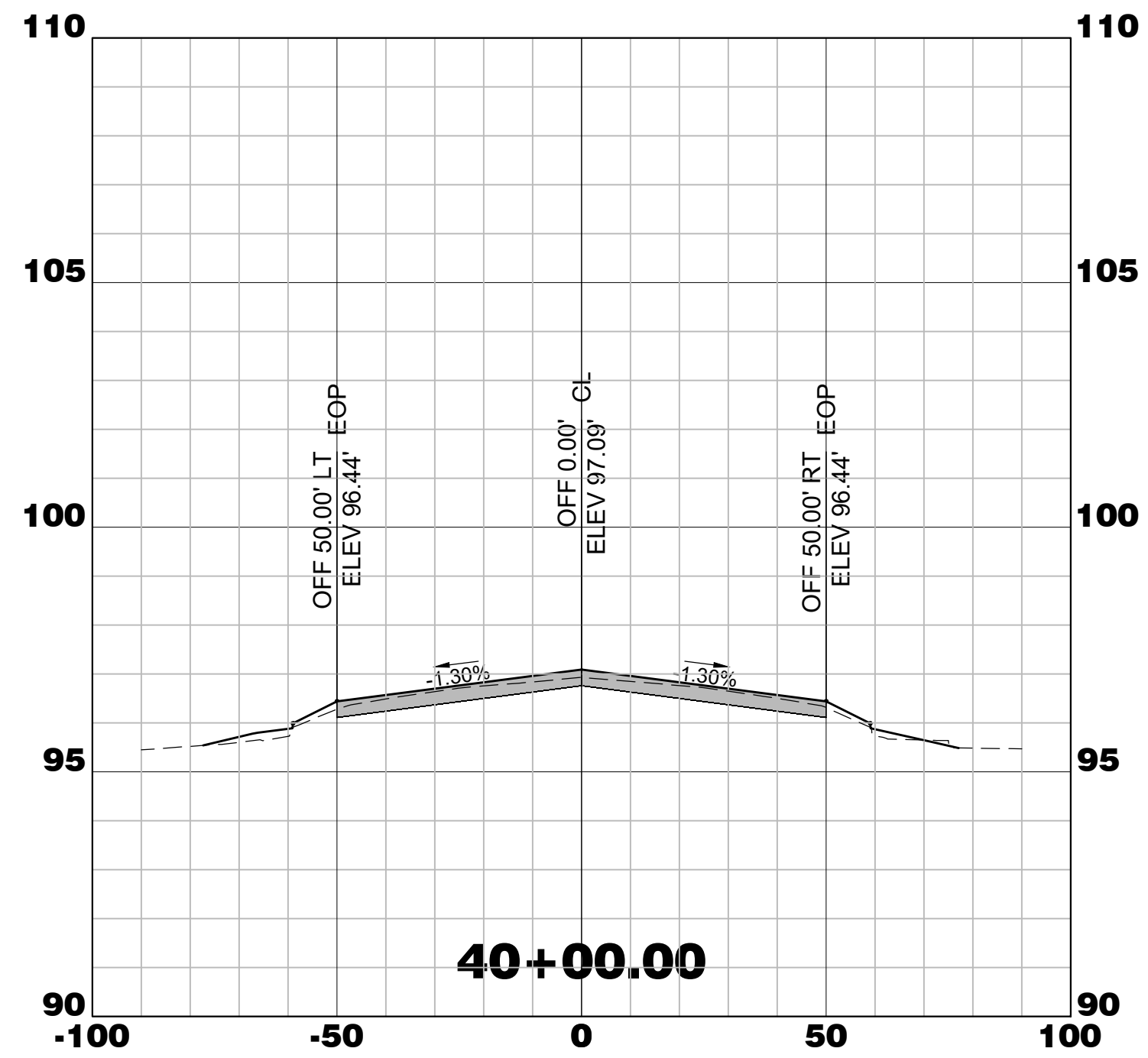
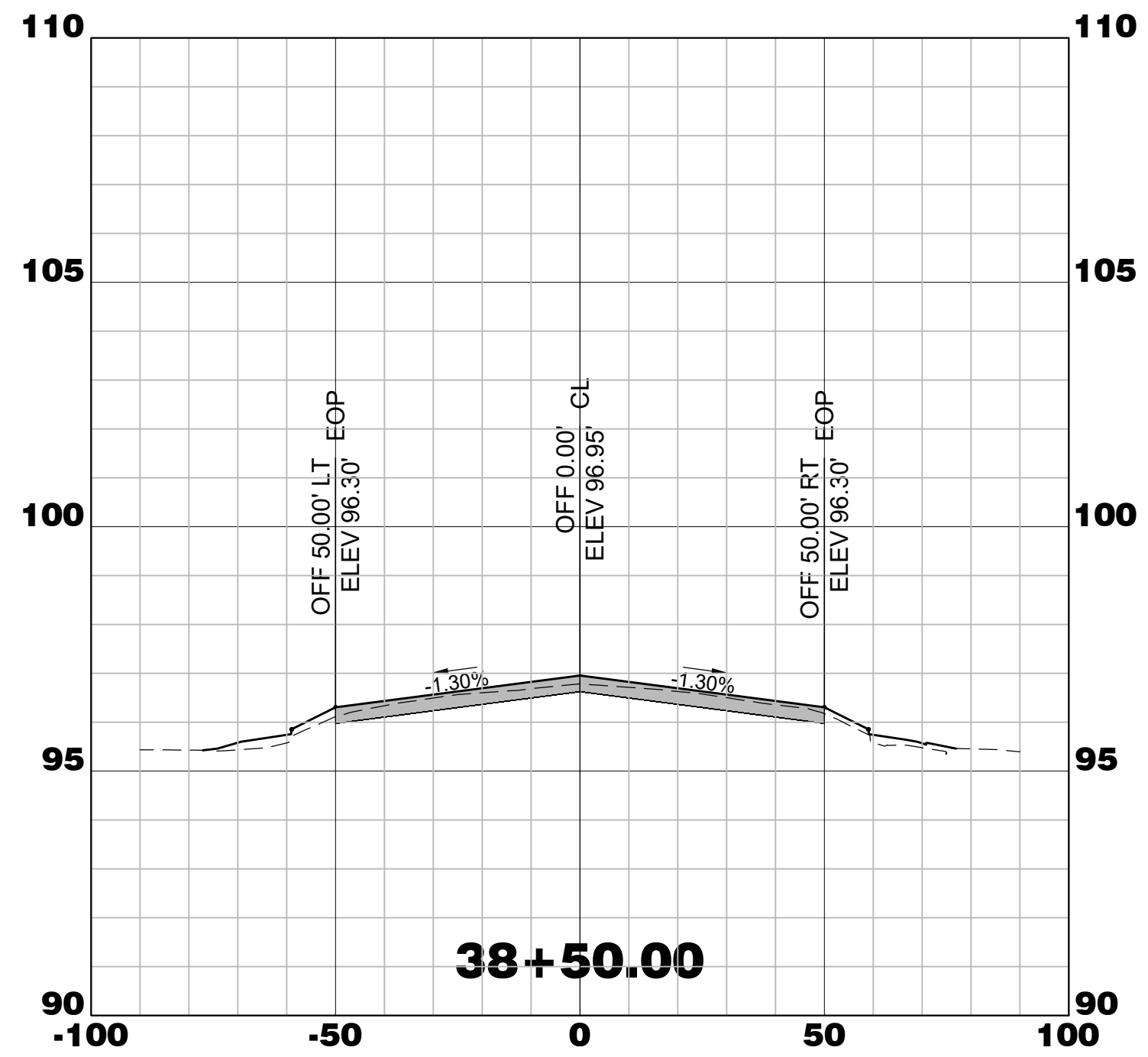
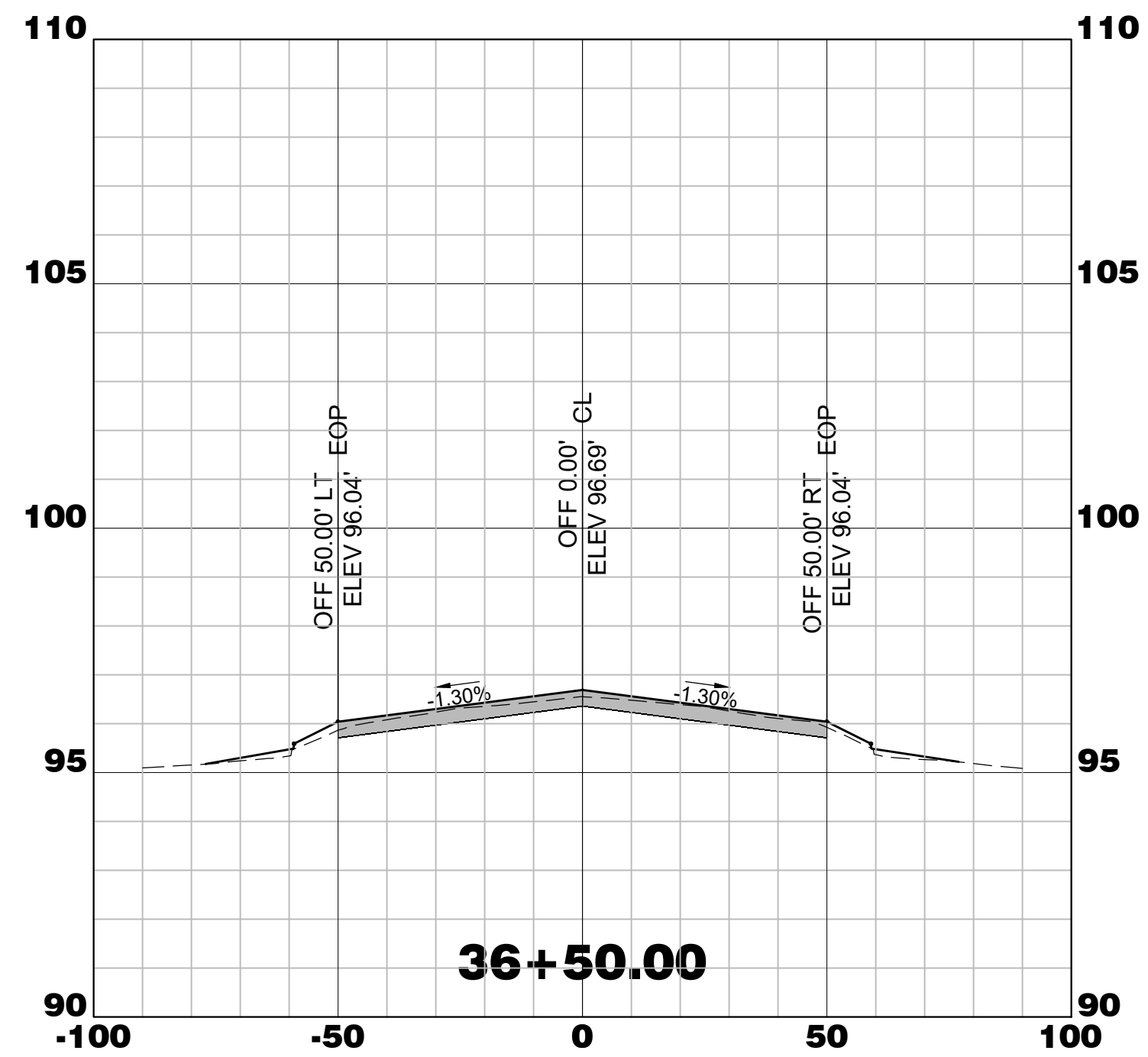
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 CROSS SECTIONS STA. 33+00 TO 35+50

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ.	Sheet Number:
1"=5' VERT.	Drawing Number: X-8

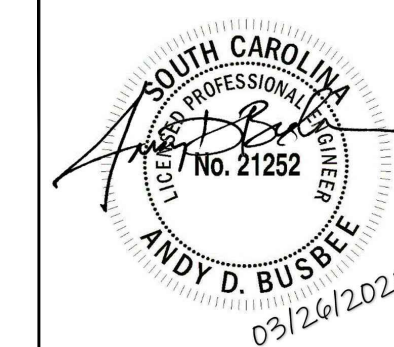
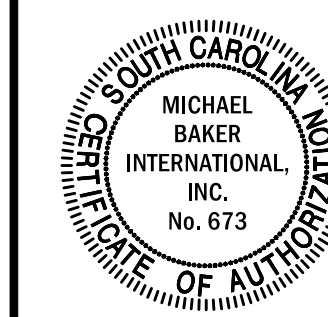
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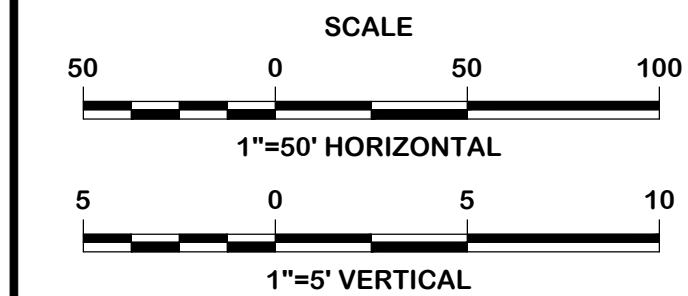
WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

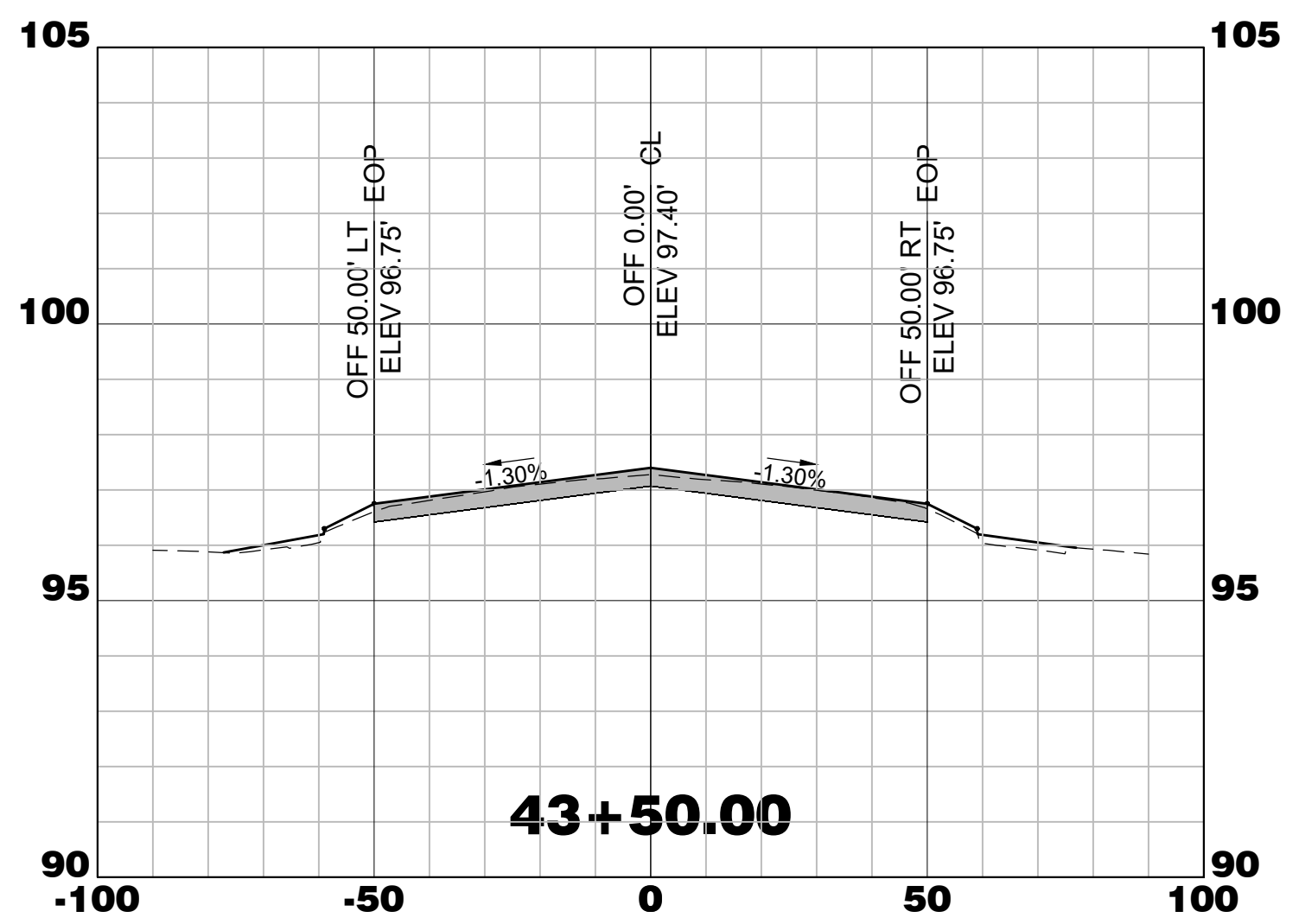
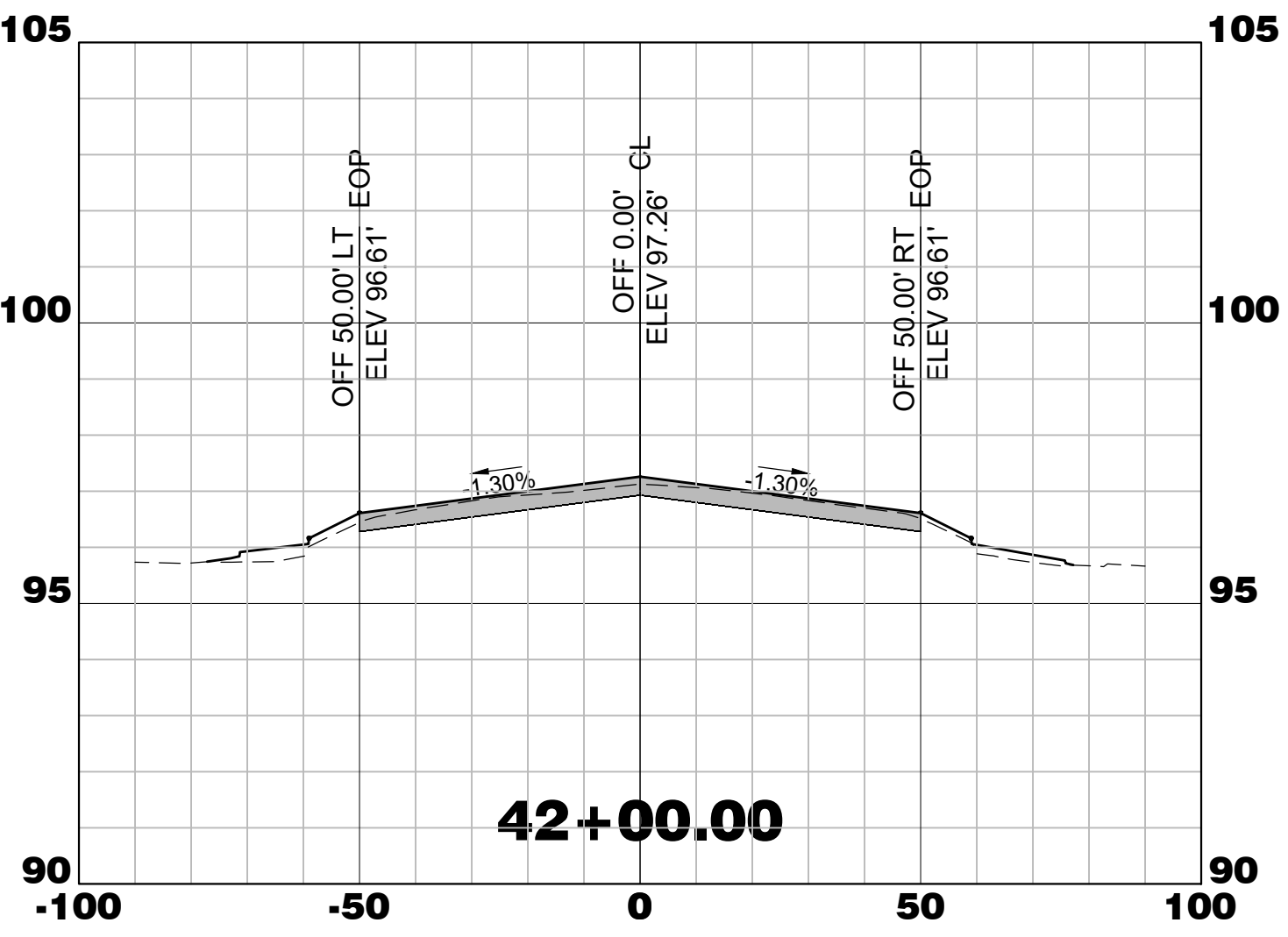
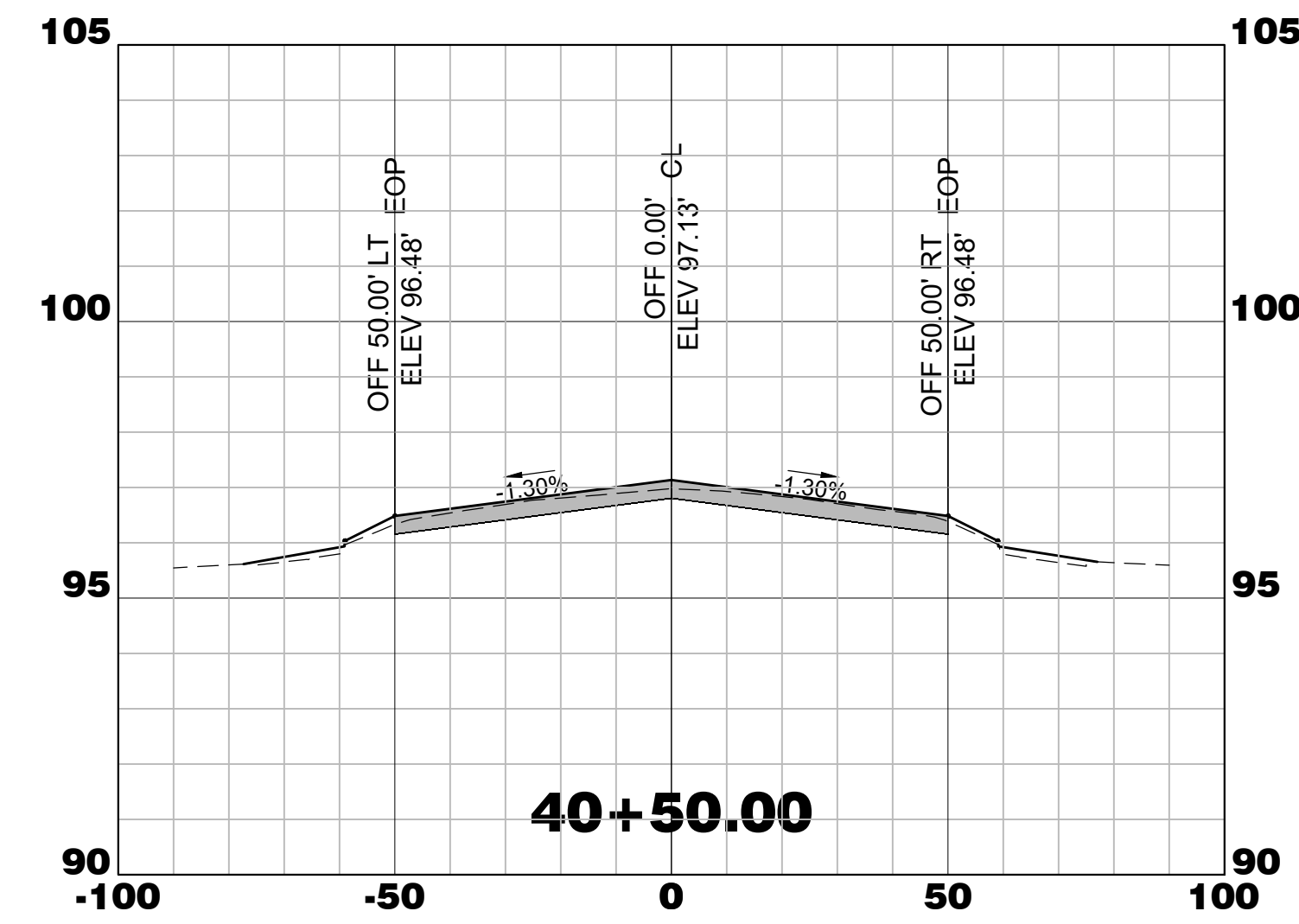
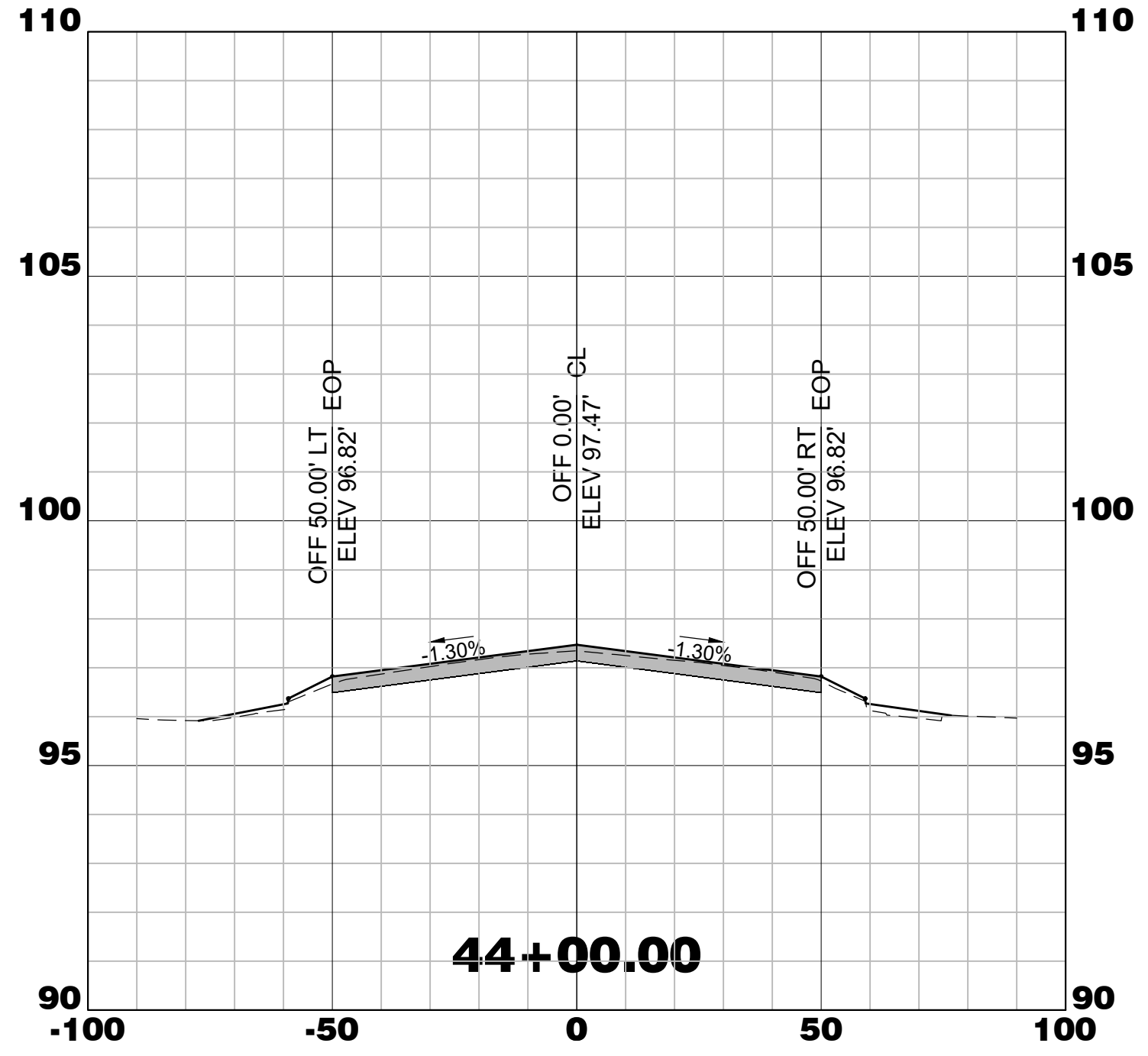
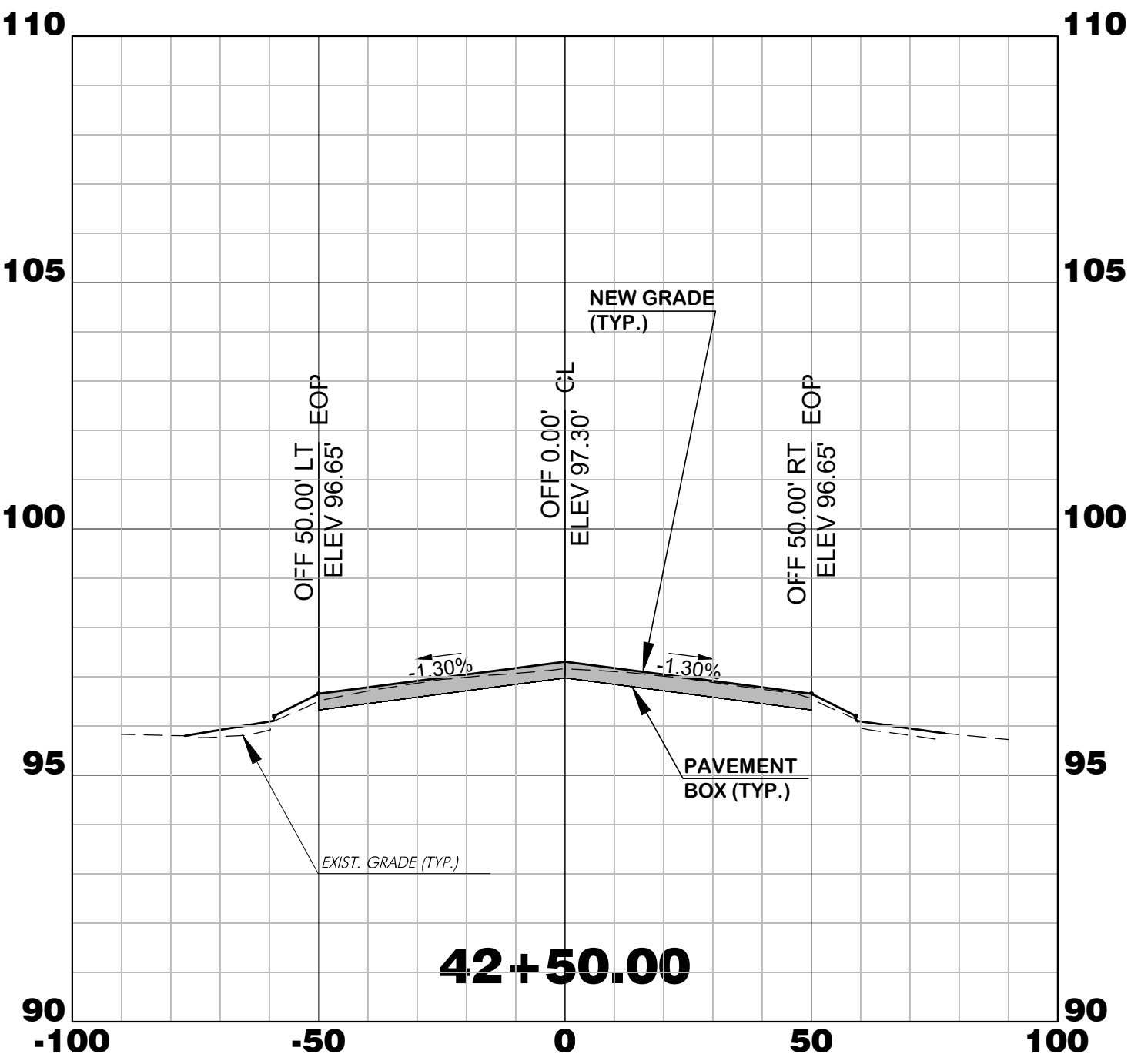
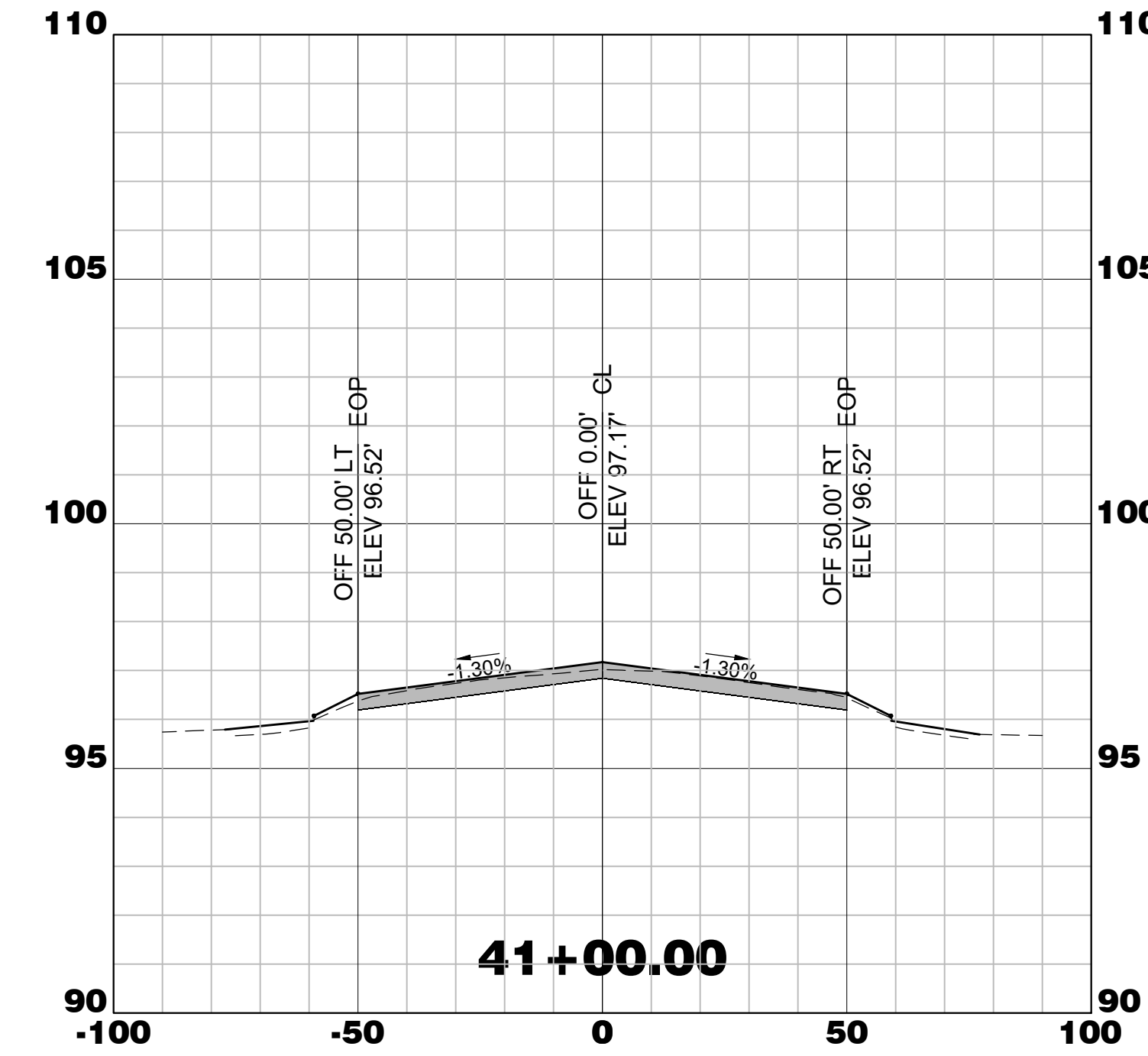
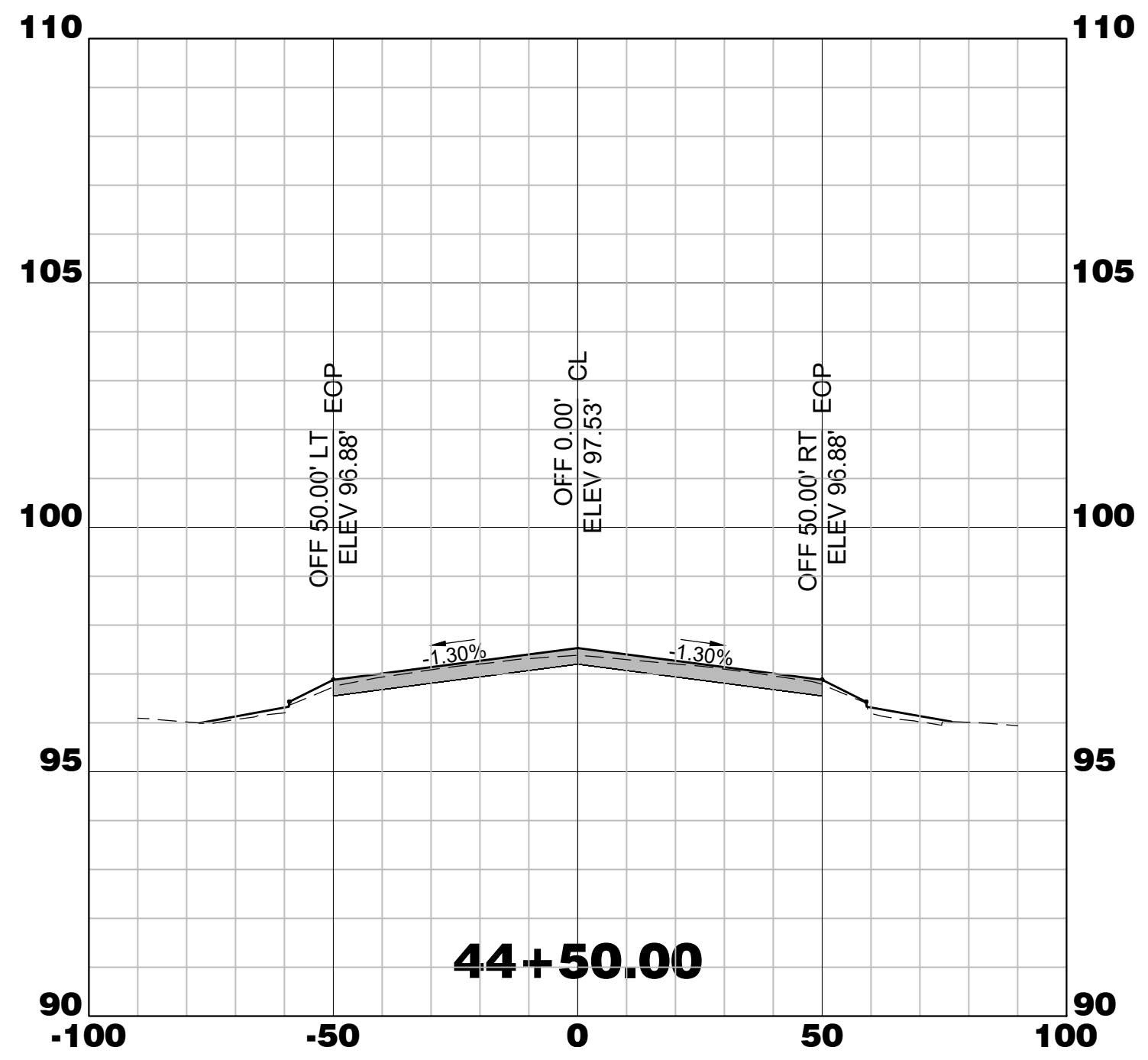
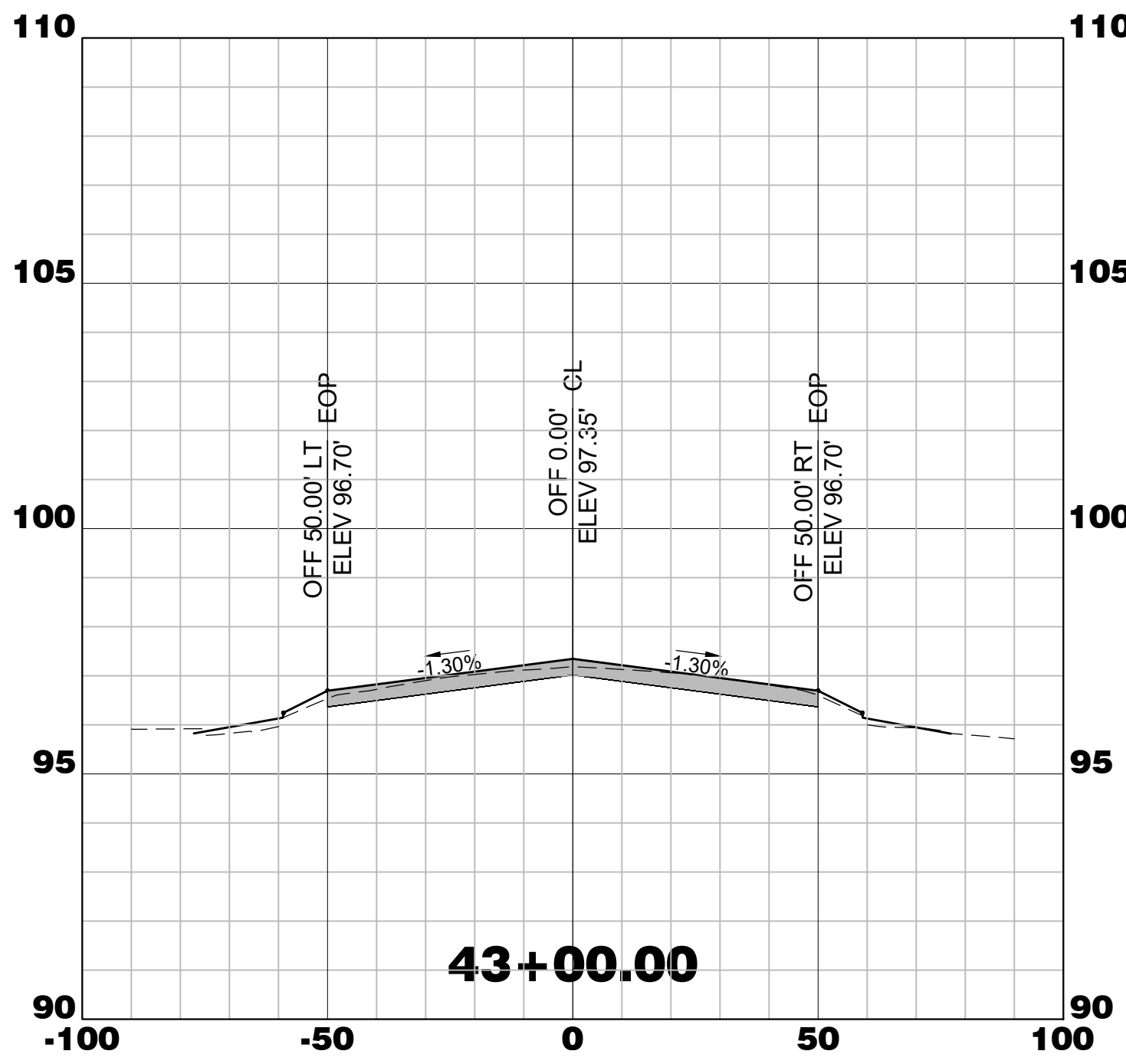
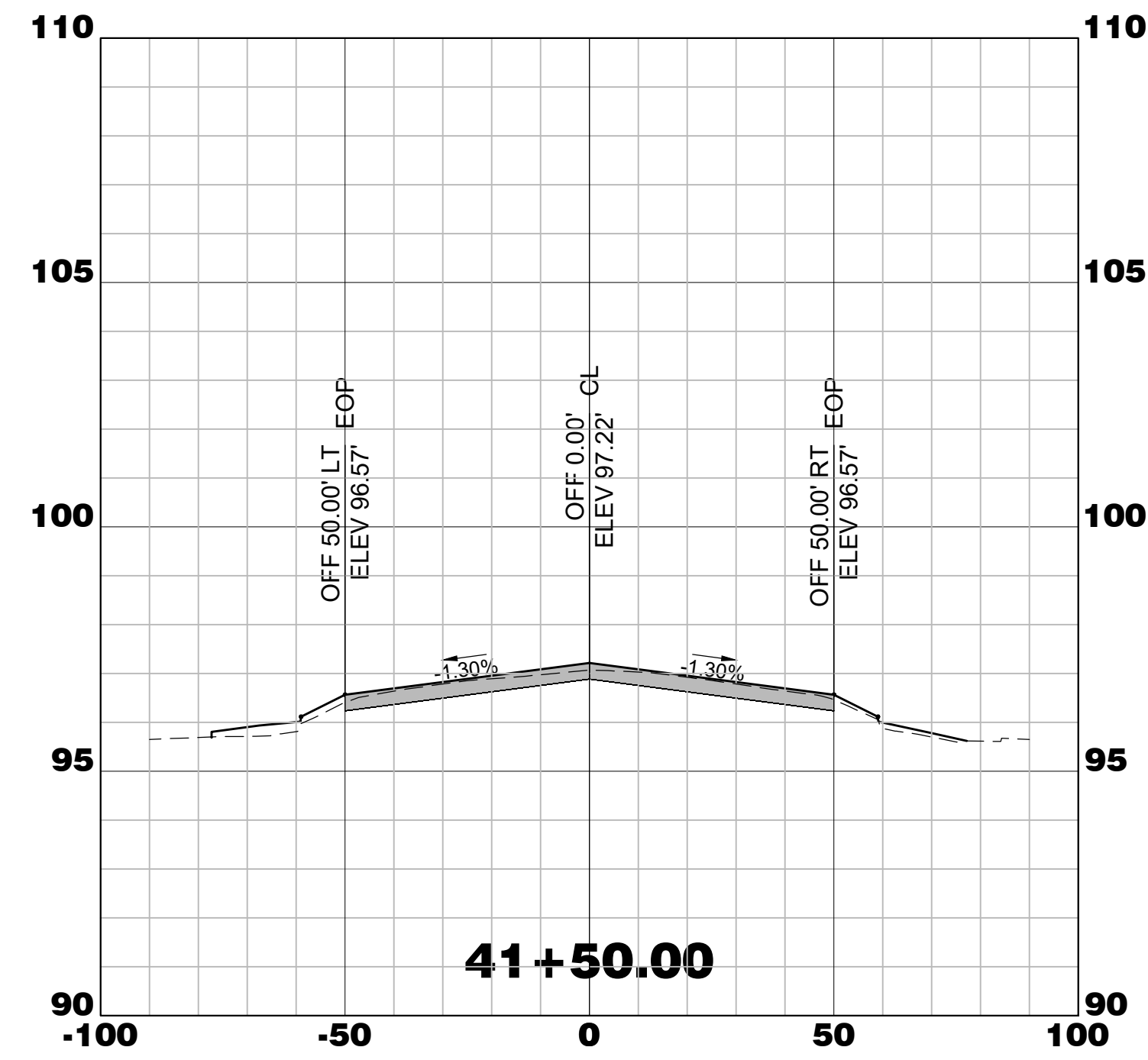
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 CROSS SECTIONS STA. 36+00 TO 40+00

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ.	Sheet Number:
1"=5' VERT.	Drawing Number: X-9

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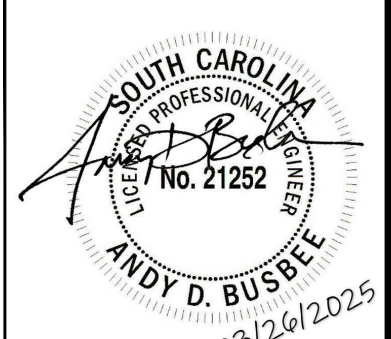


WALTERBORO, SOUTH CAROLINA

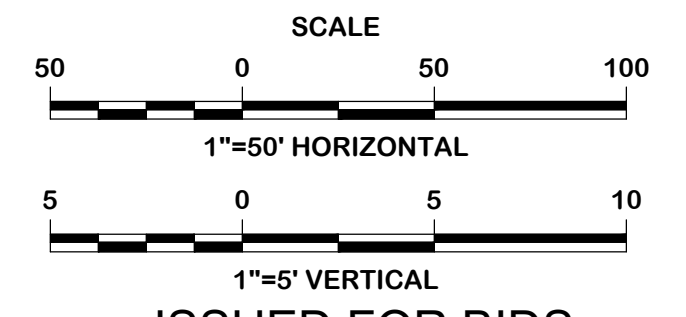
Michael Baker

INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

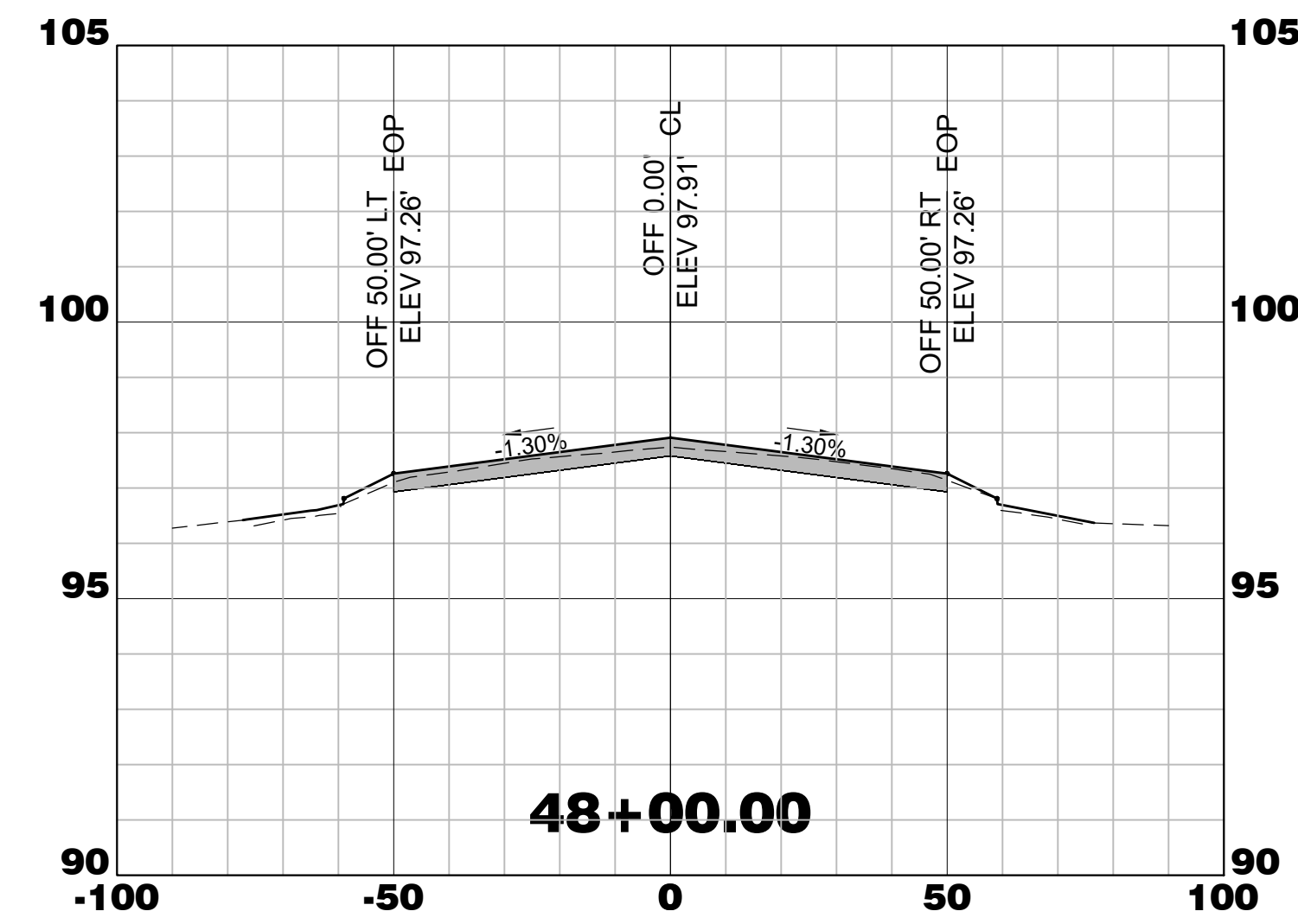
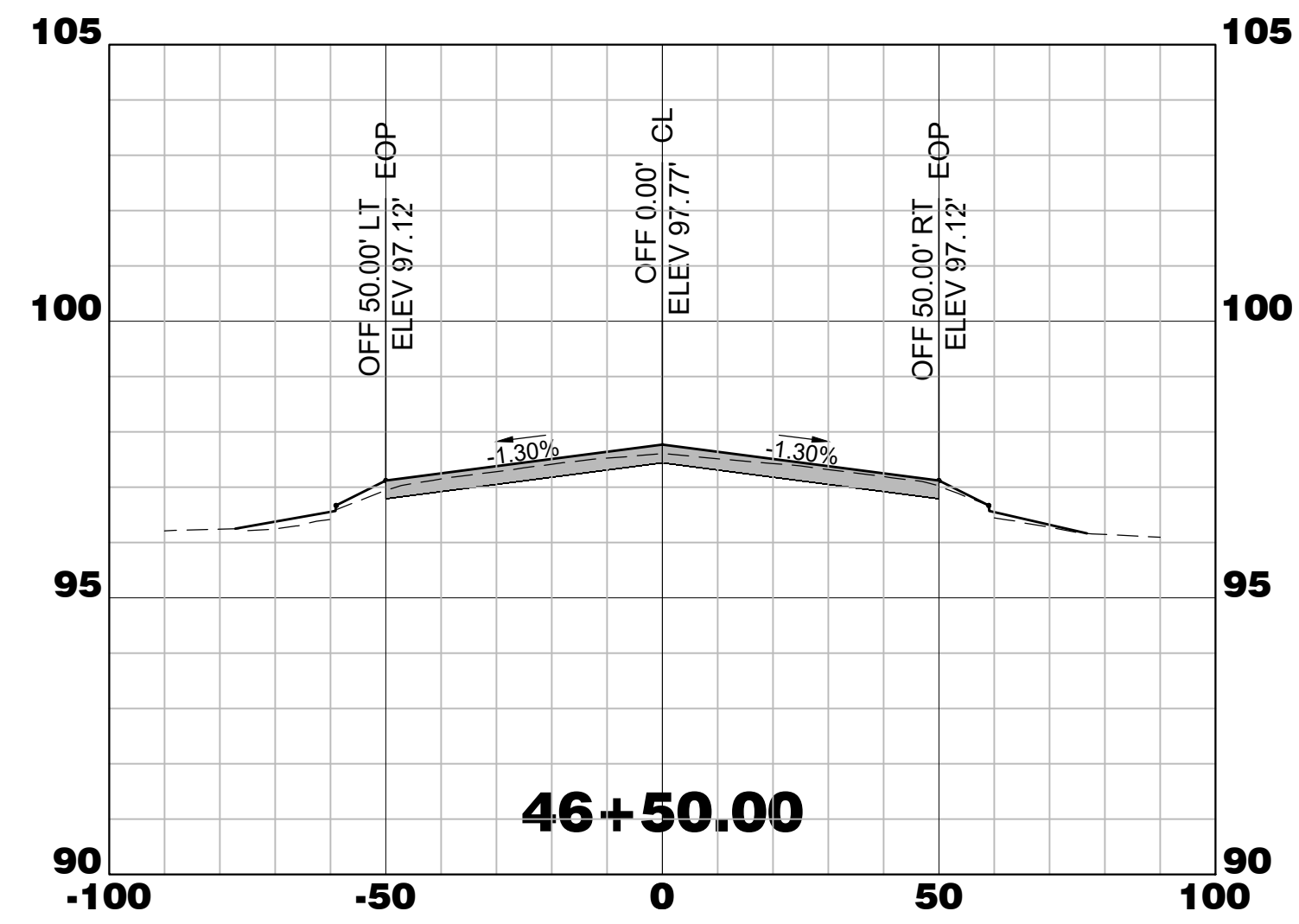
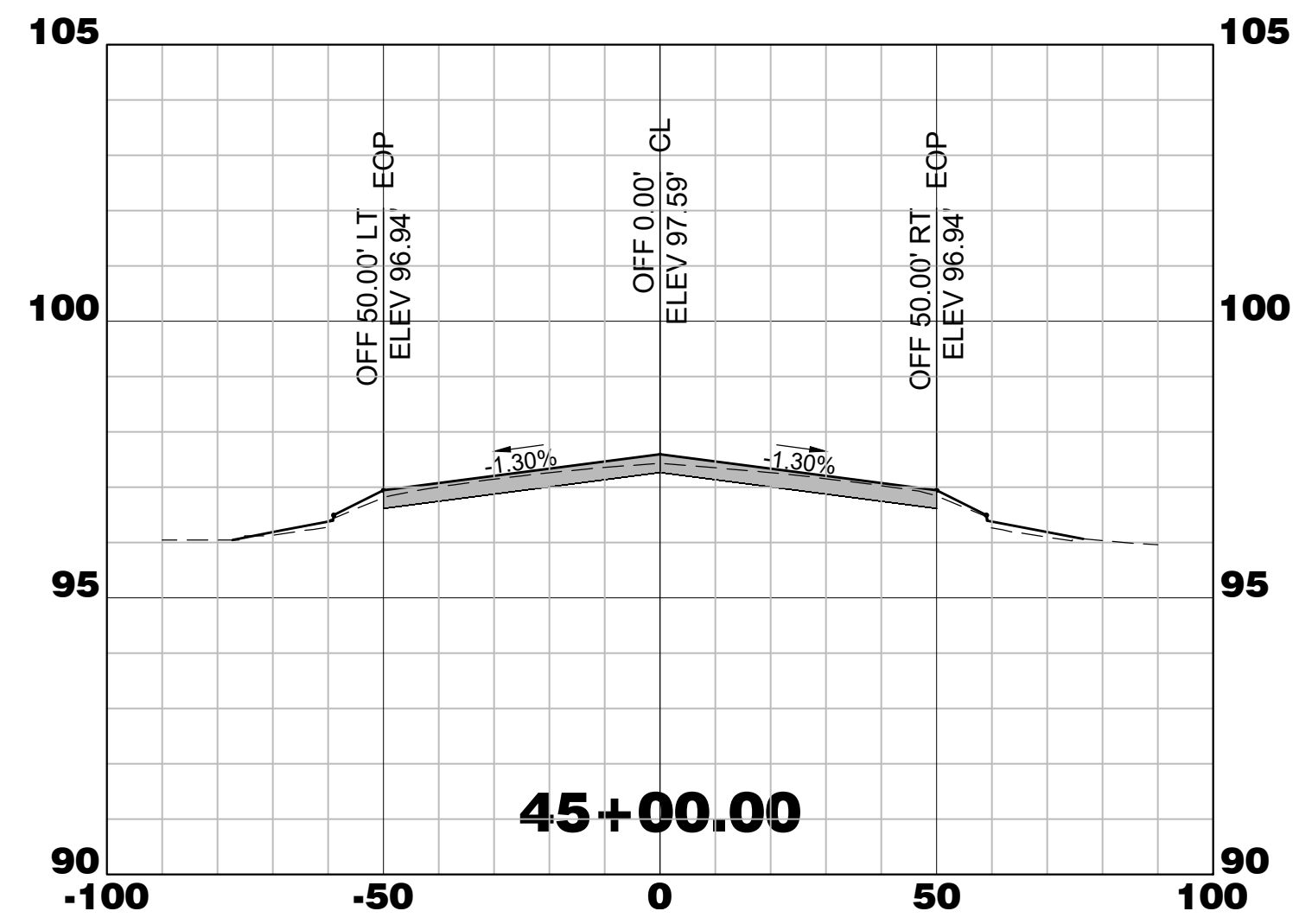
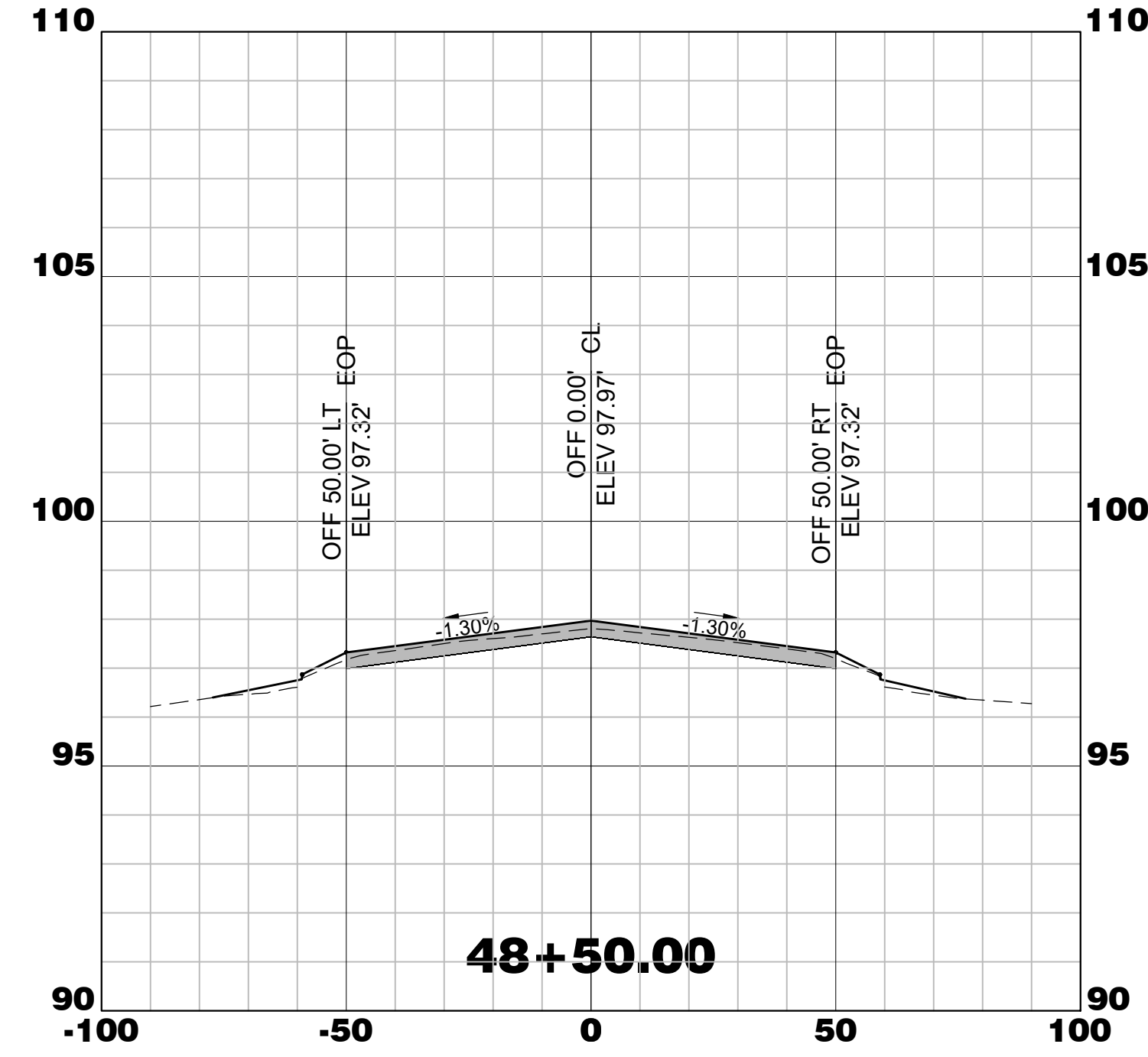
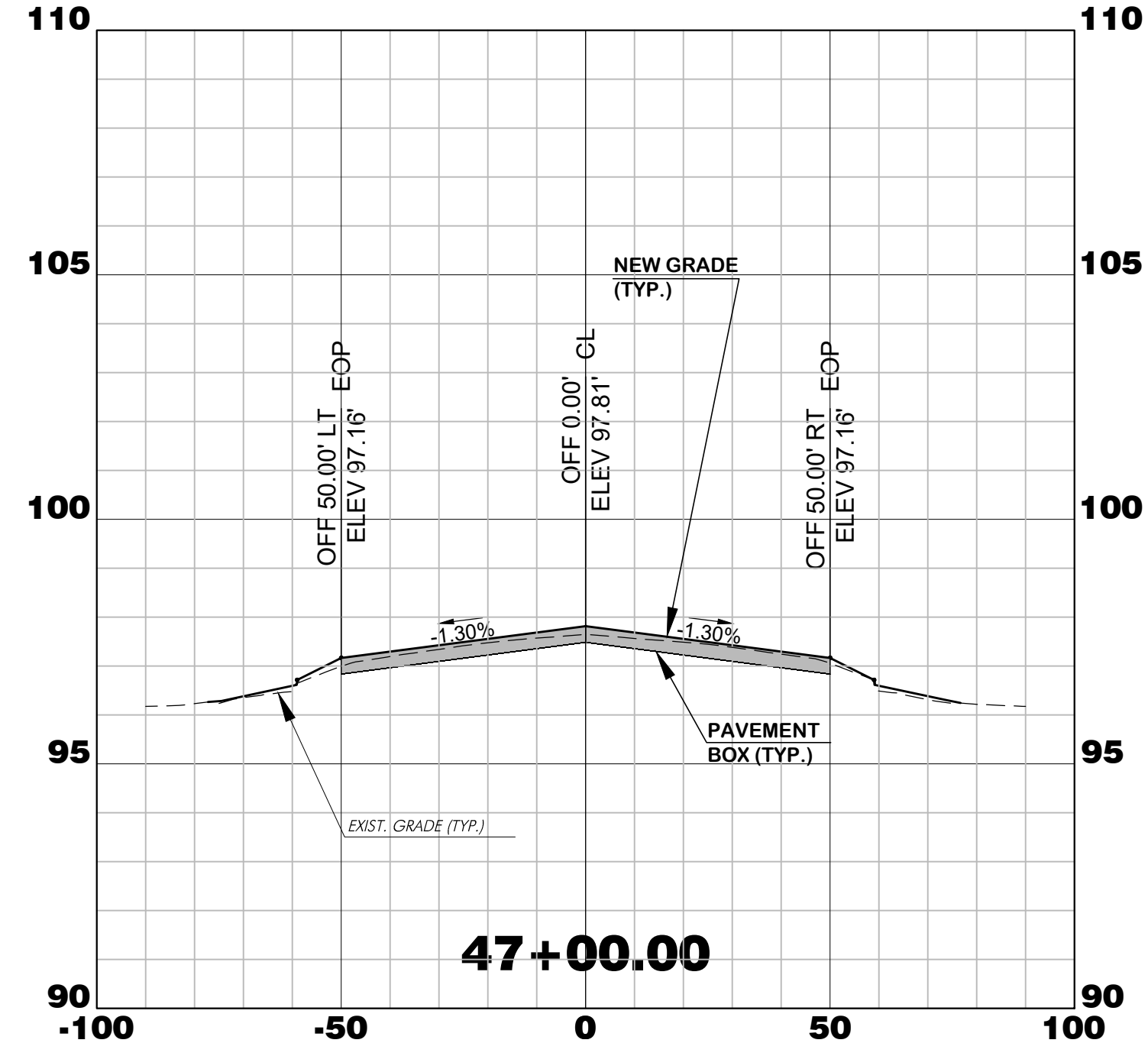
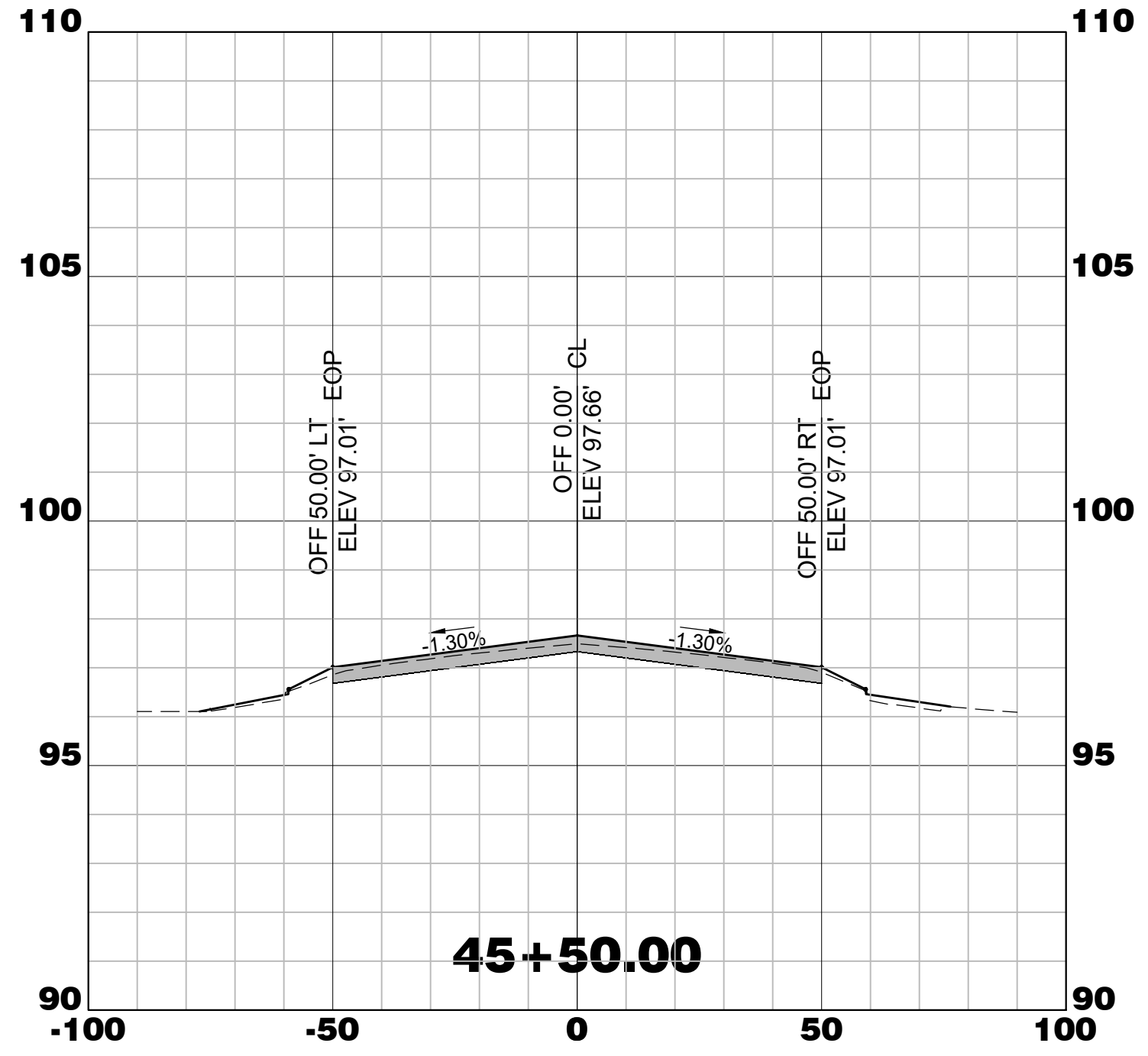
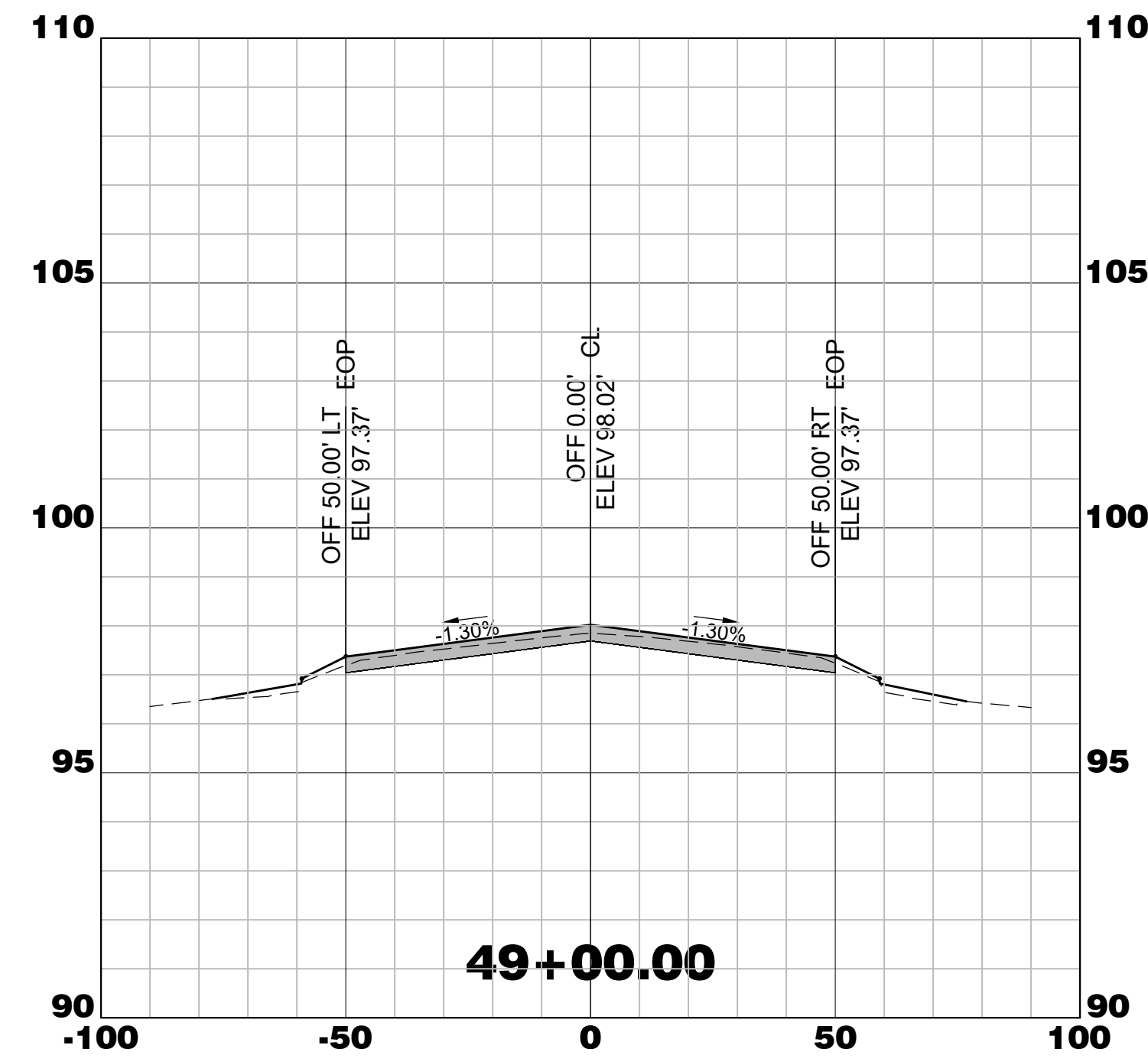
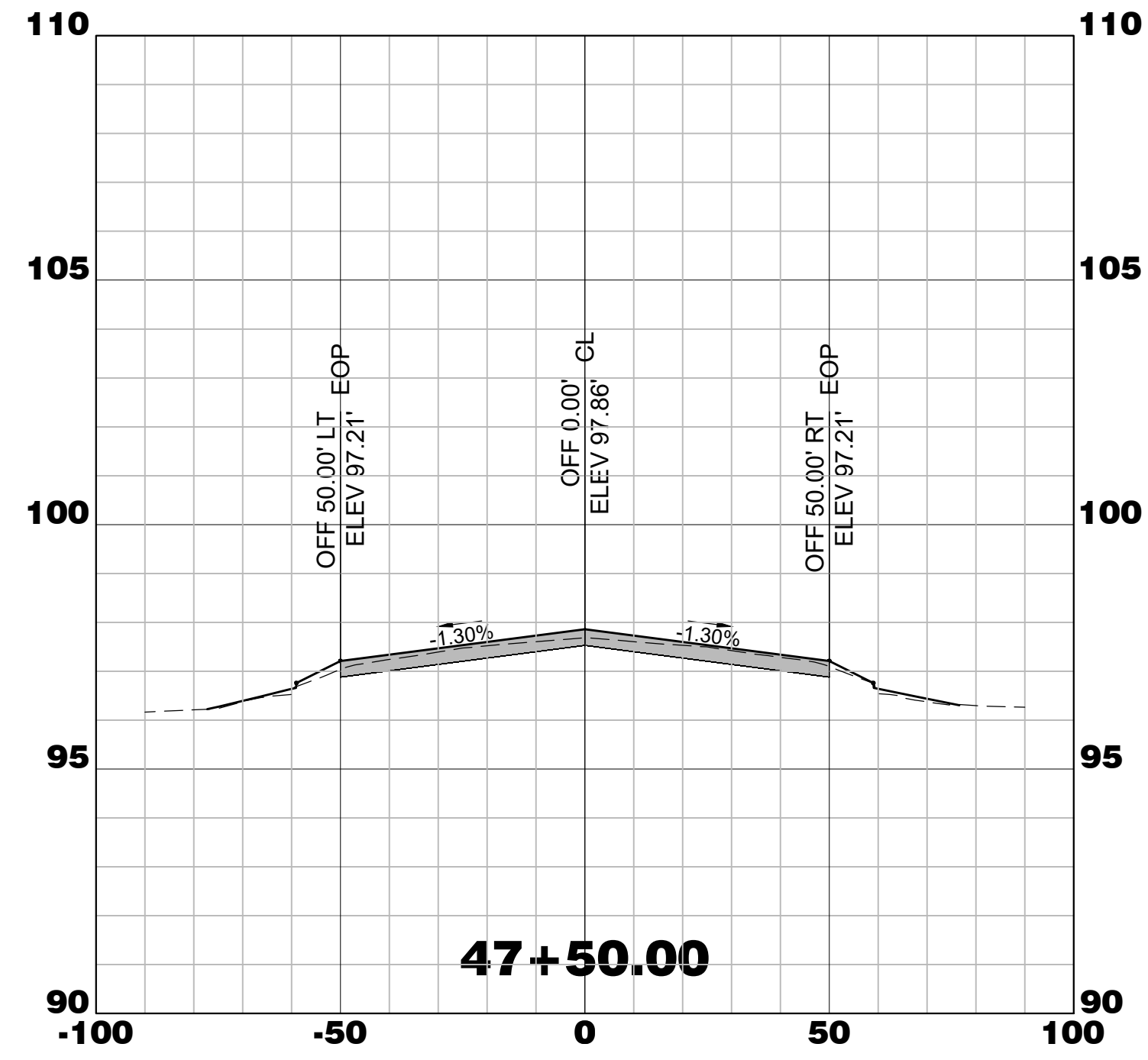
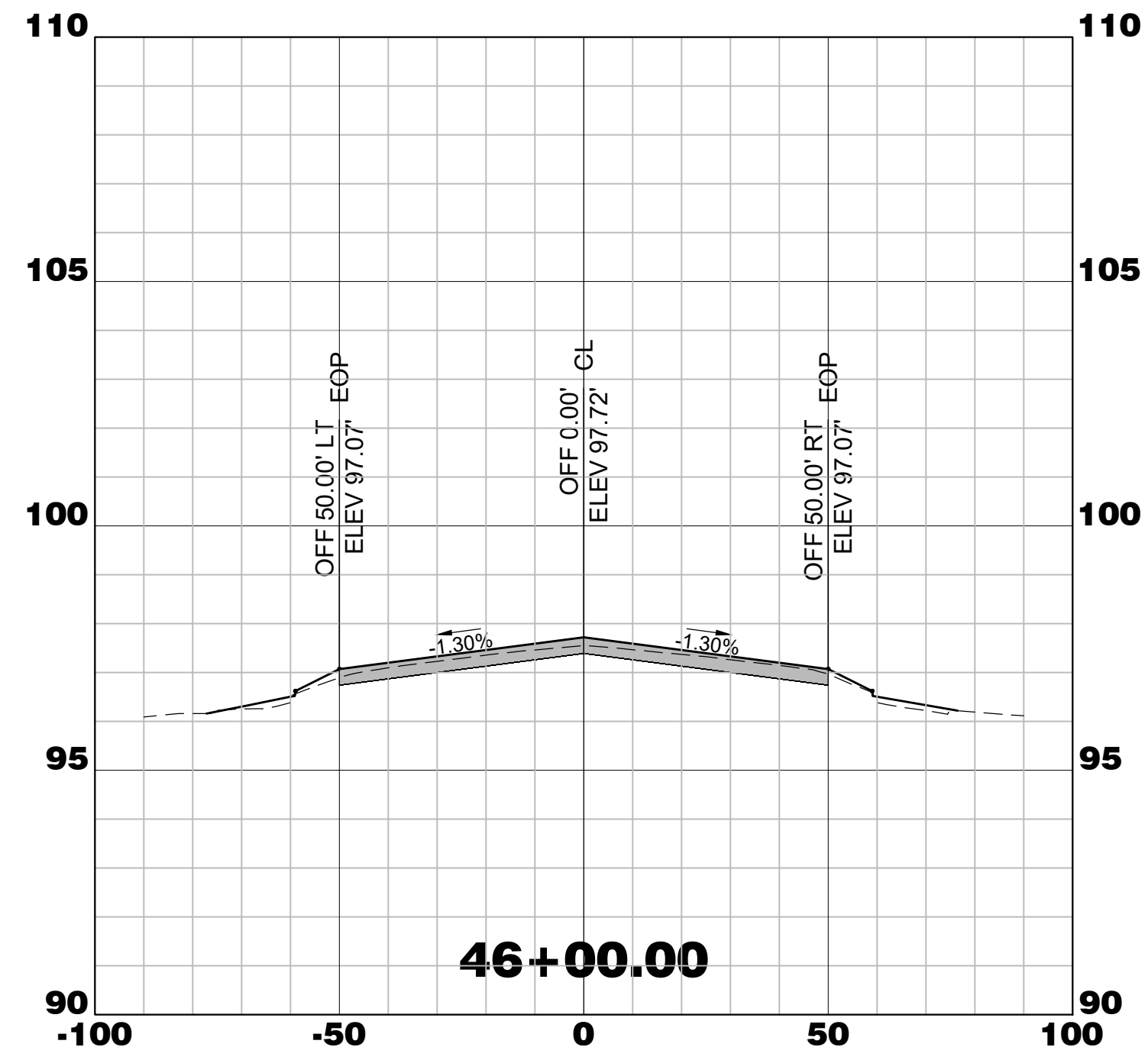
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 CROSS SECTIONS STA. 40+00 TO 44+50

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ. 1"=5' VERT.	Sheet Number: X-10

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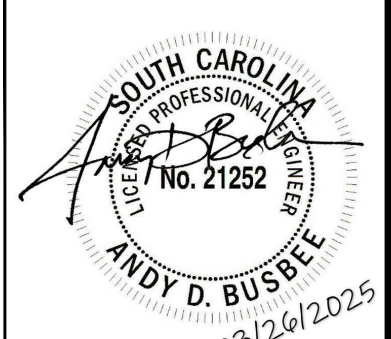


WALTERBORO, SOUTH CAROLINA

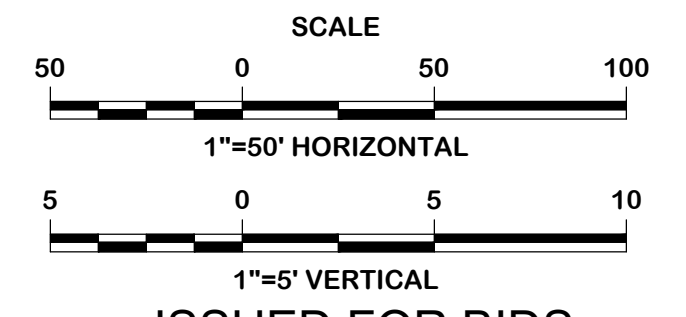
Michael Baker

INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
RUNWAY 5-23 CROSS SECTIONS STA. 45+00 TO 49+00

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ.	Sheet Number:
1"=5' VERT.	Drawing Number: X-11



WALTERBORO, SOUTH CAROLINA

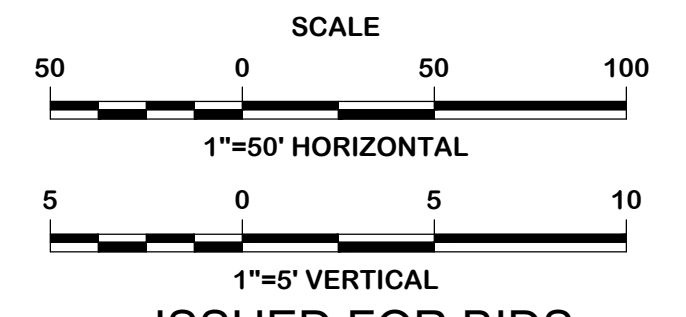
Michael Baker

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Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



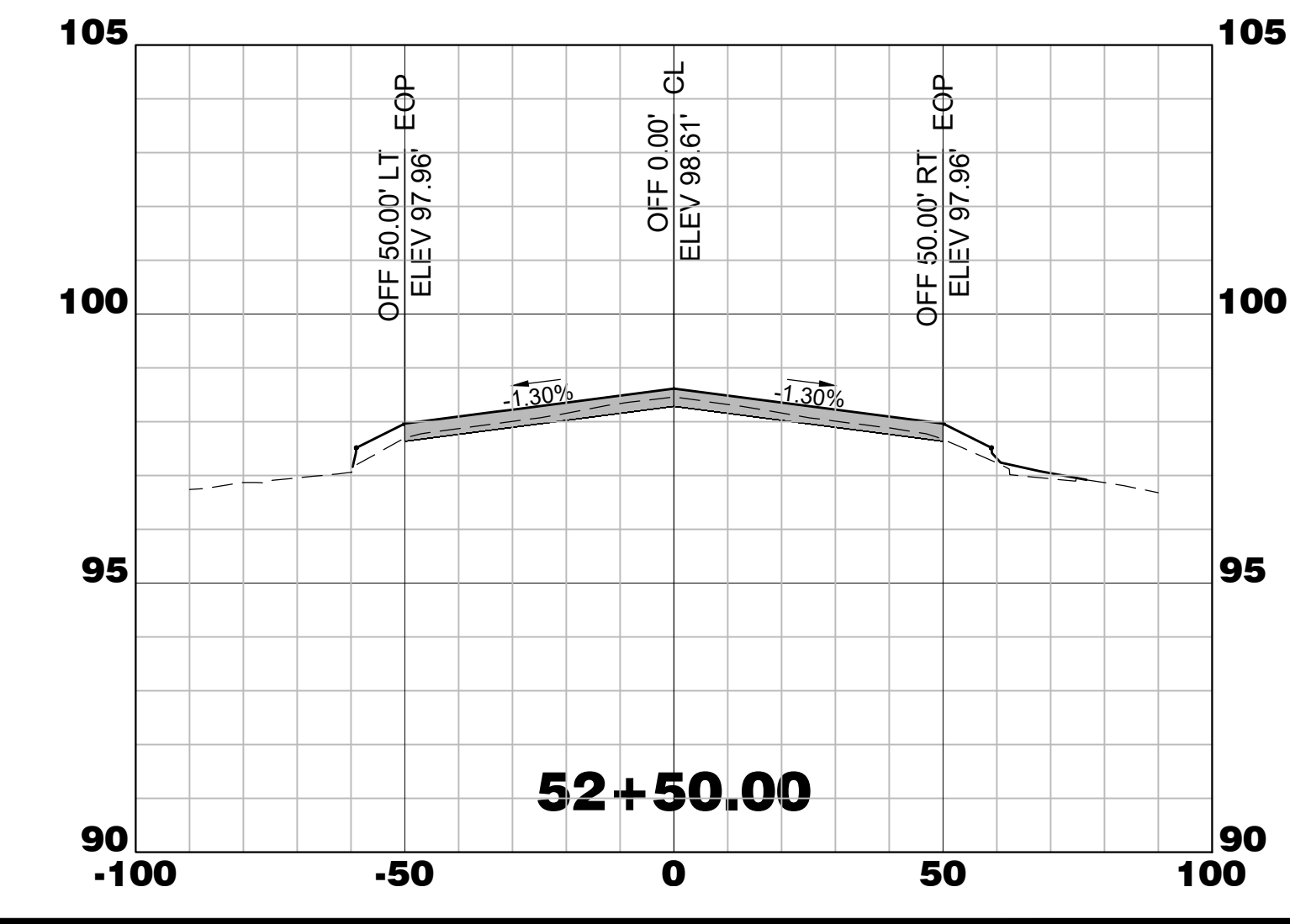
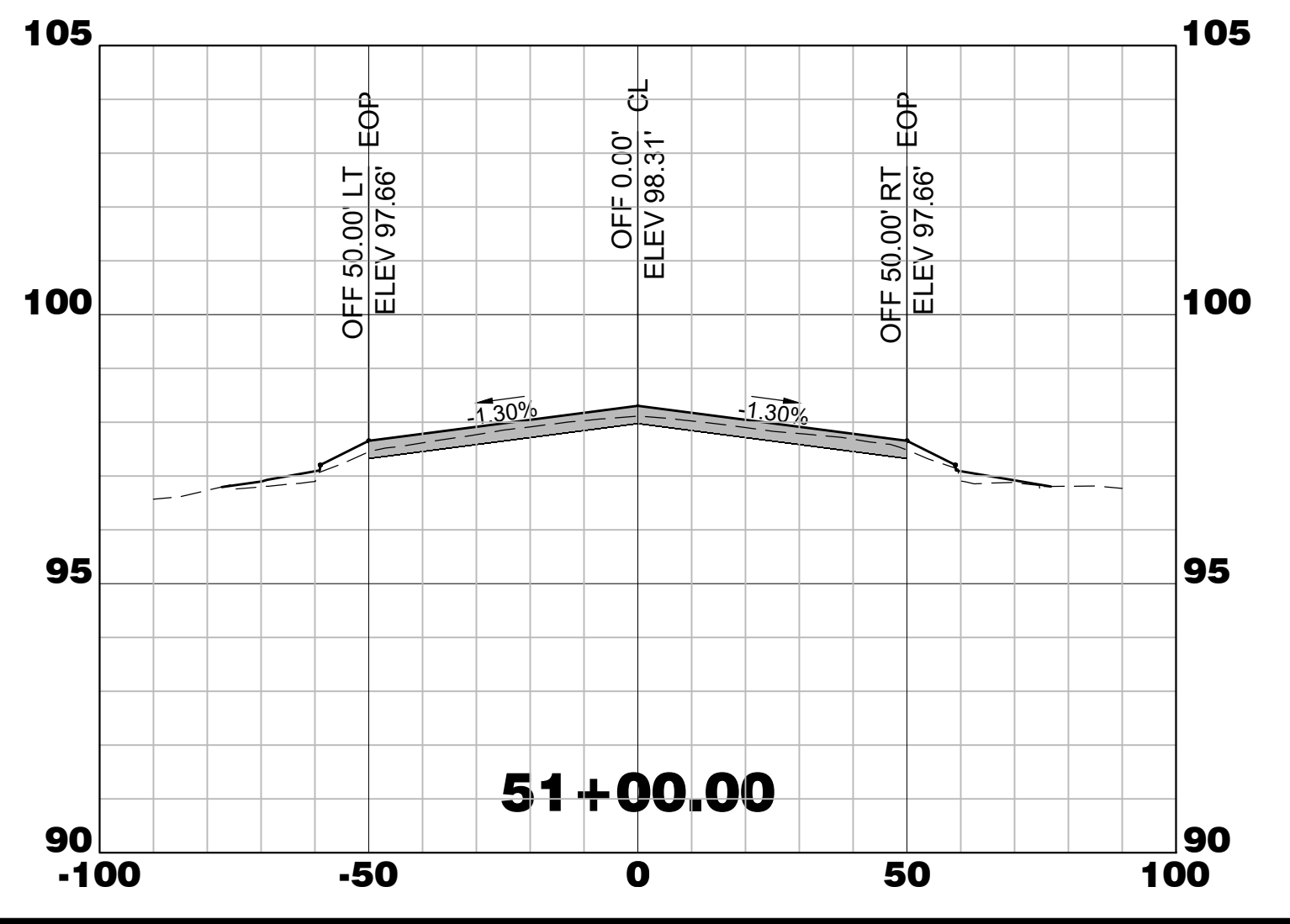
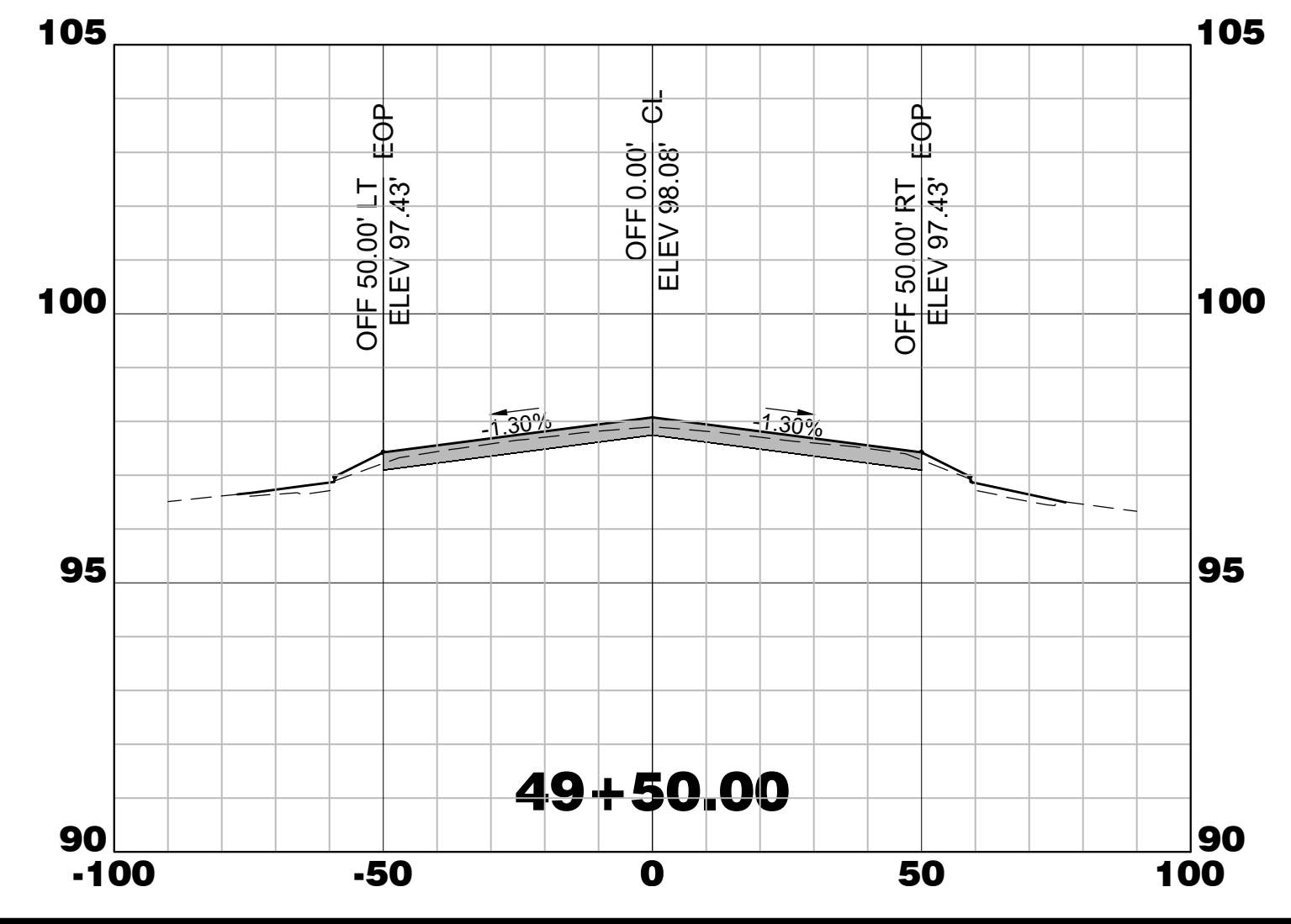
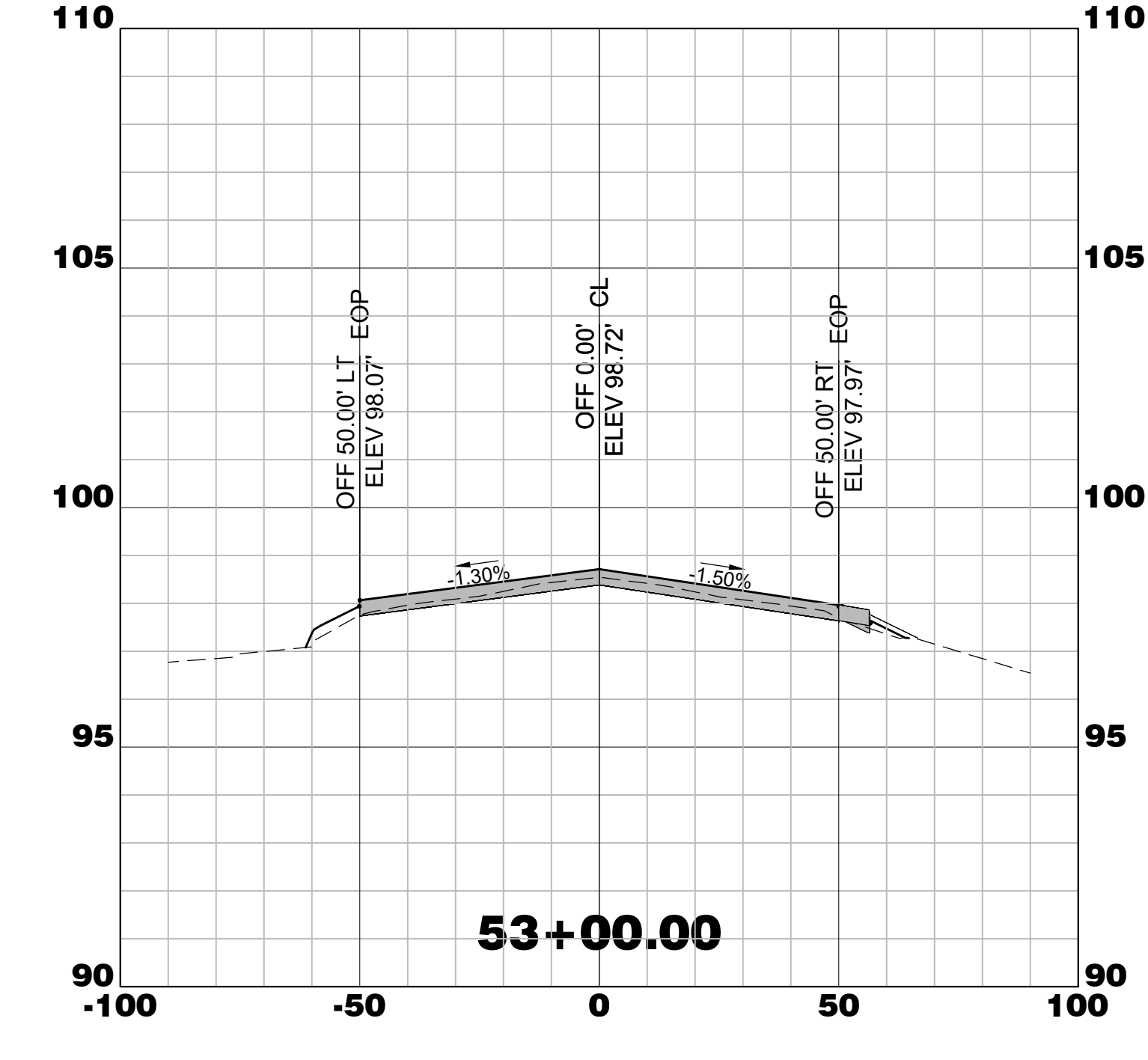
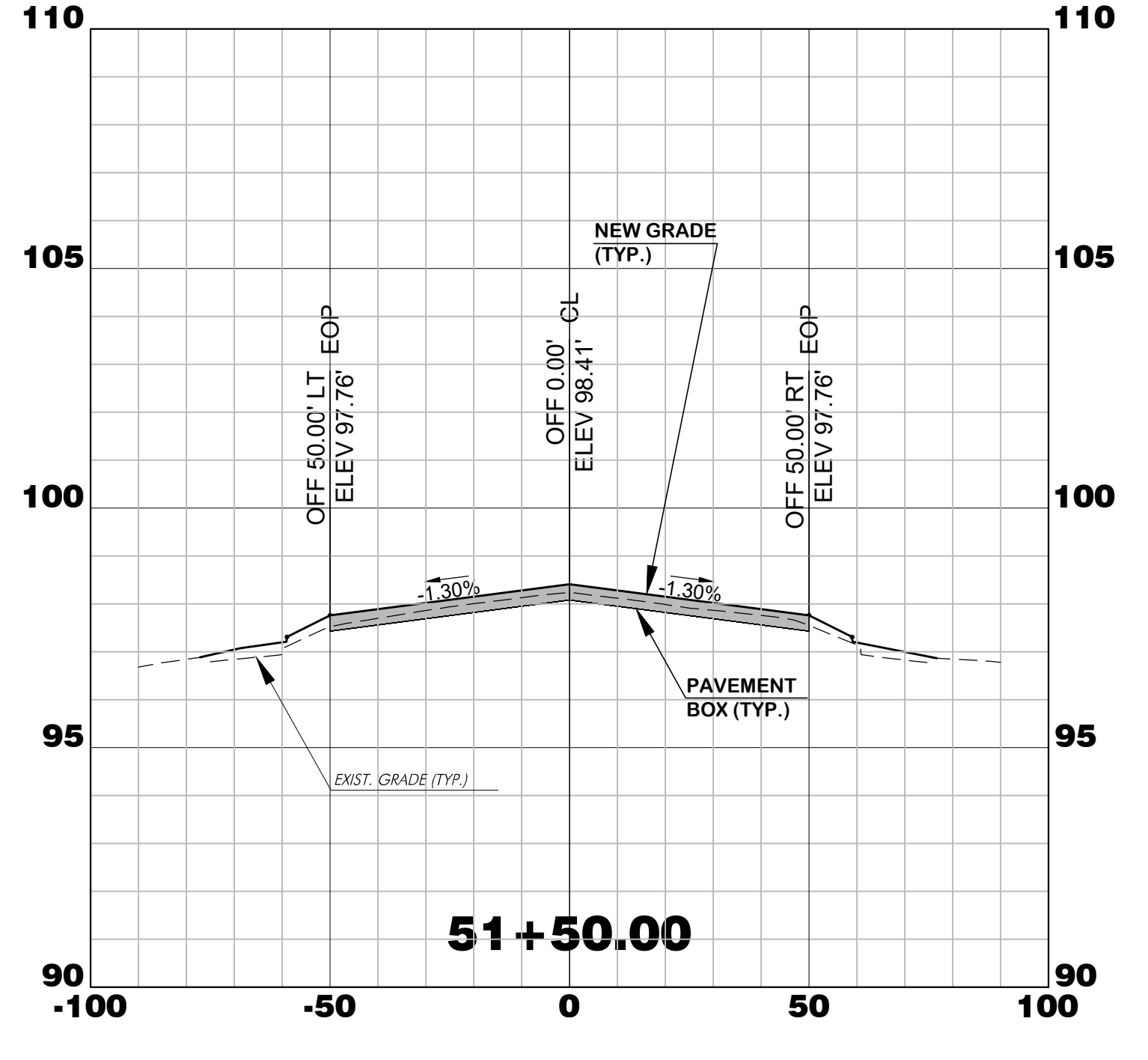
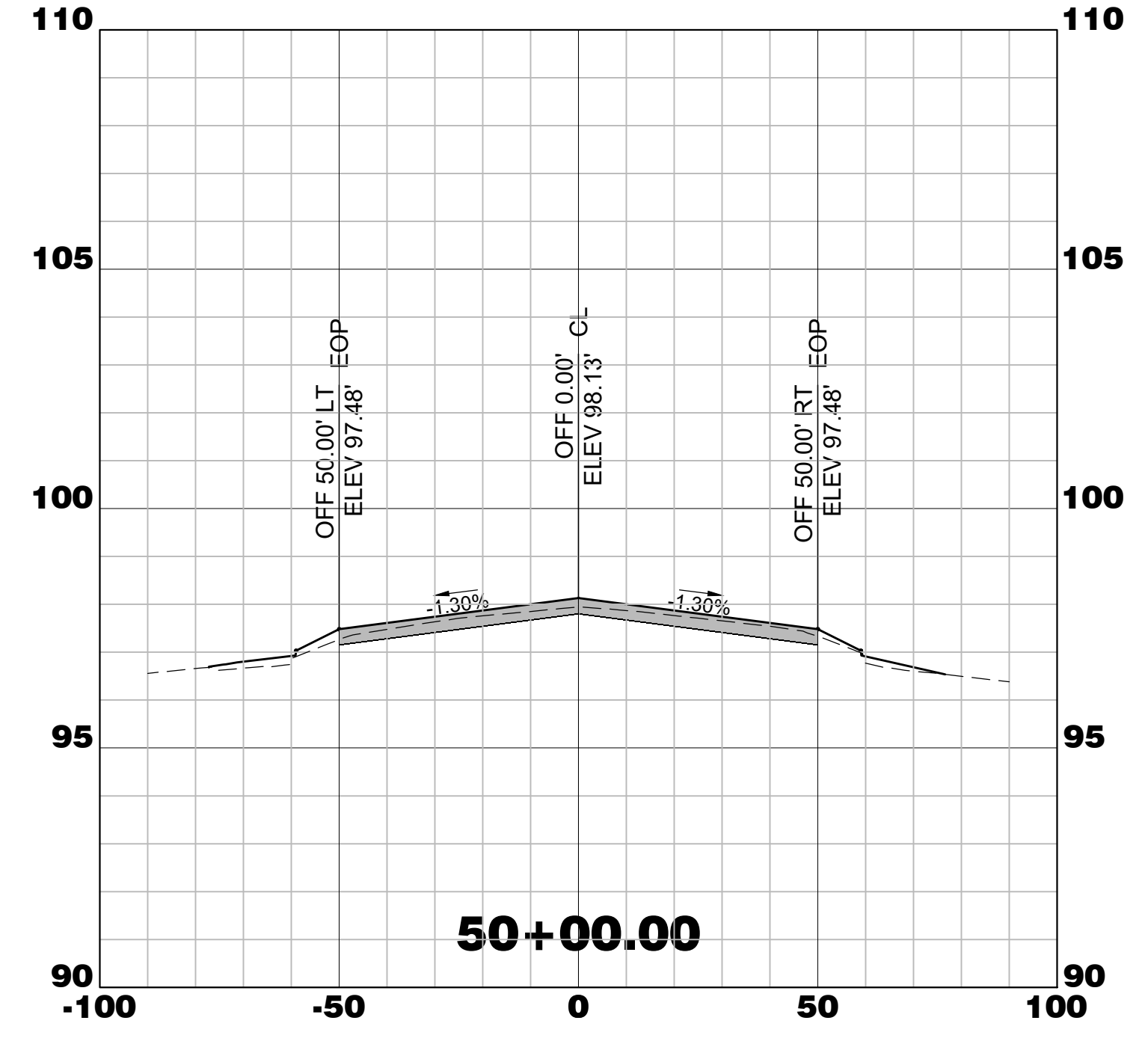
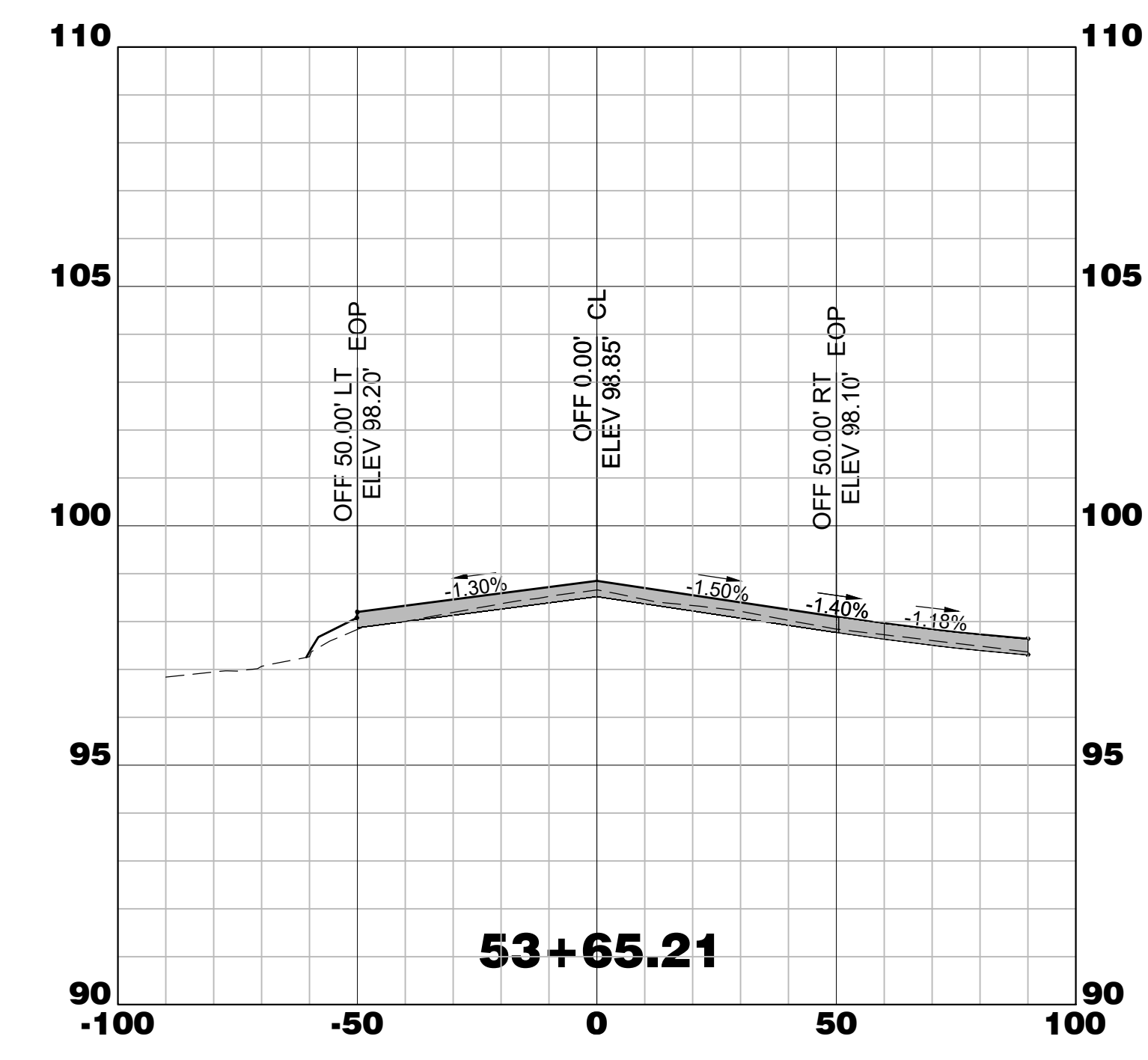
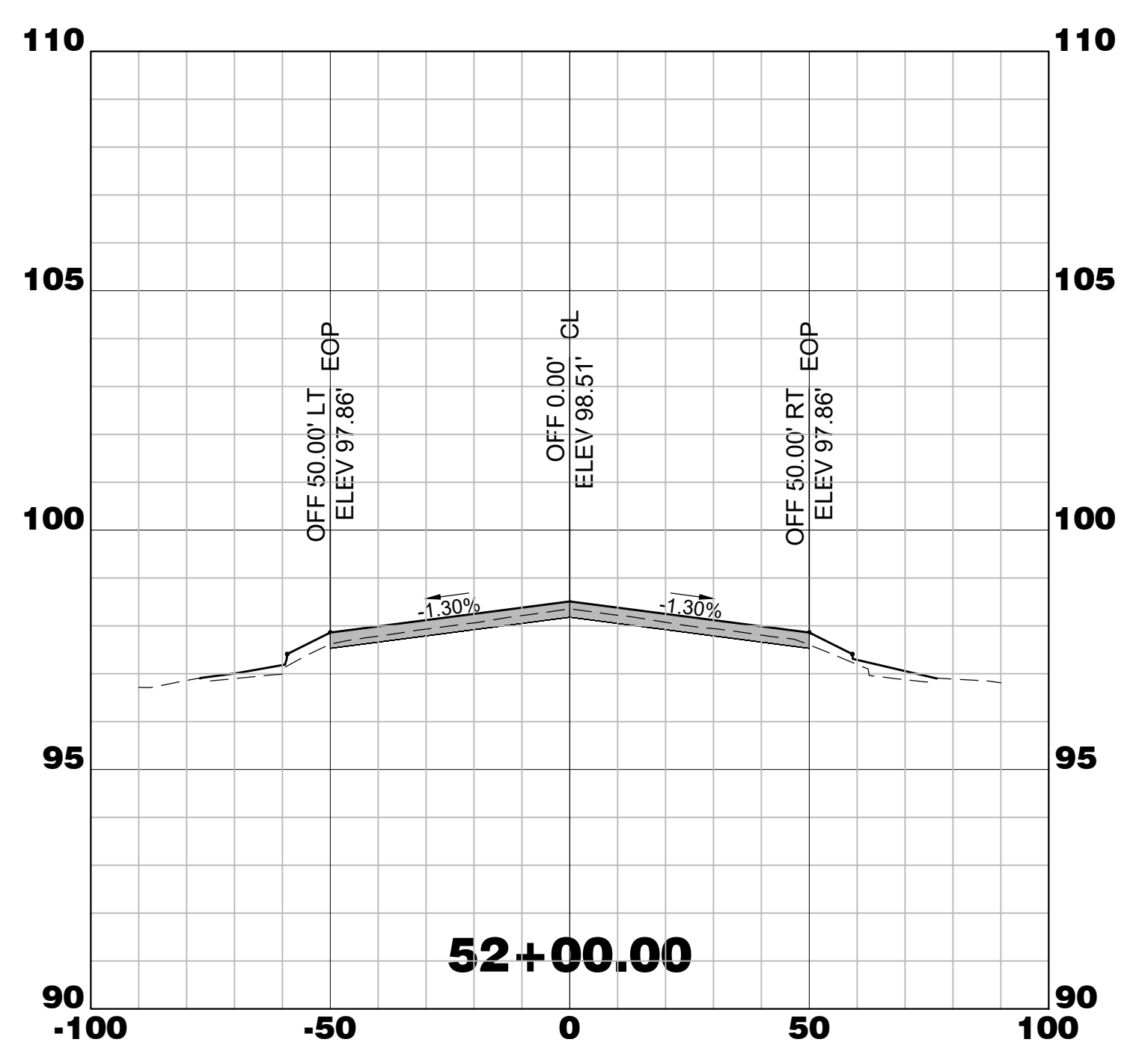
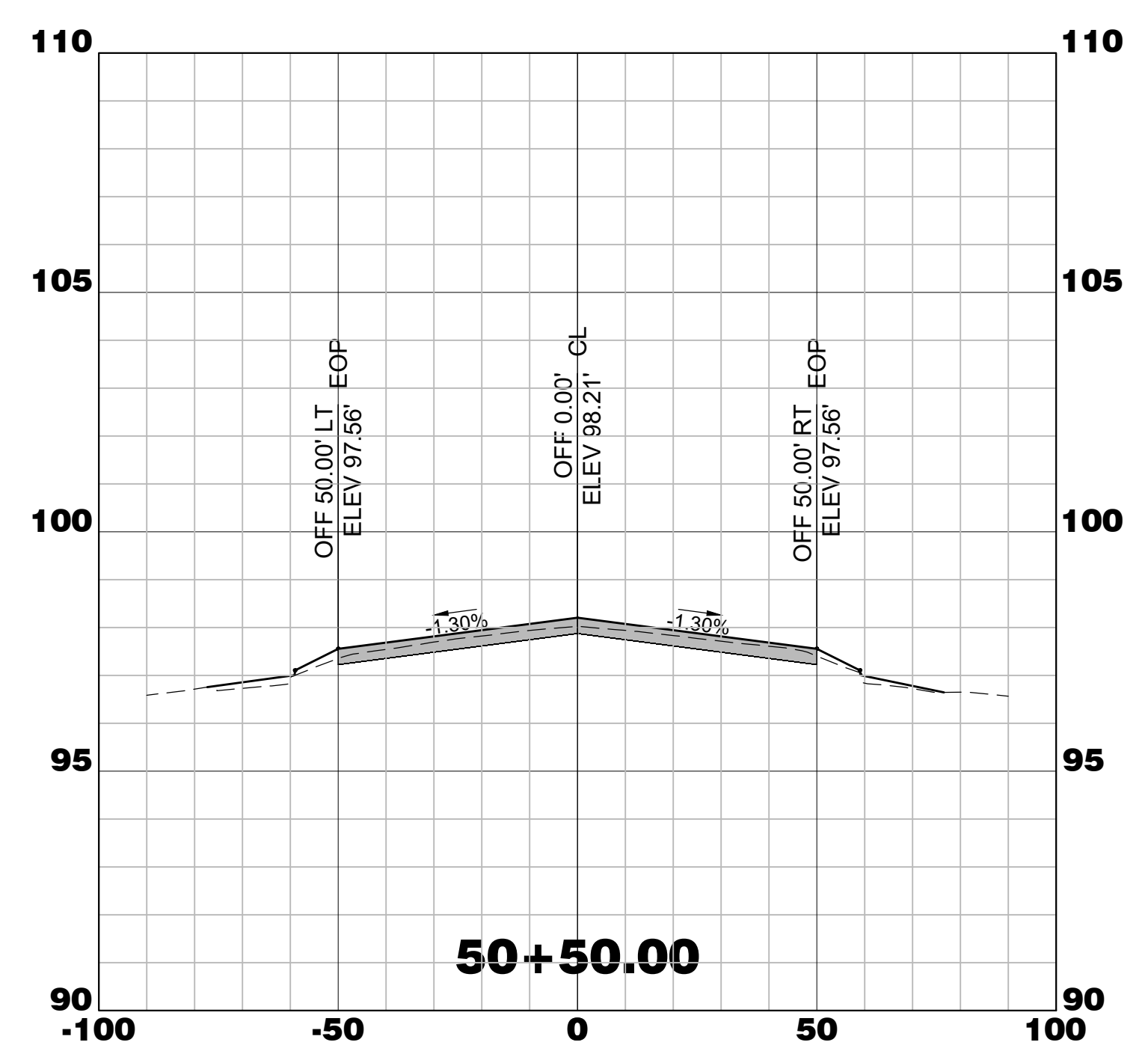
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
**RUNWAY 5-23 CROSS SECTIONS
STA. 49+50 TO 53+65.21**

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ. 1"=5' VERT.	Sheet Number: X-12



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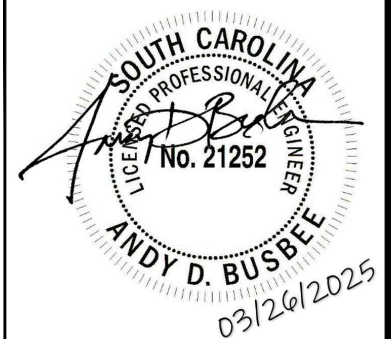


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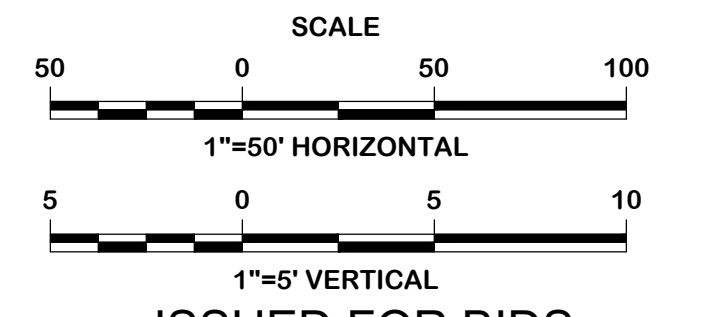
Michael Baker

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Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

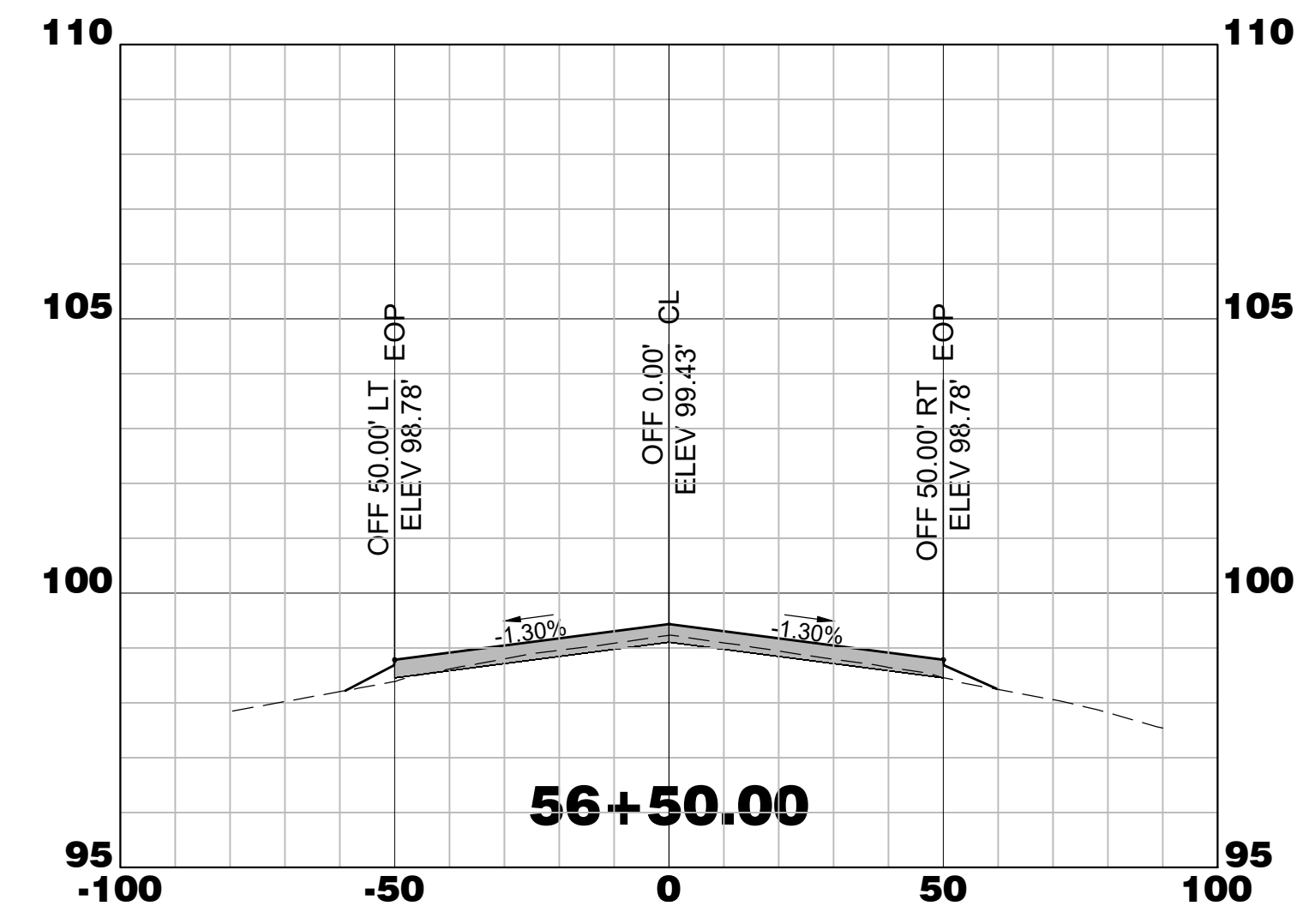
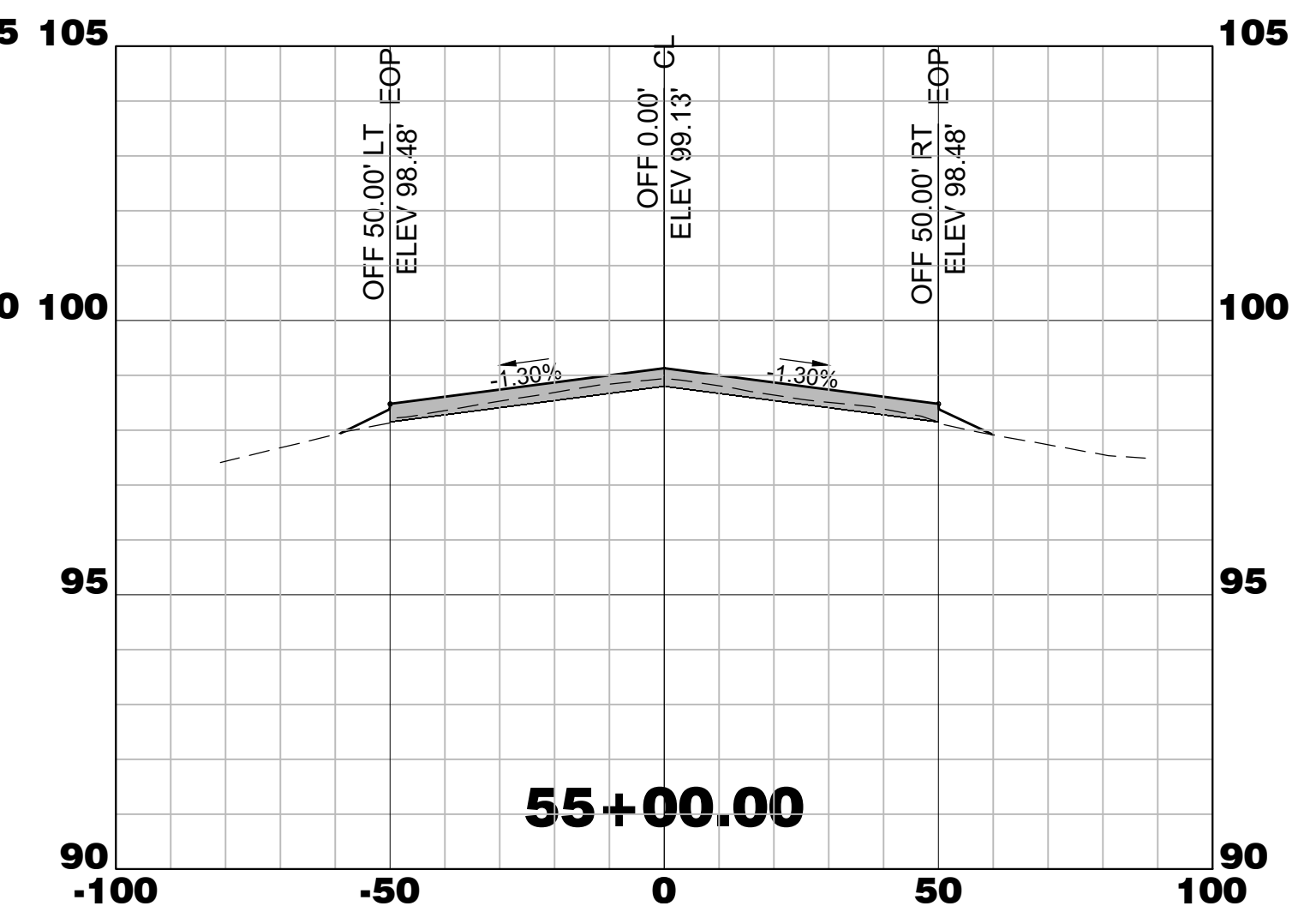
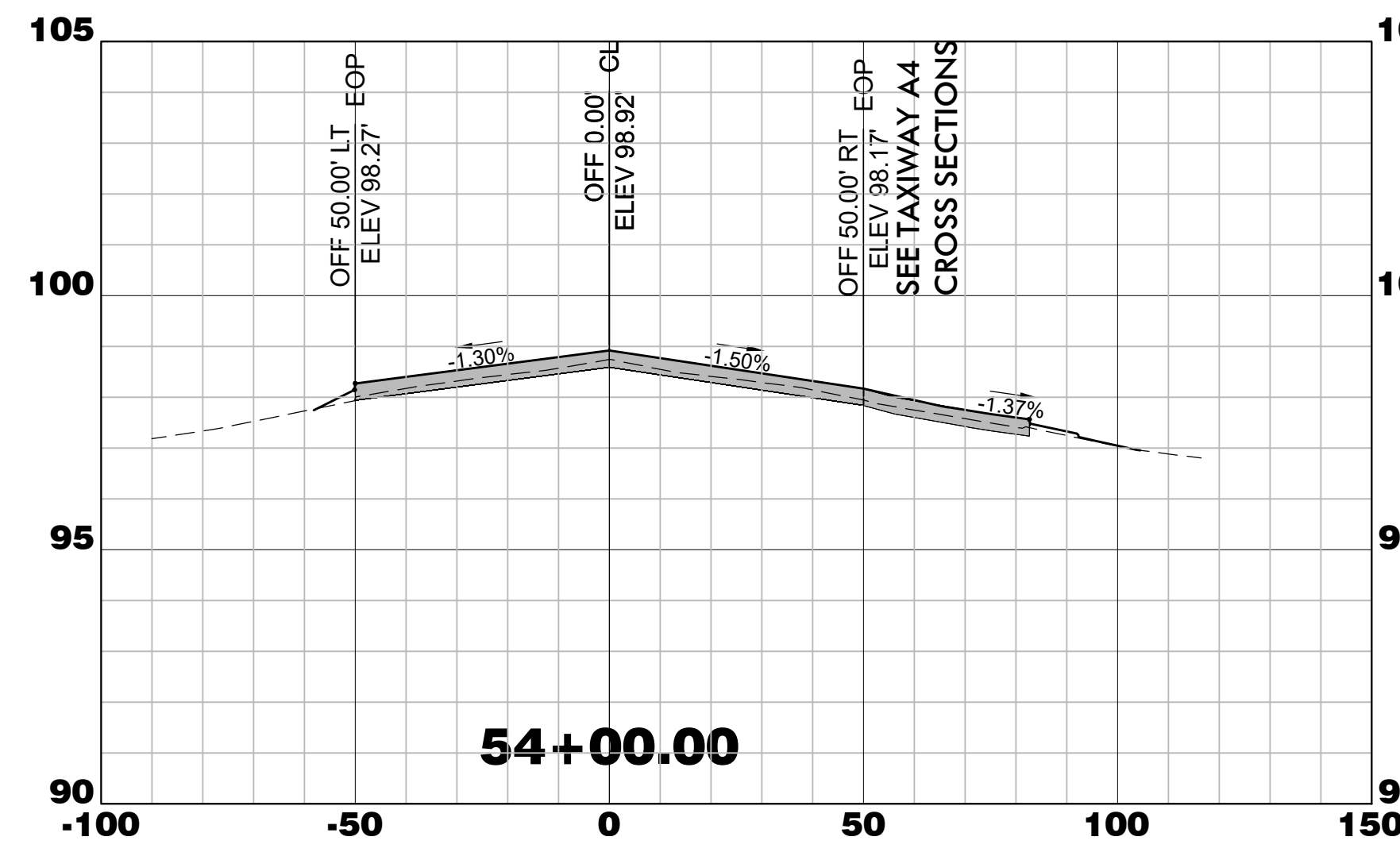
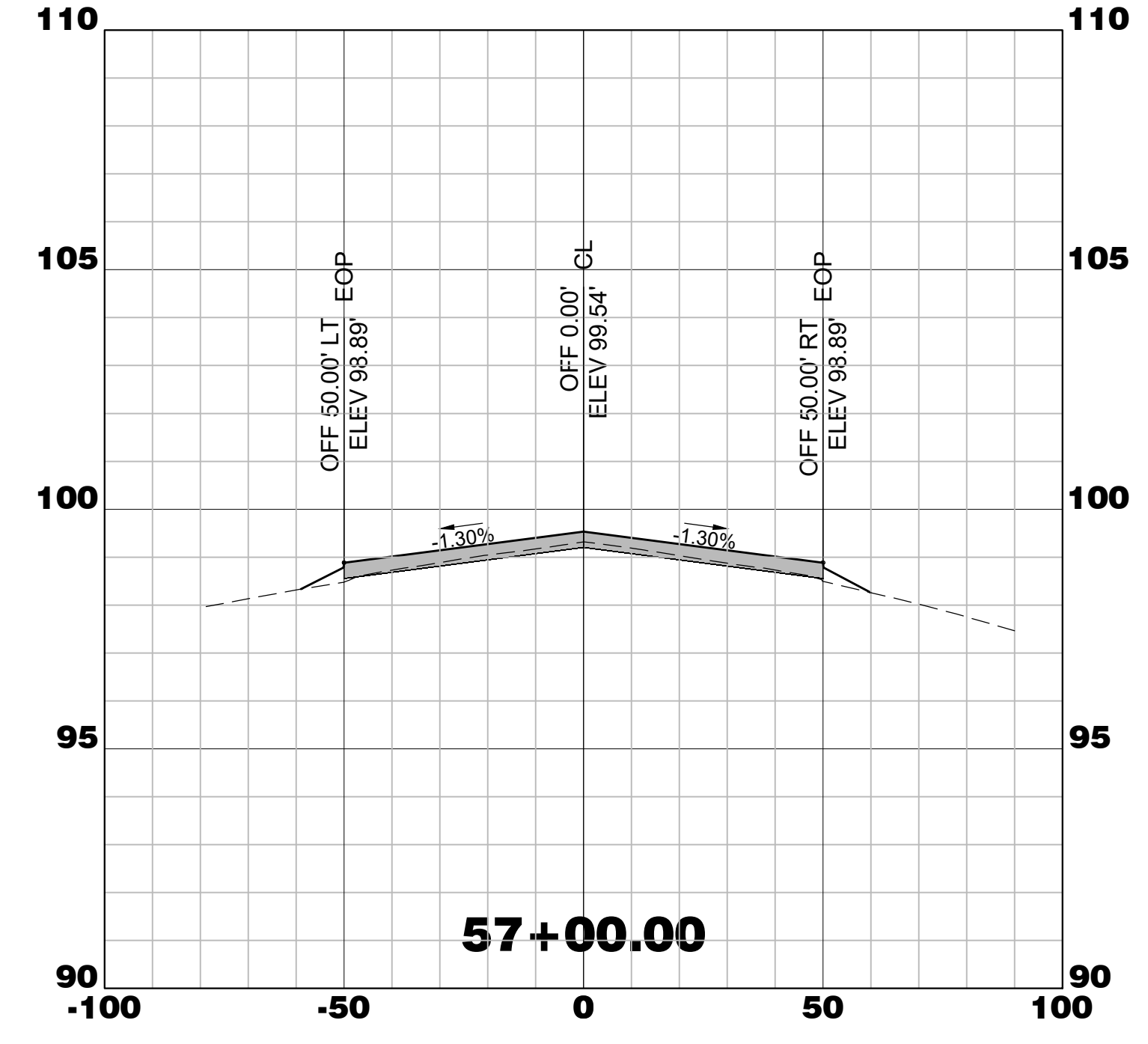
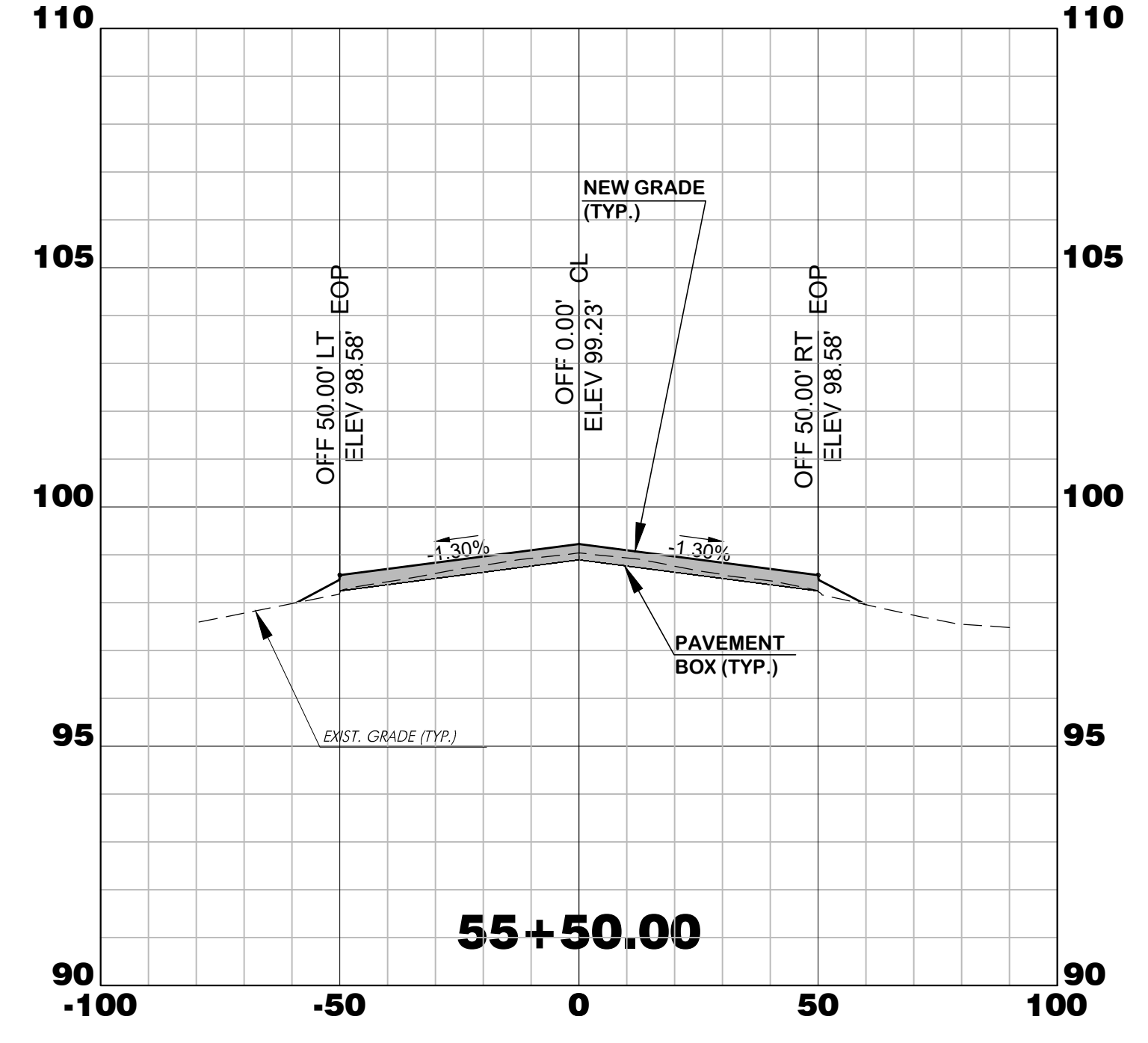
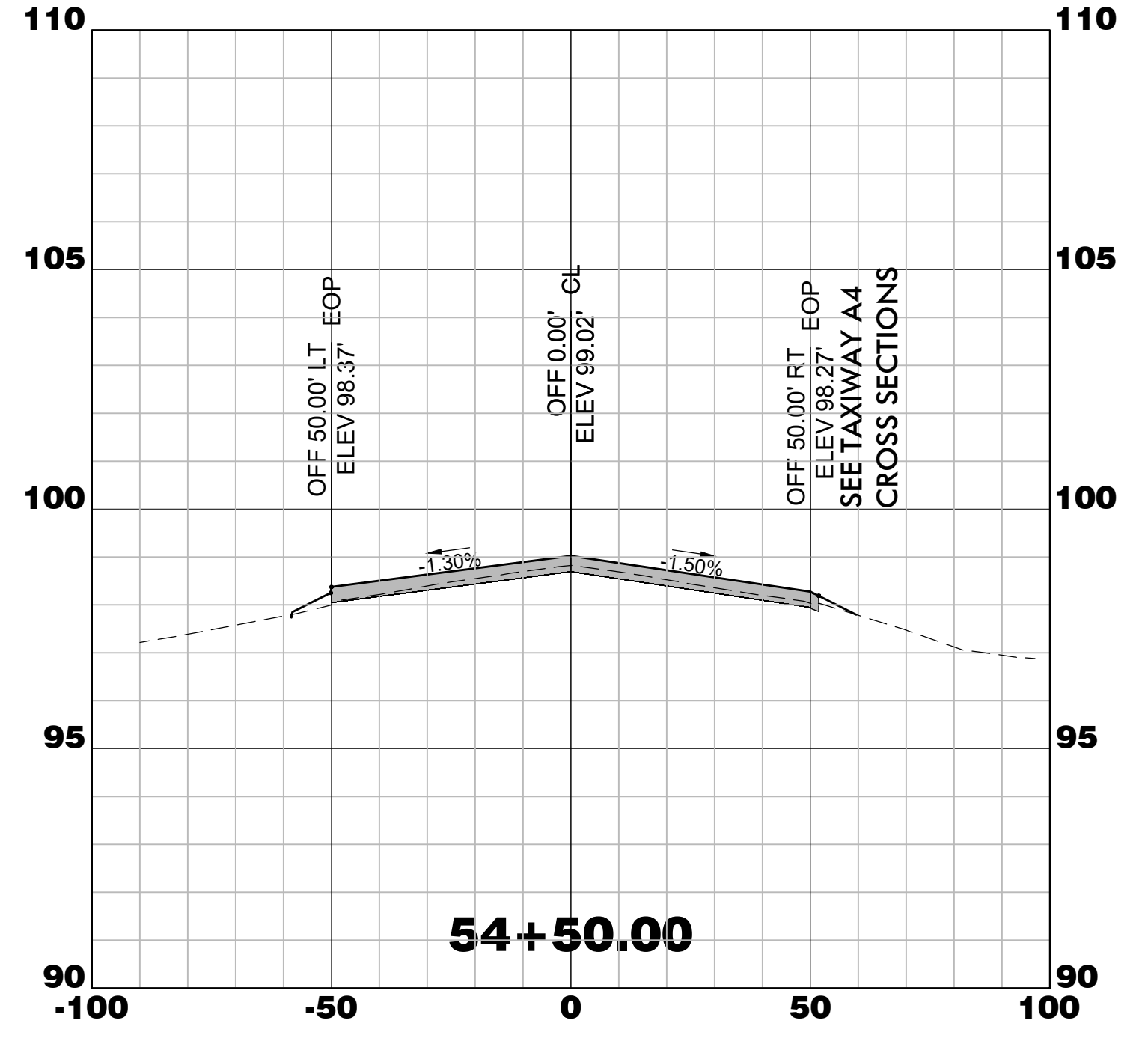
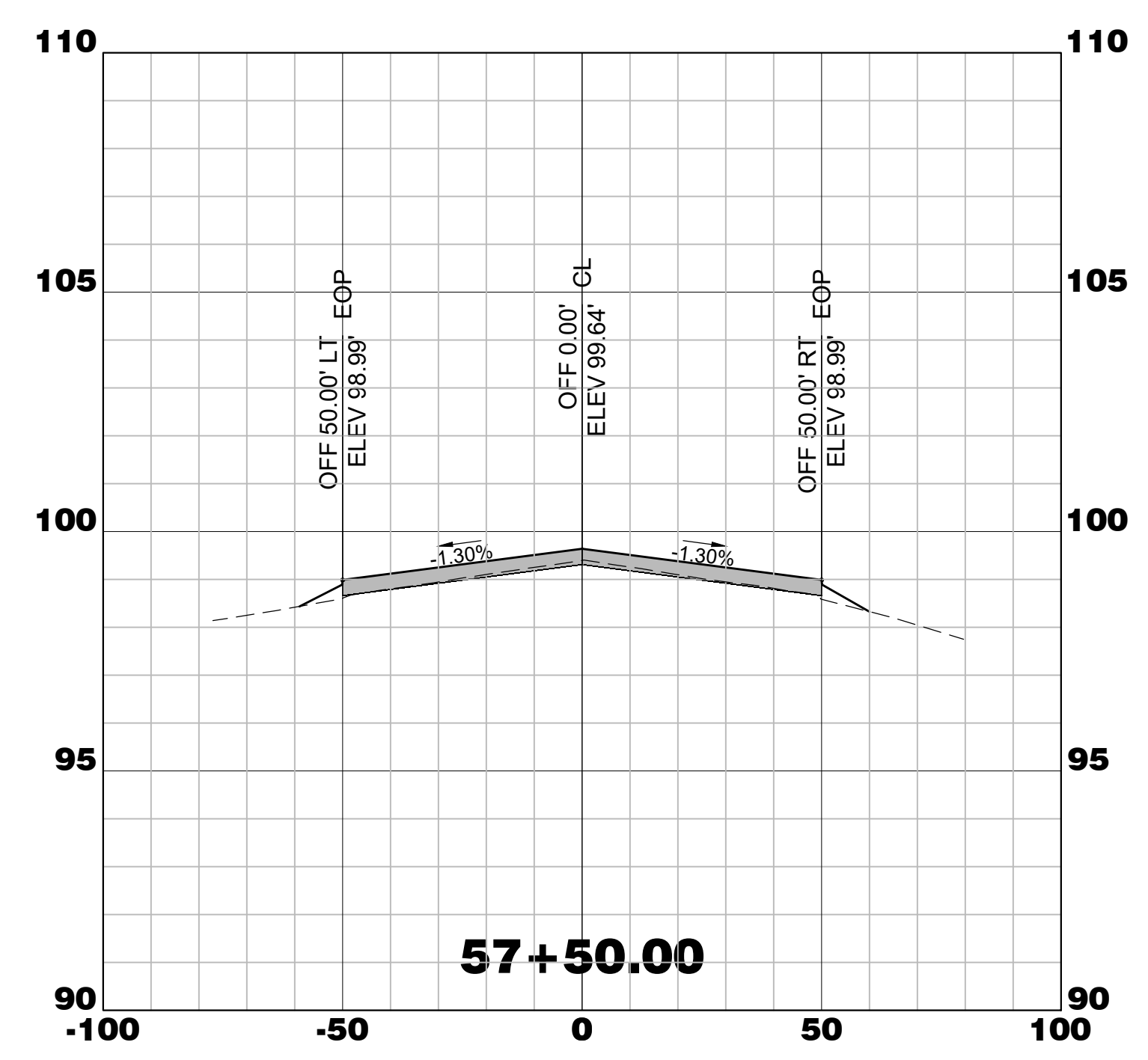
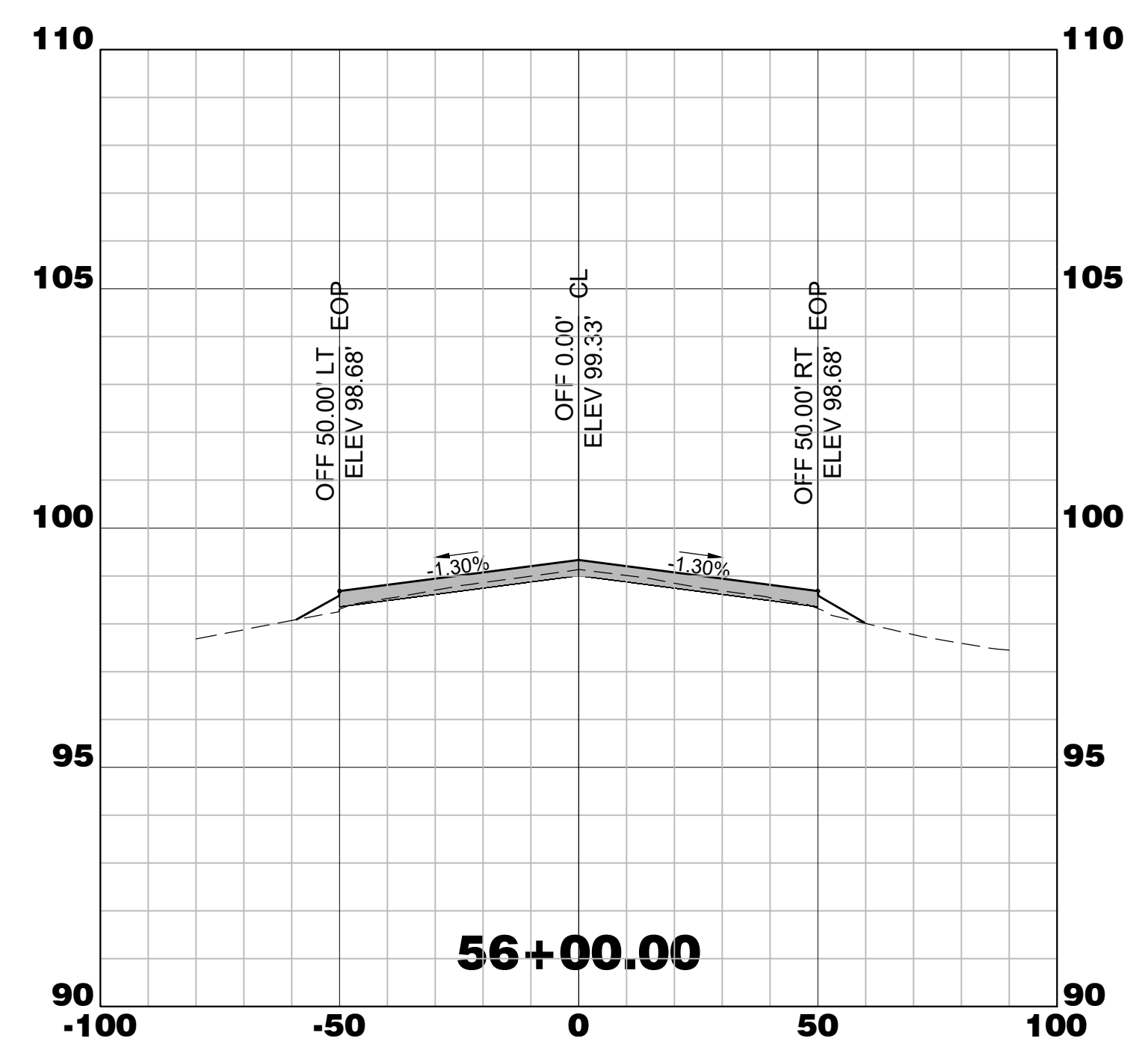
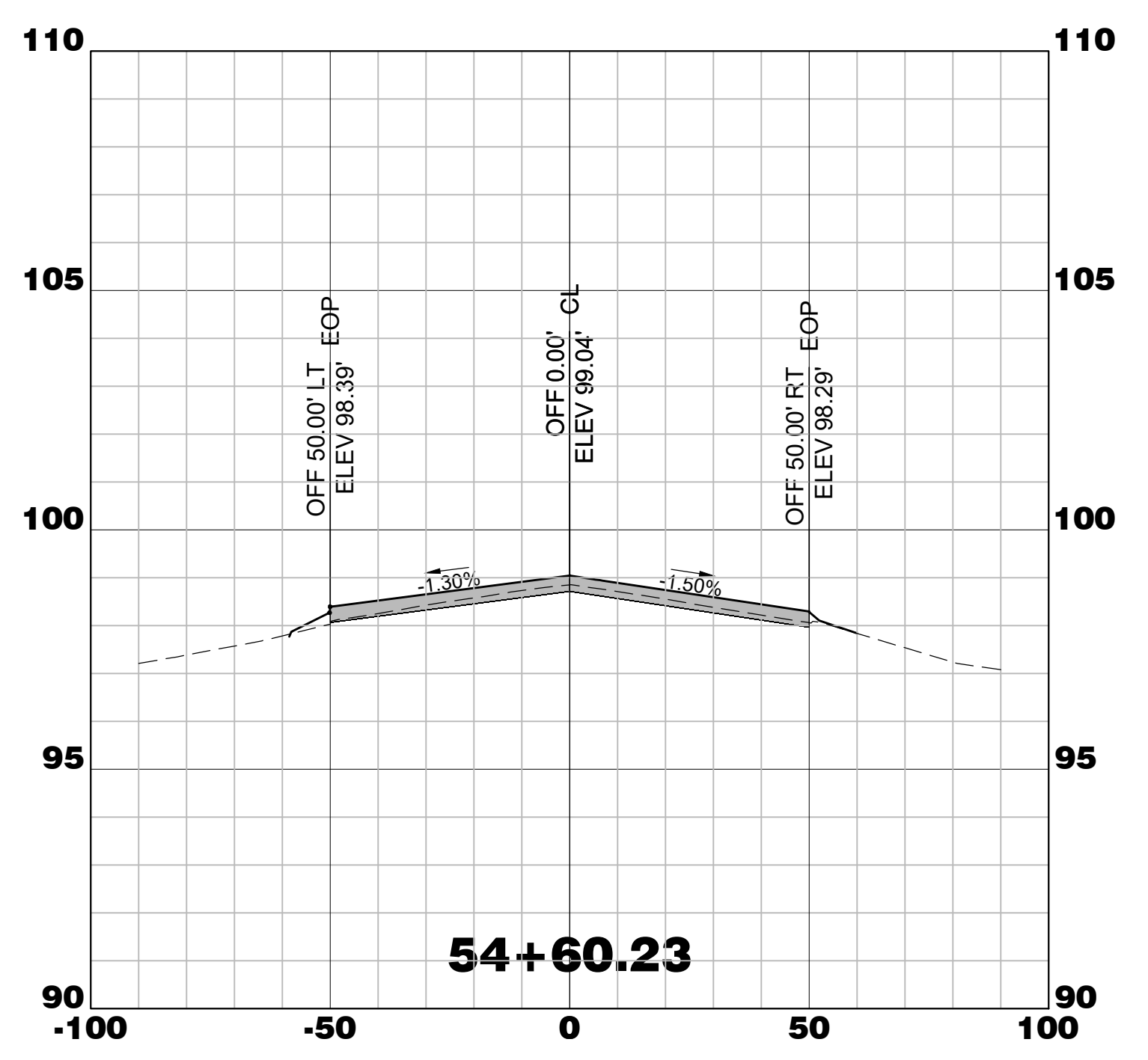
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**RUNWAY 5-23 CROSS SECTIONS
STA. 54+00 TO 57+50**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"=50 HORIZ.** Sheet Number:

1"=5' VERT. Drawing Number:
X-13



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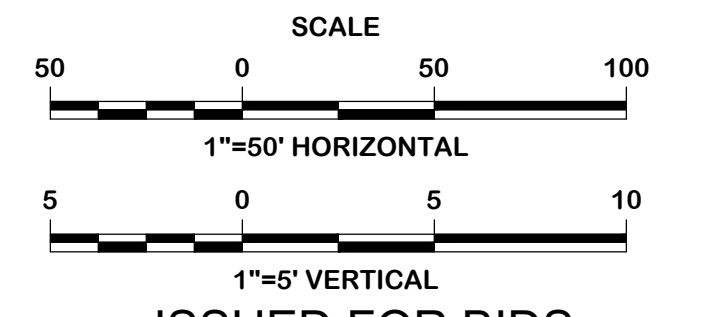
Michael Baker

INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



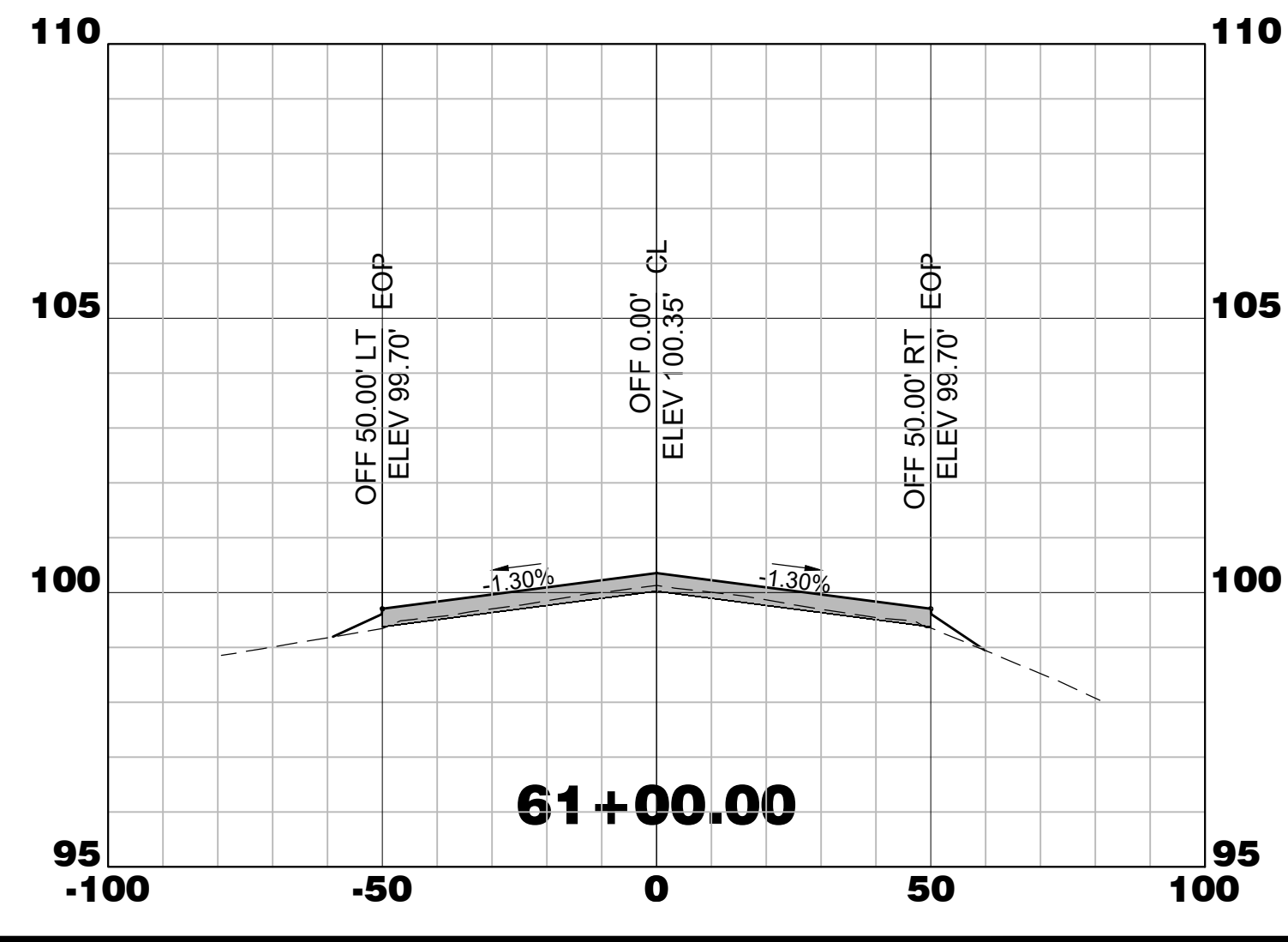
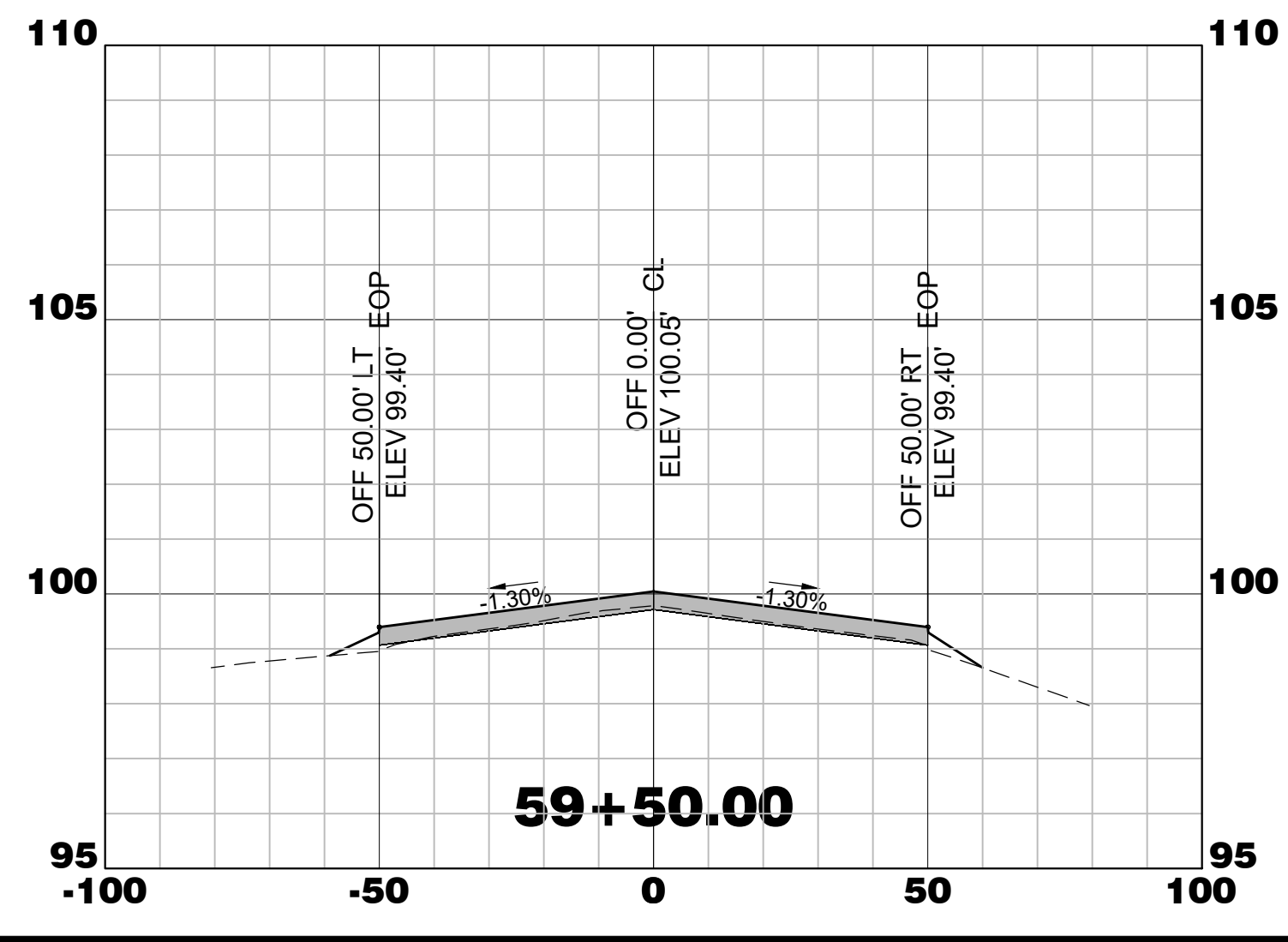
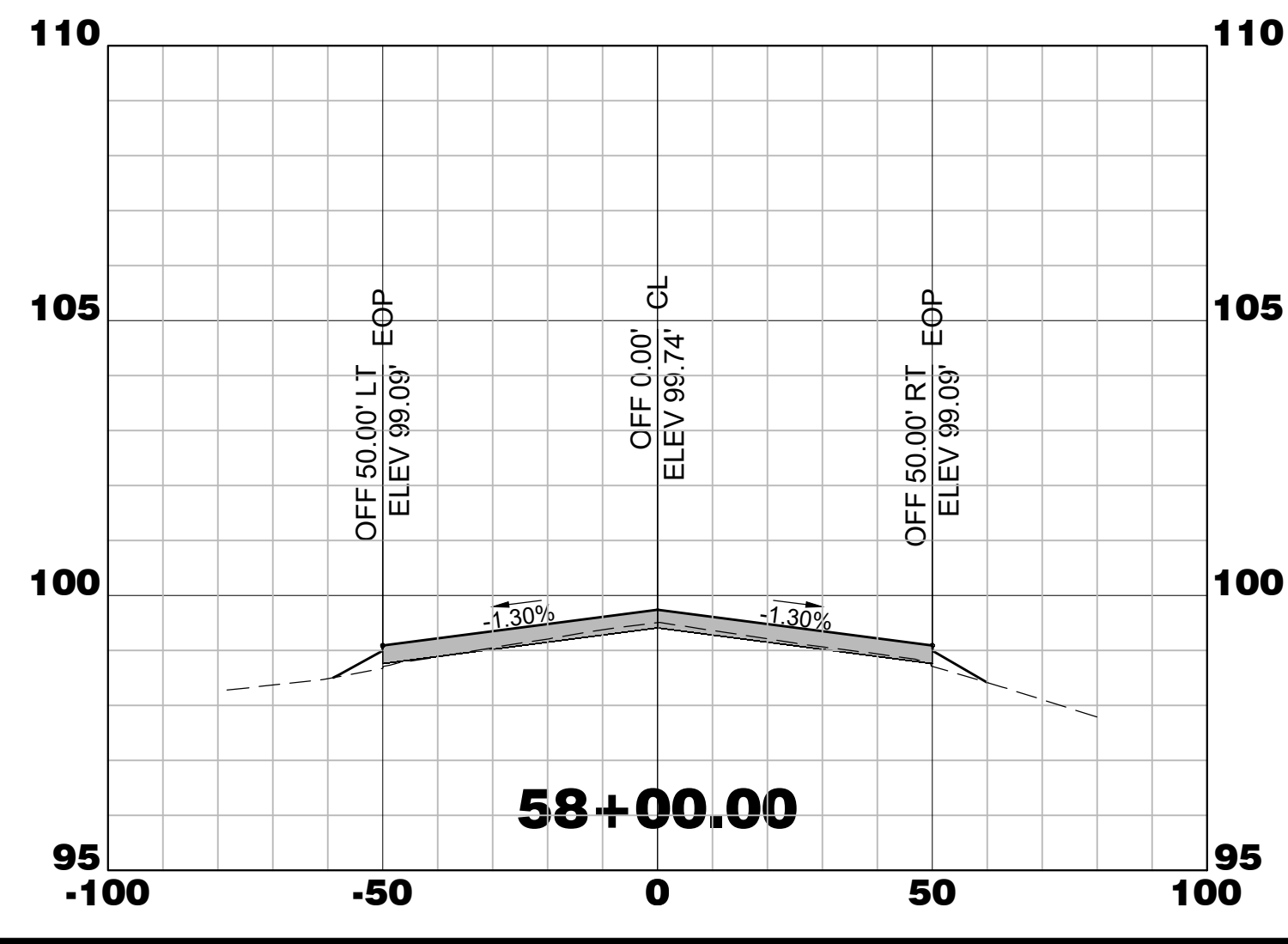
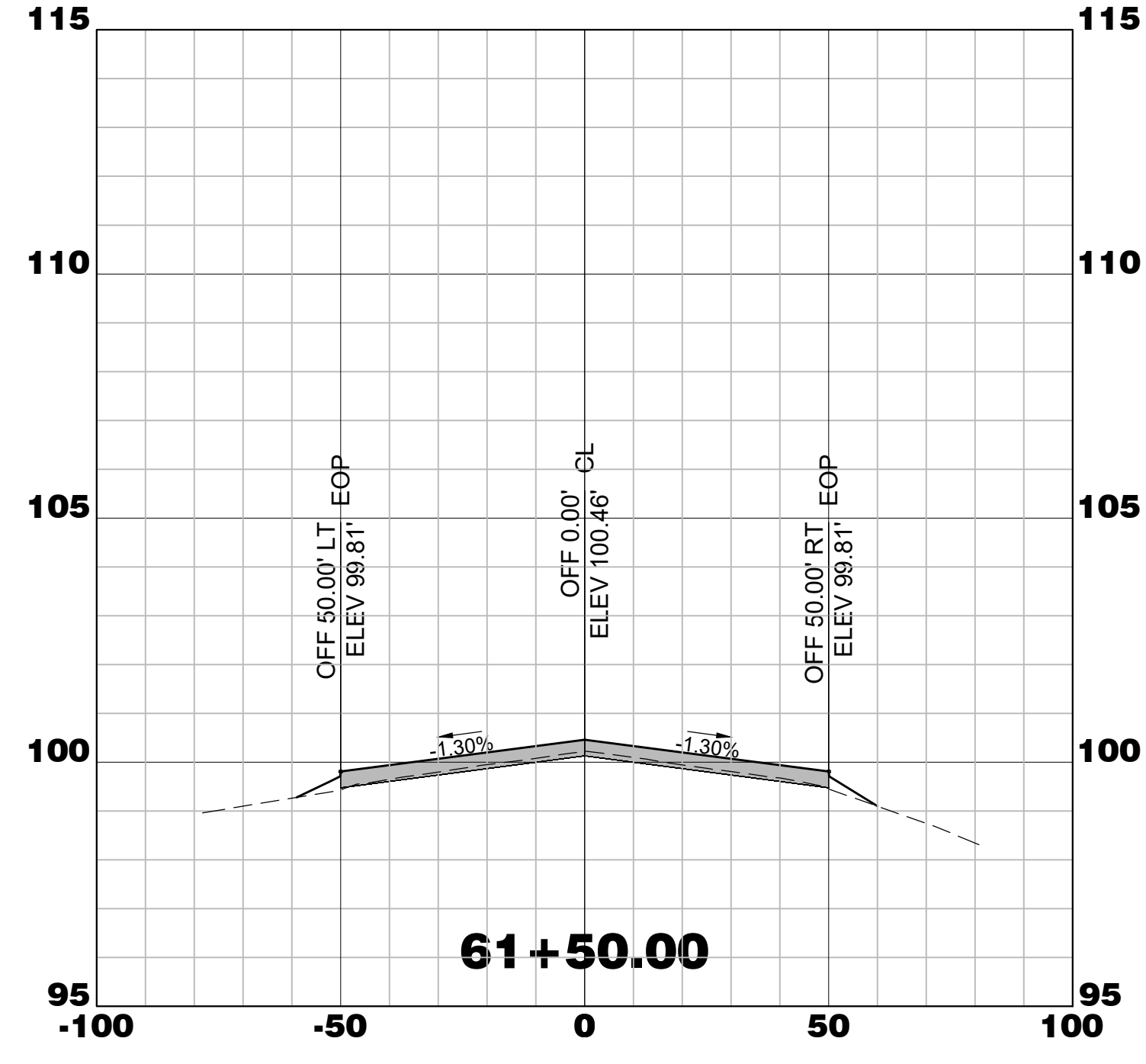
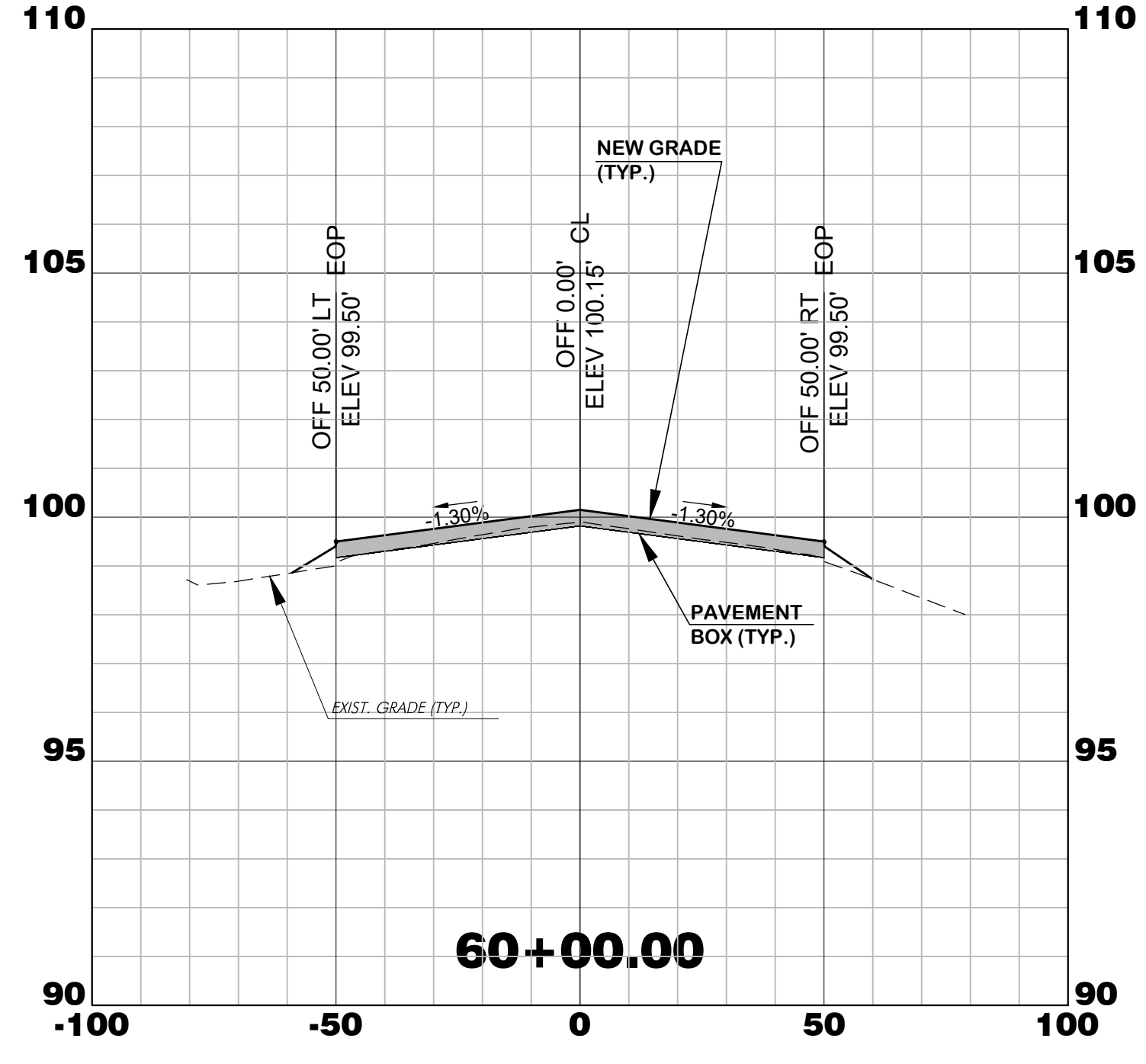
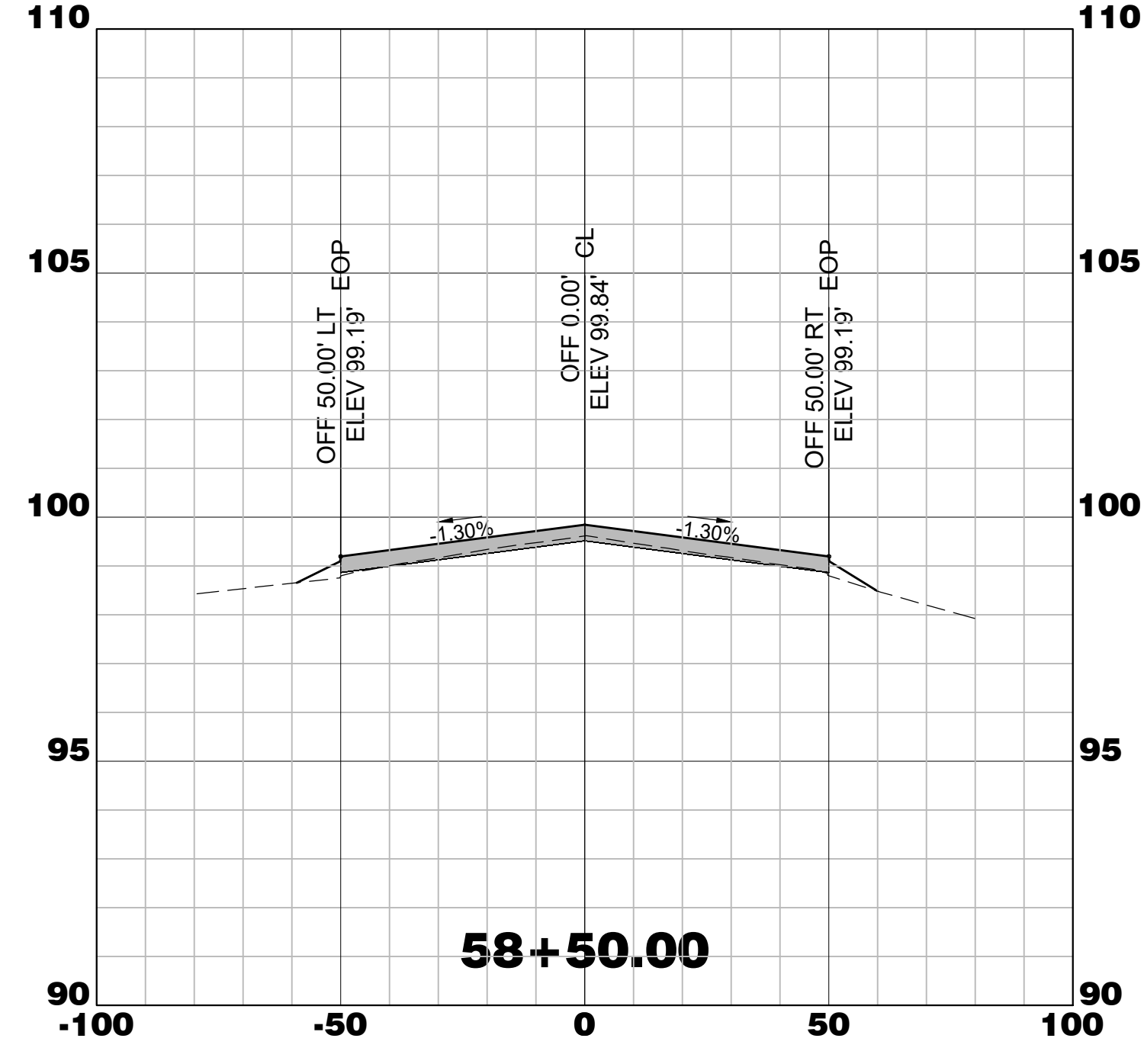
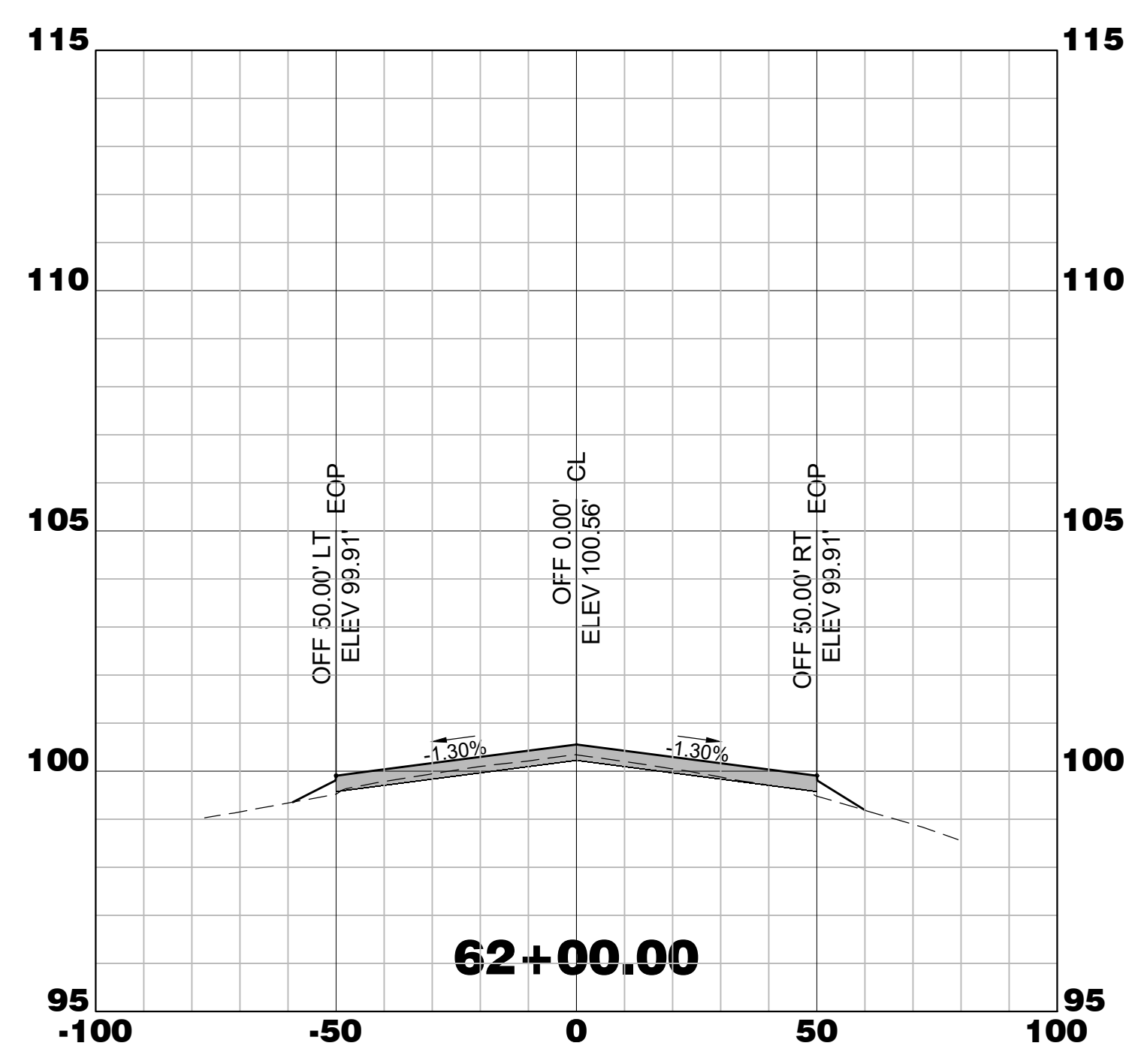
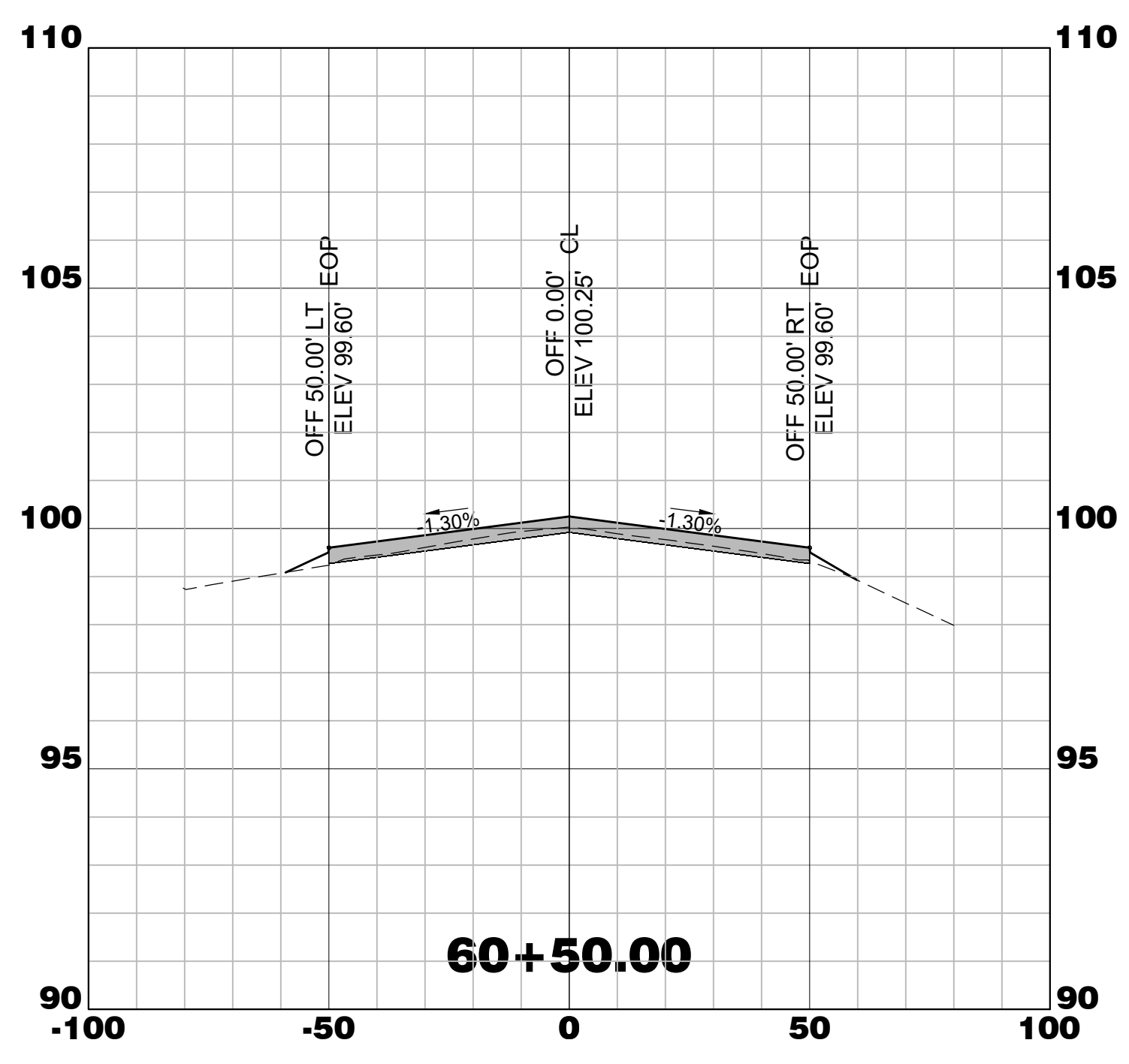
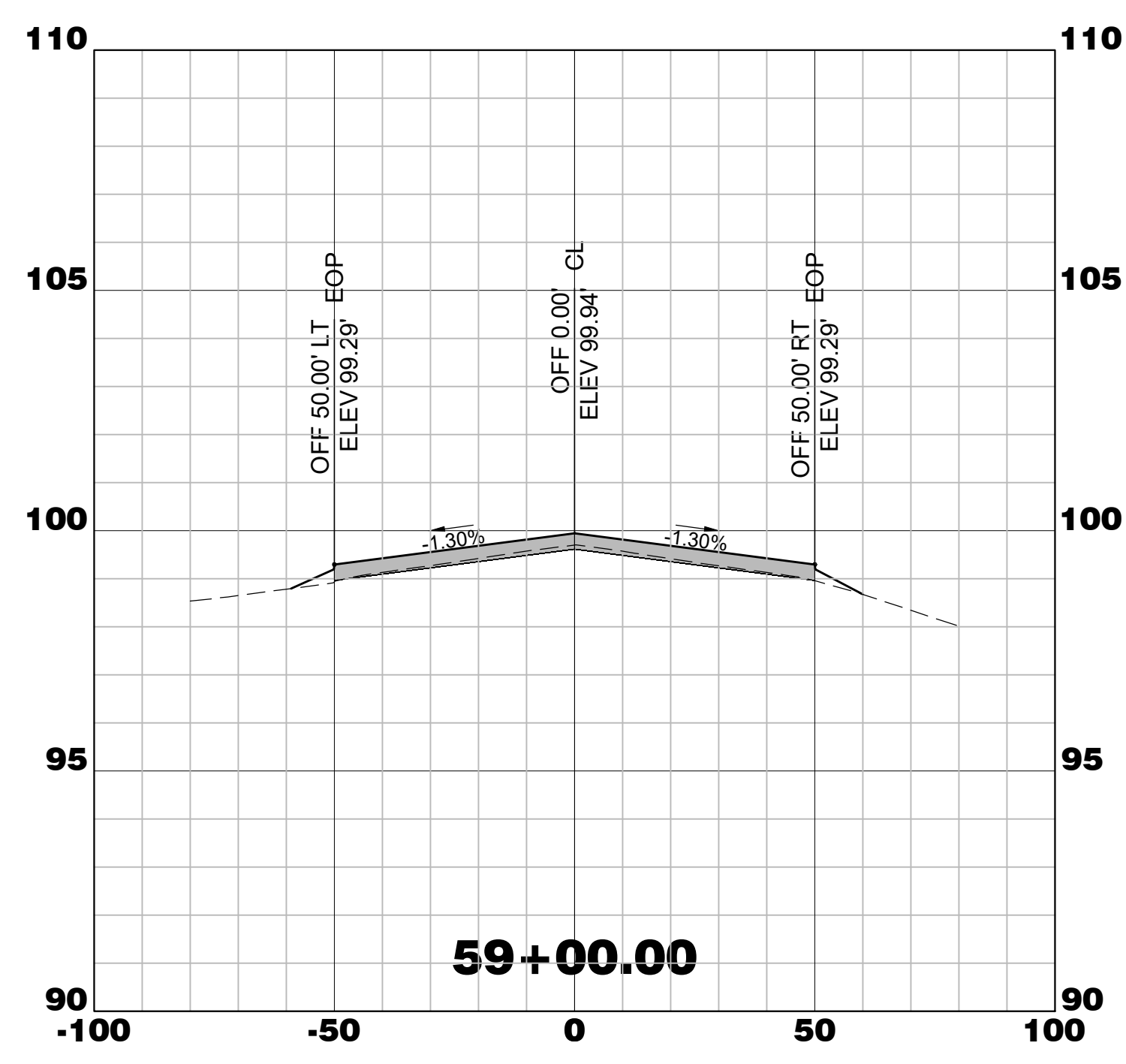
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

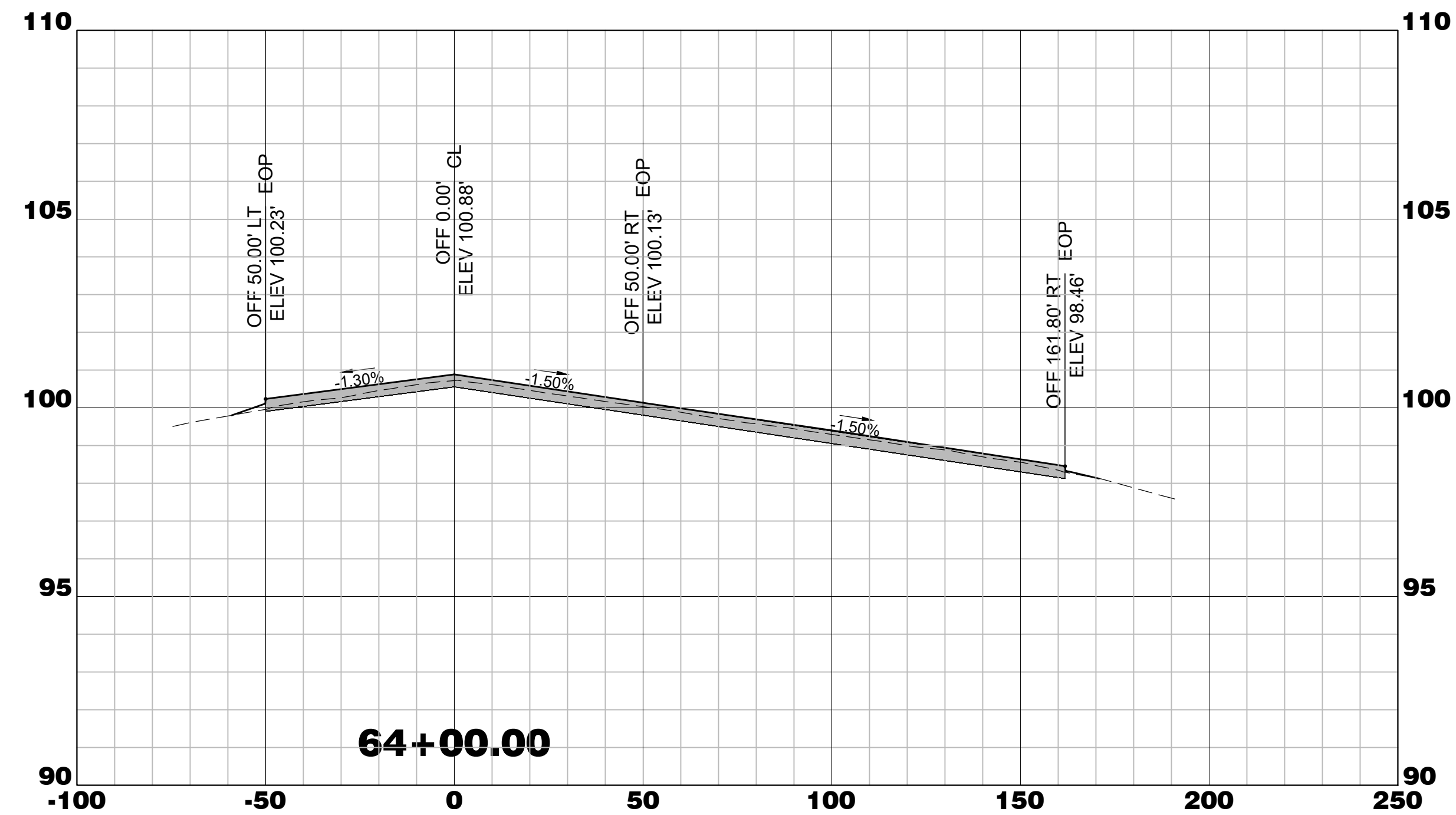
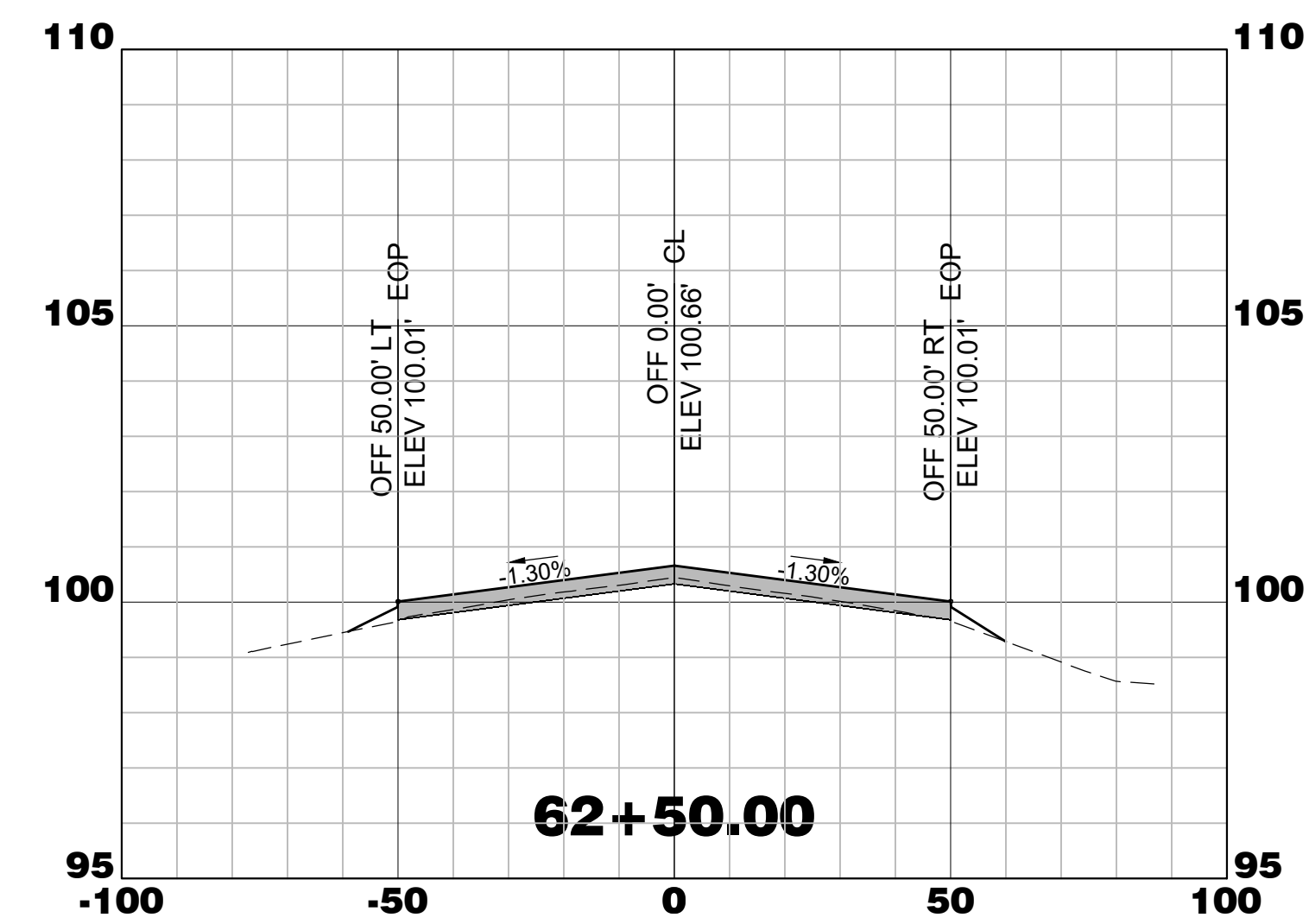
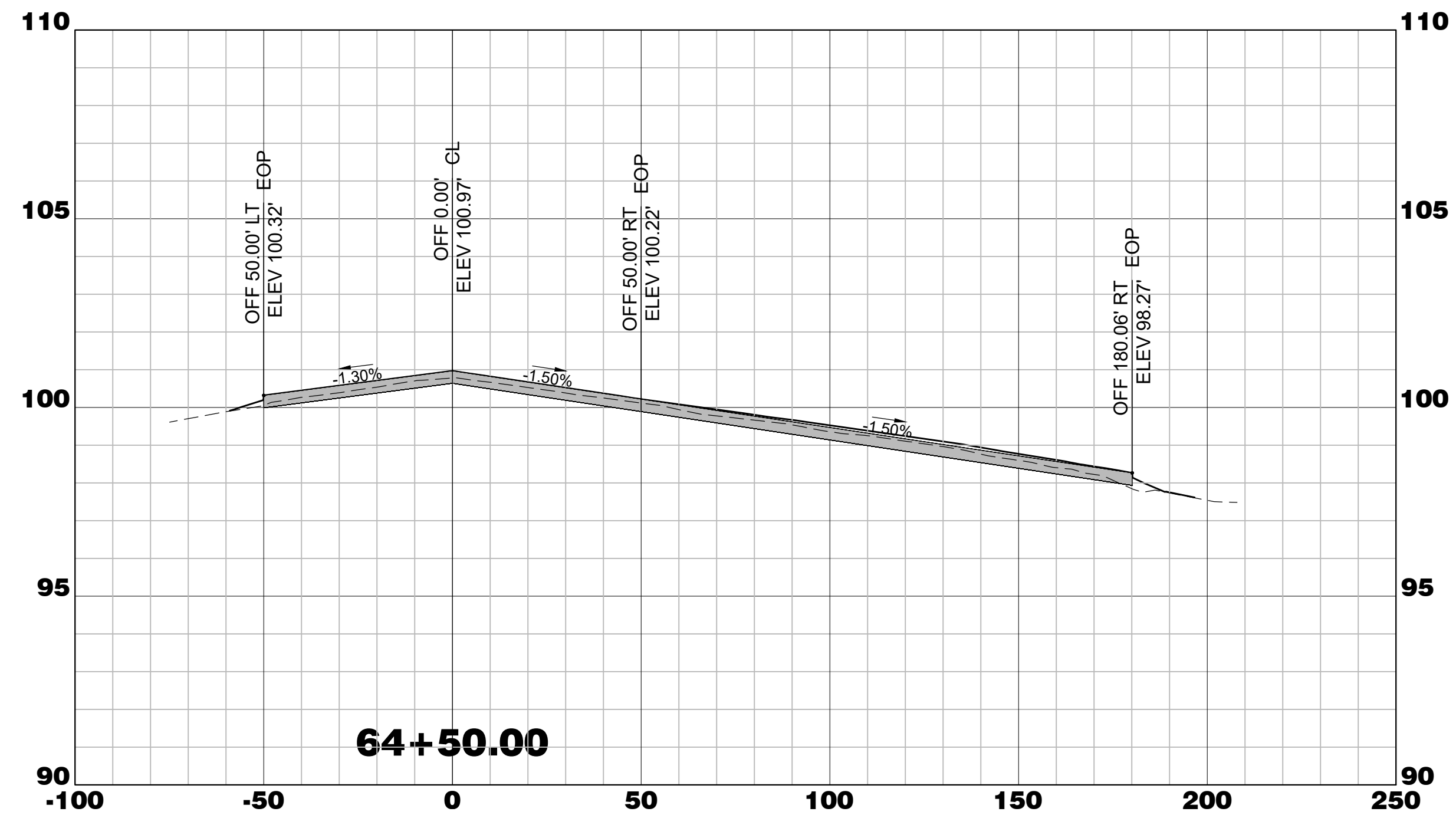
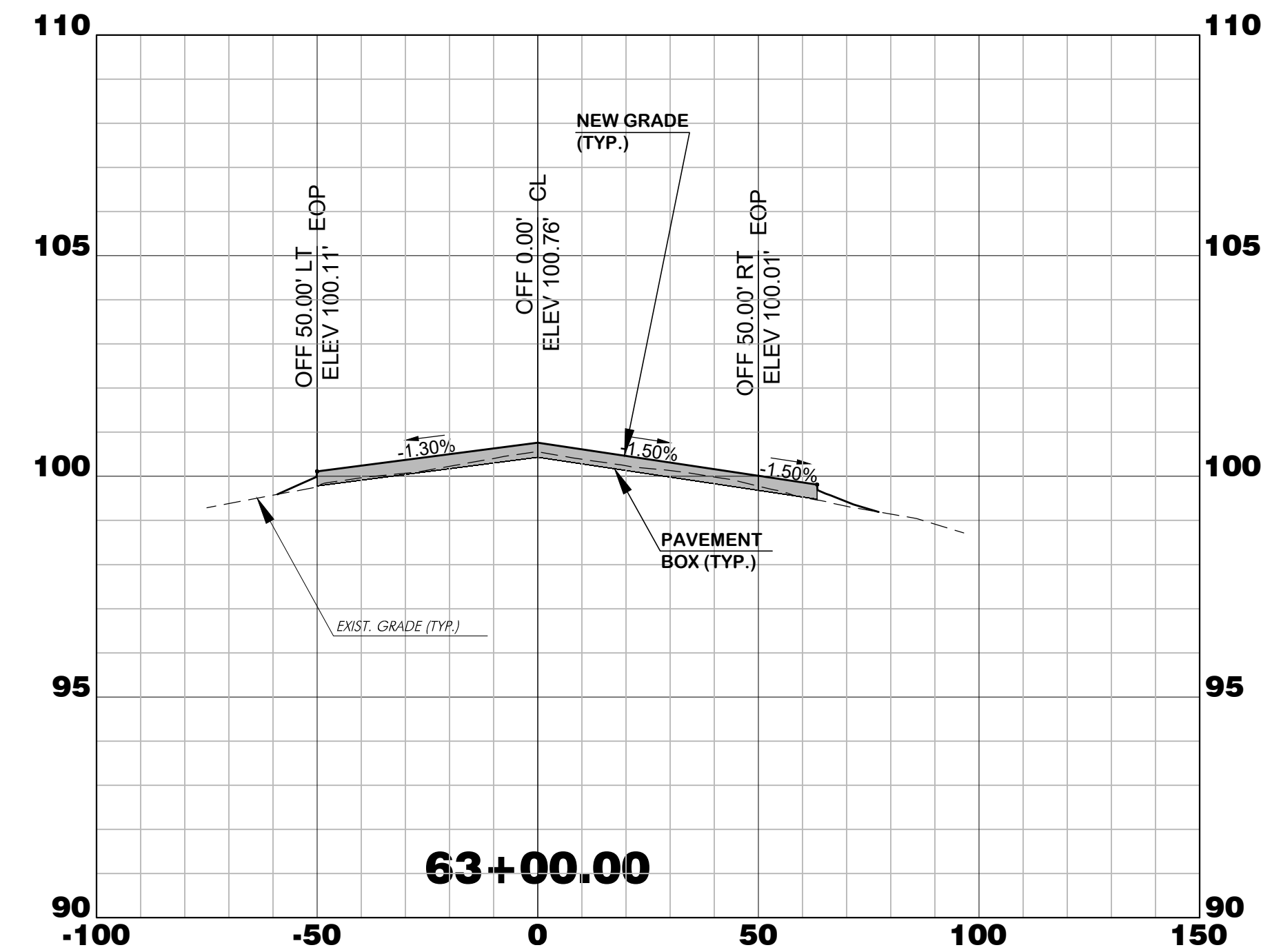
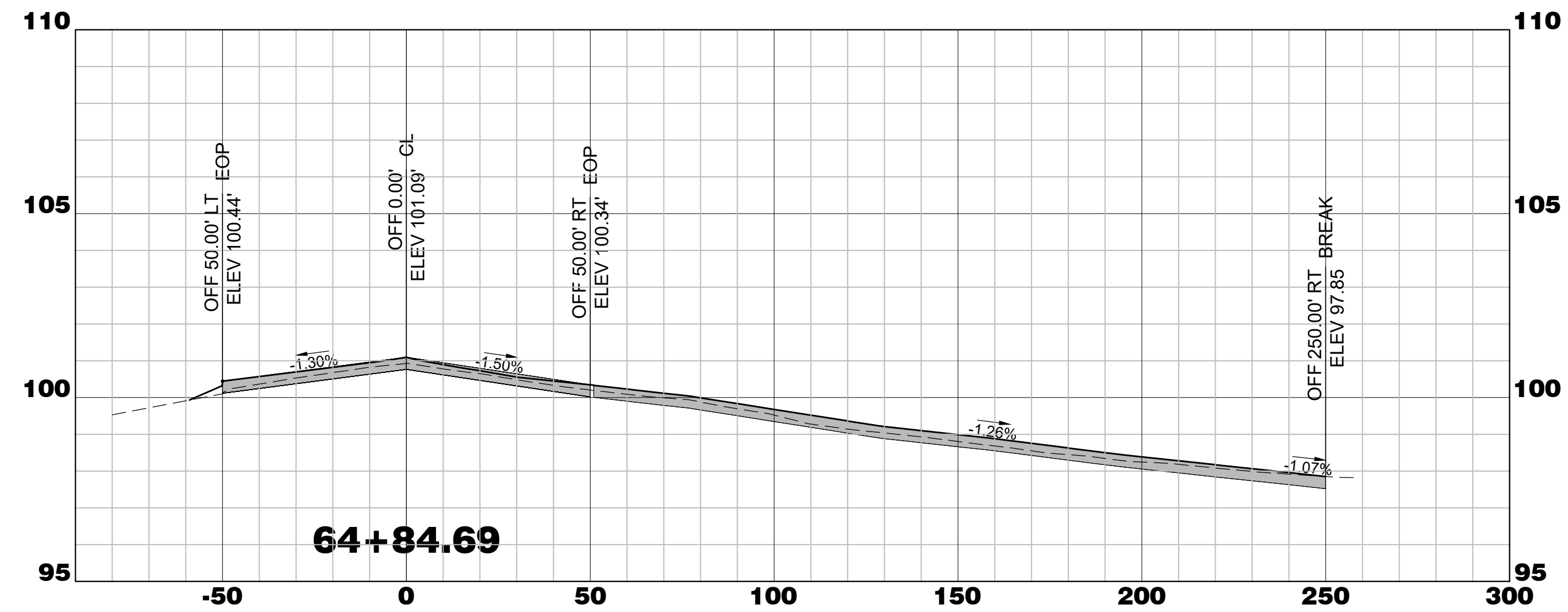
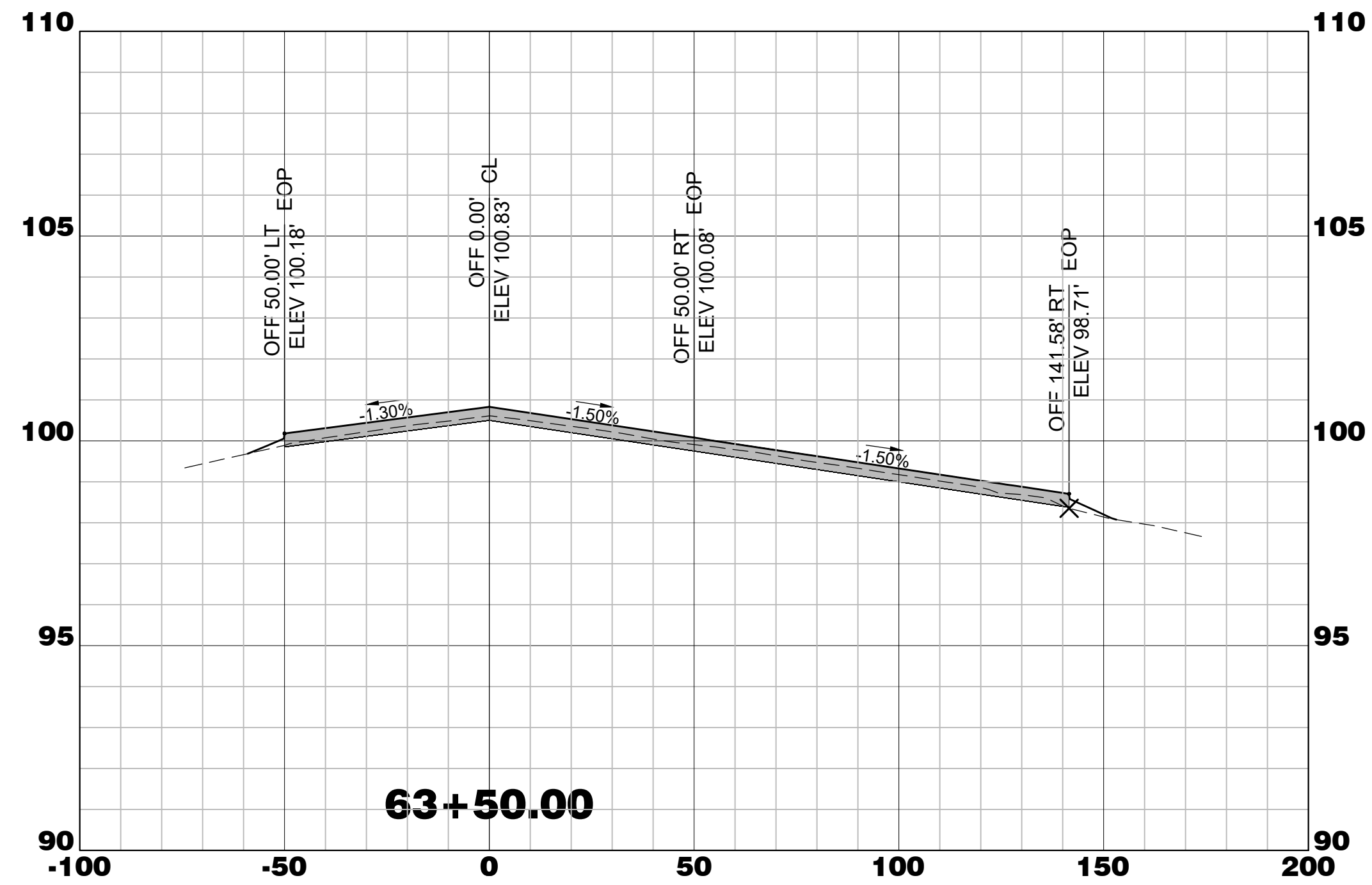
Drawing Name:
RUNWAY 5-23 CROSS SECTIONS STA. 59+00 TO 62+00

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ.	Sheet Number:
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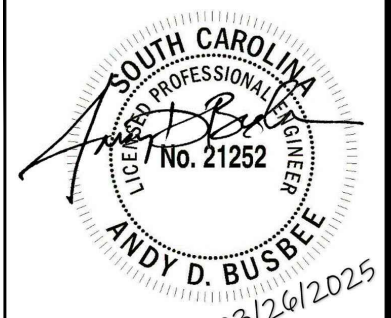


WALTERBORO, SOUTH CAROLINA

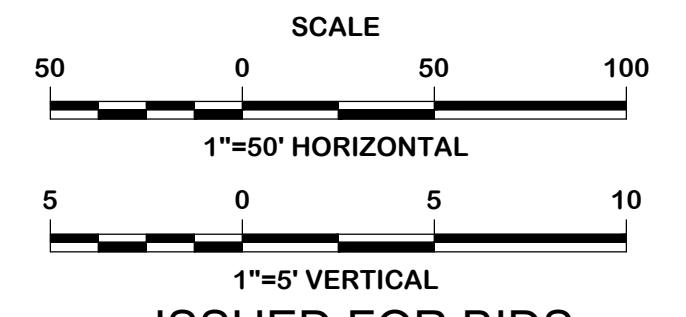
Michael Baker

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Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
**RUNWAY 5-23 CROSS SECTIONS
STA. 62+50 TO 64+84.69**

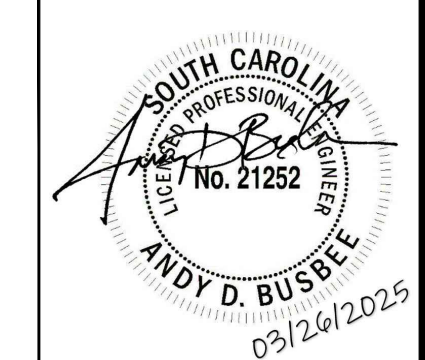
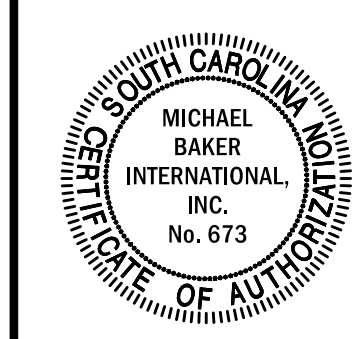
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Date: MARCH, 2025	Division: AIRPORTS
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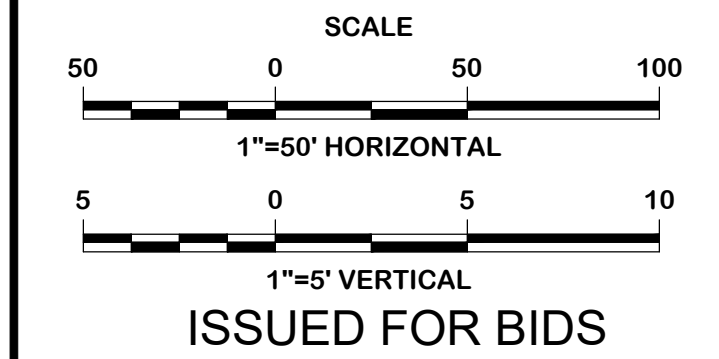
WALTERBORO, SOUTH CAROLINA

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Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



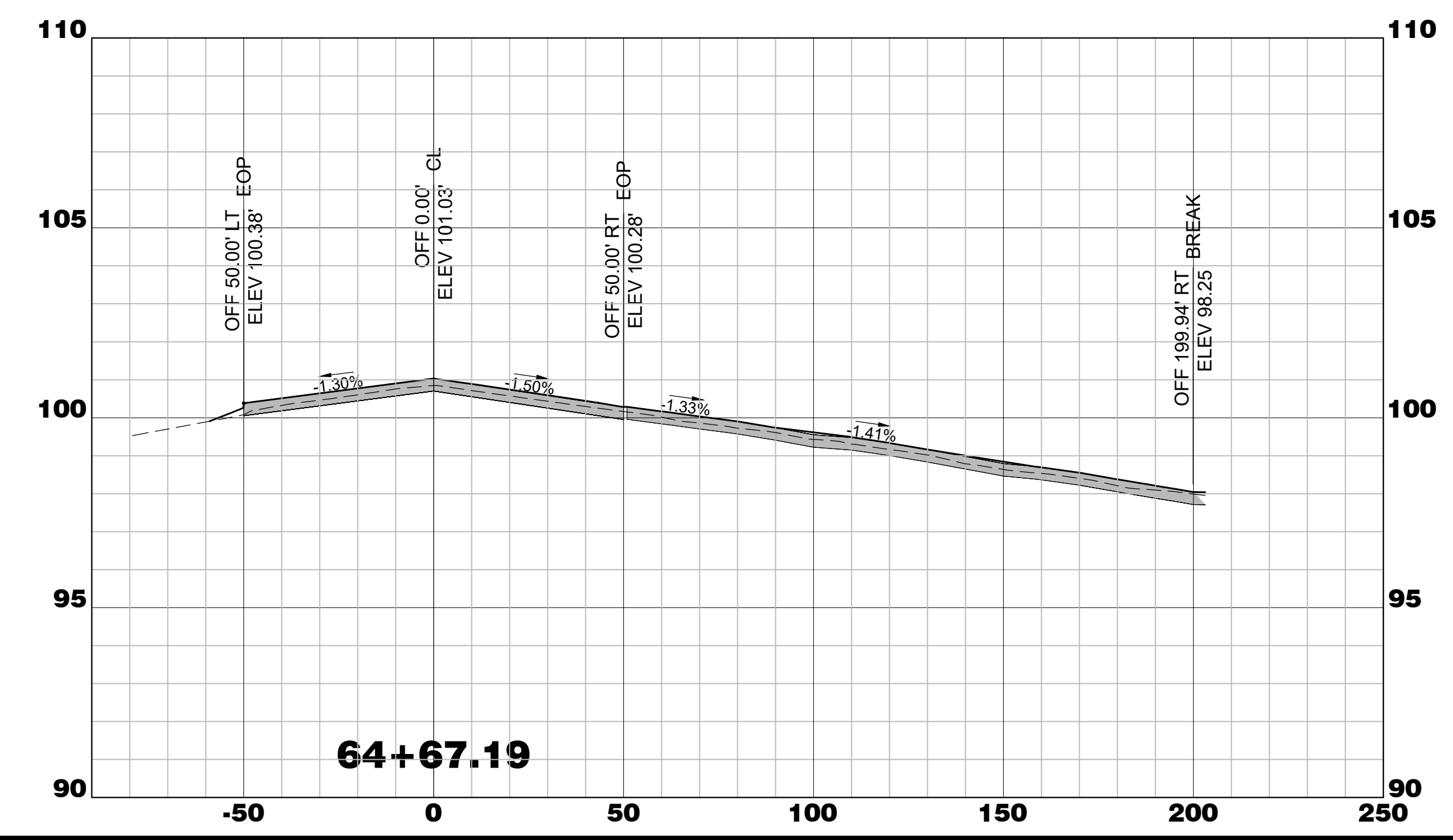
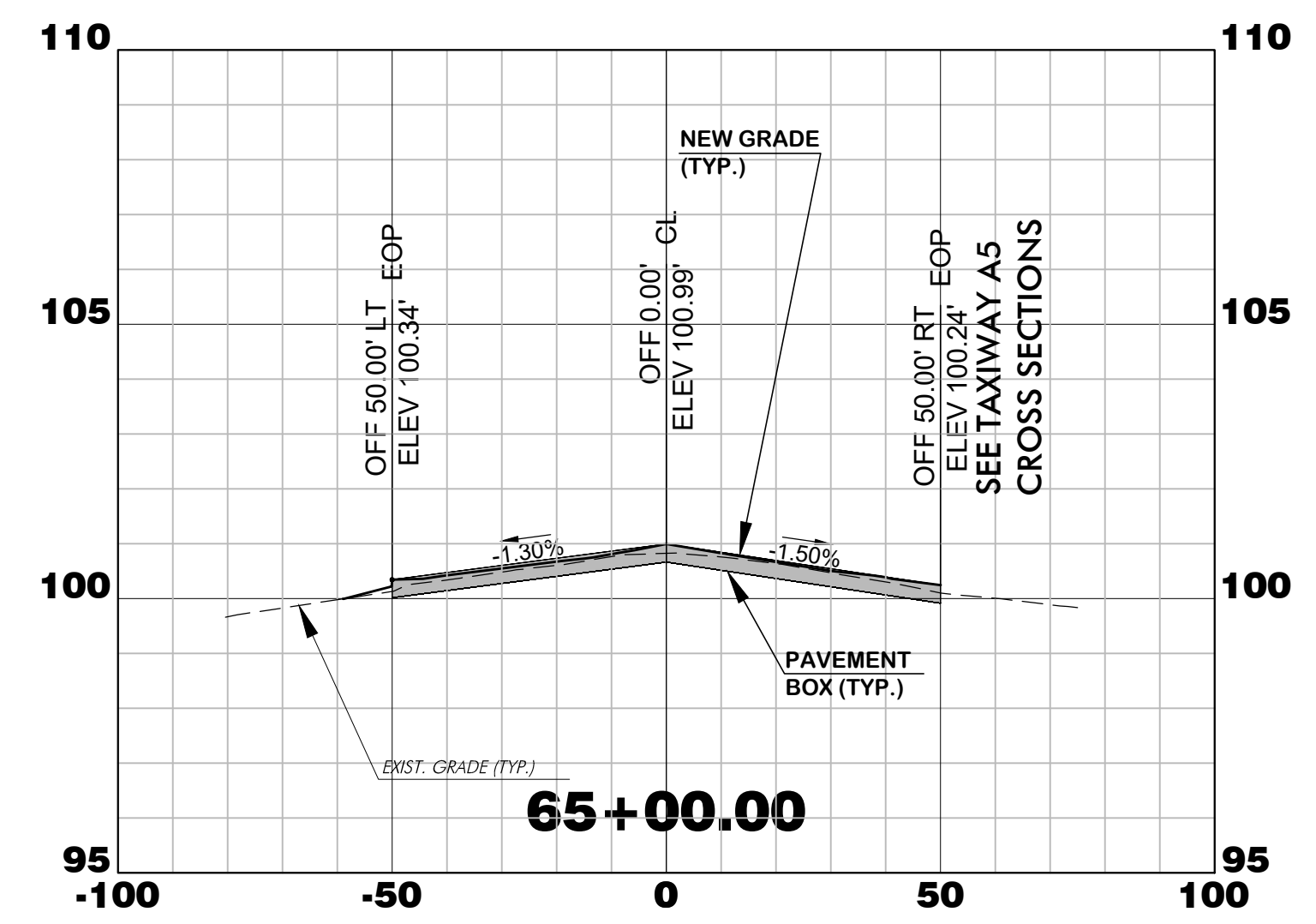
ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
**RUNWAY 5-23 CROSS SECTIONS
STA. 64+67.19 TO 65+02**

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ. 1"=5' VERT.	Sheet Number: X-16



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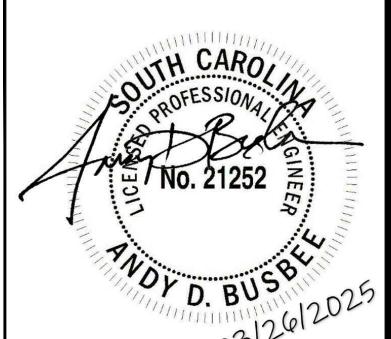


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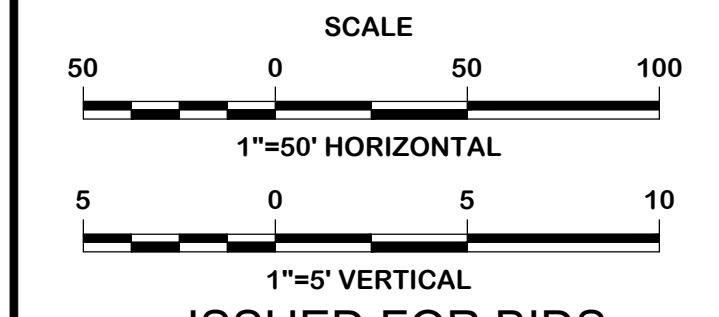
Michael Baker

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700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



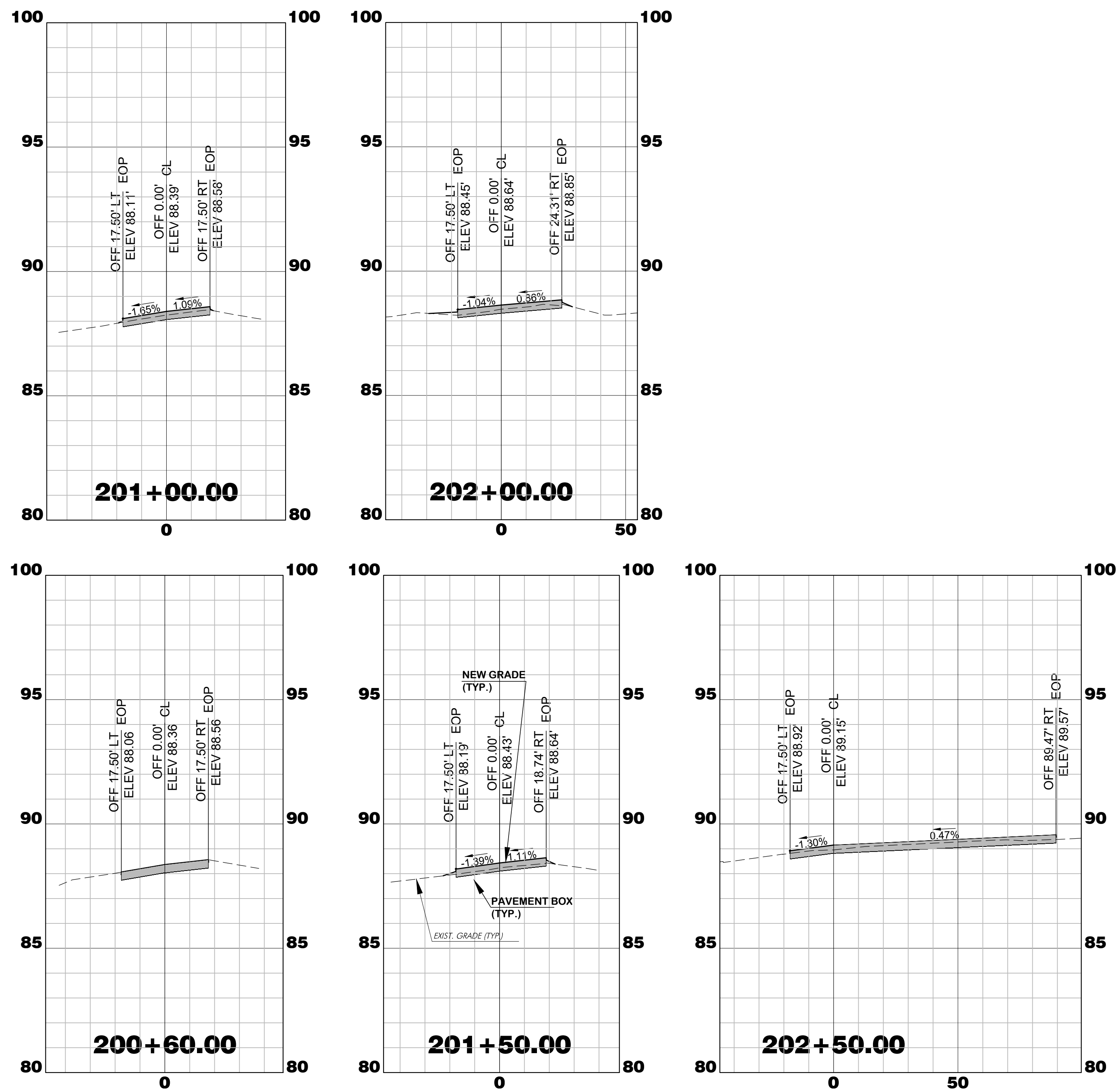
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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
TAXIWAY A1 CROSS SECTIONS

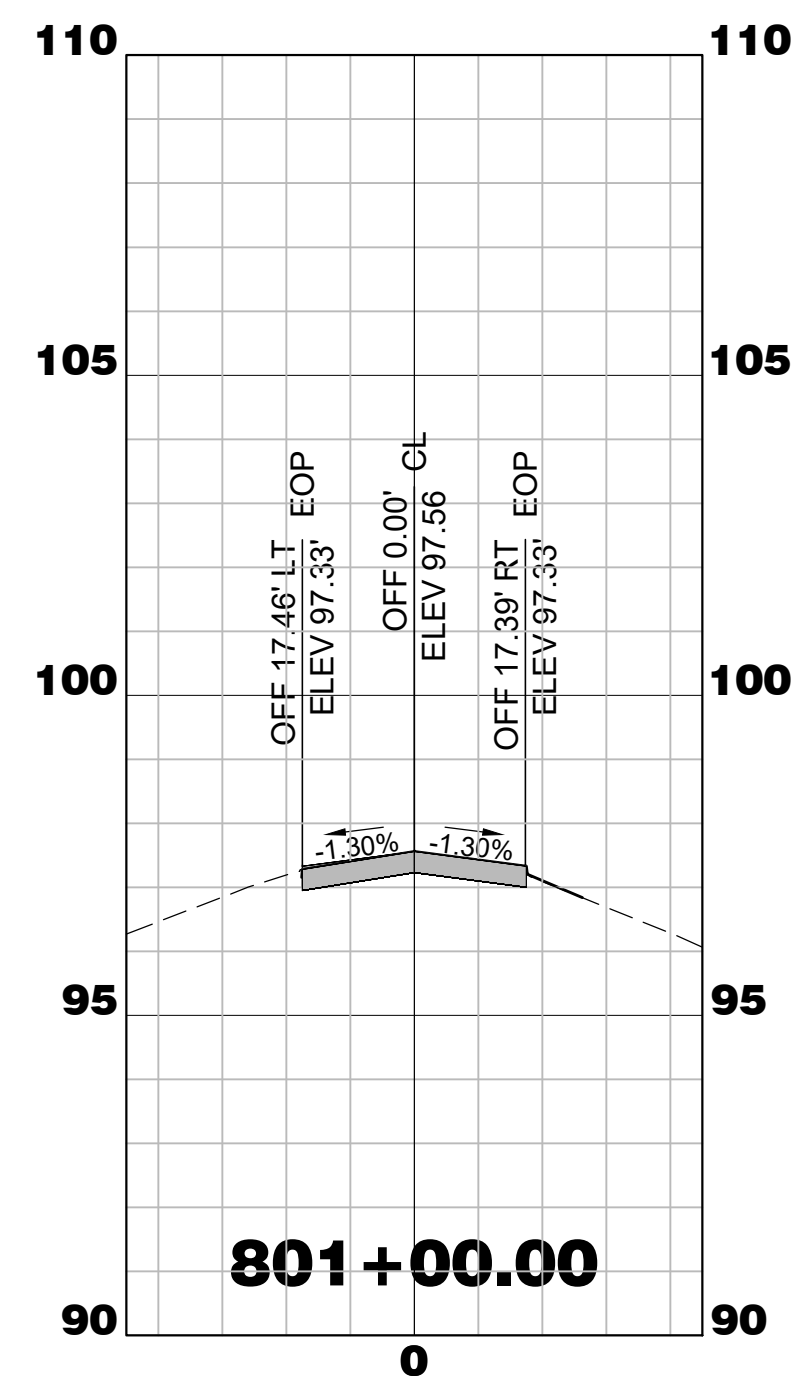
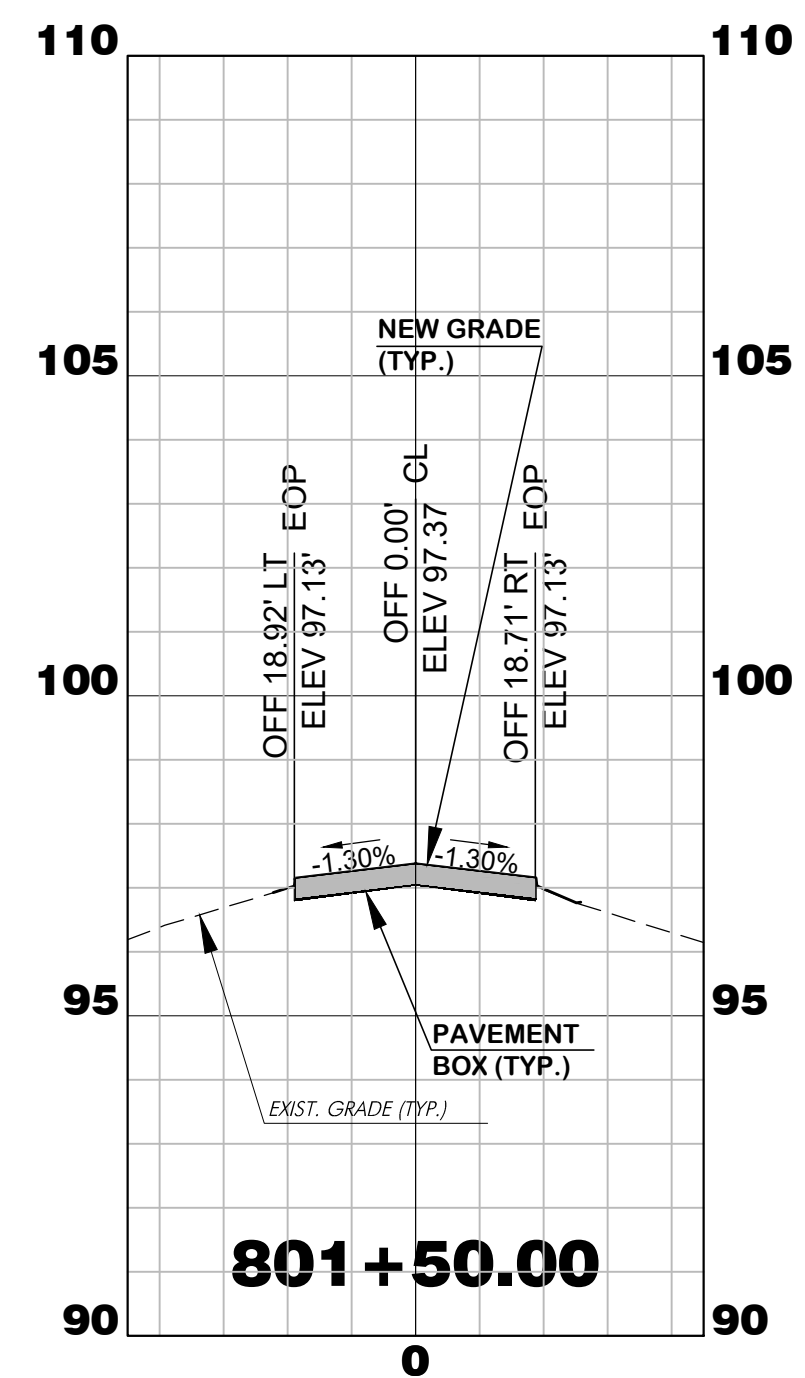
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Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ.	Sheet Number:
1"=5' VERT.	Drawing Number: X-17



TW A1 CROSS SECTIONS

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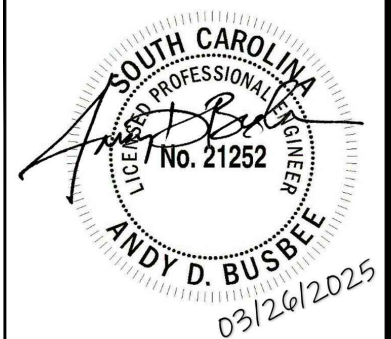
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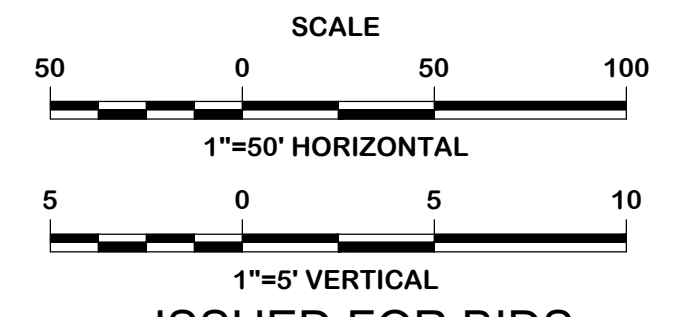
WALTERBORO, SOUTH CAROLINA

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INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

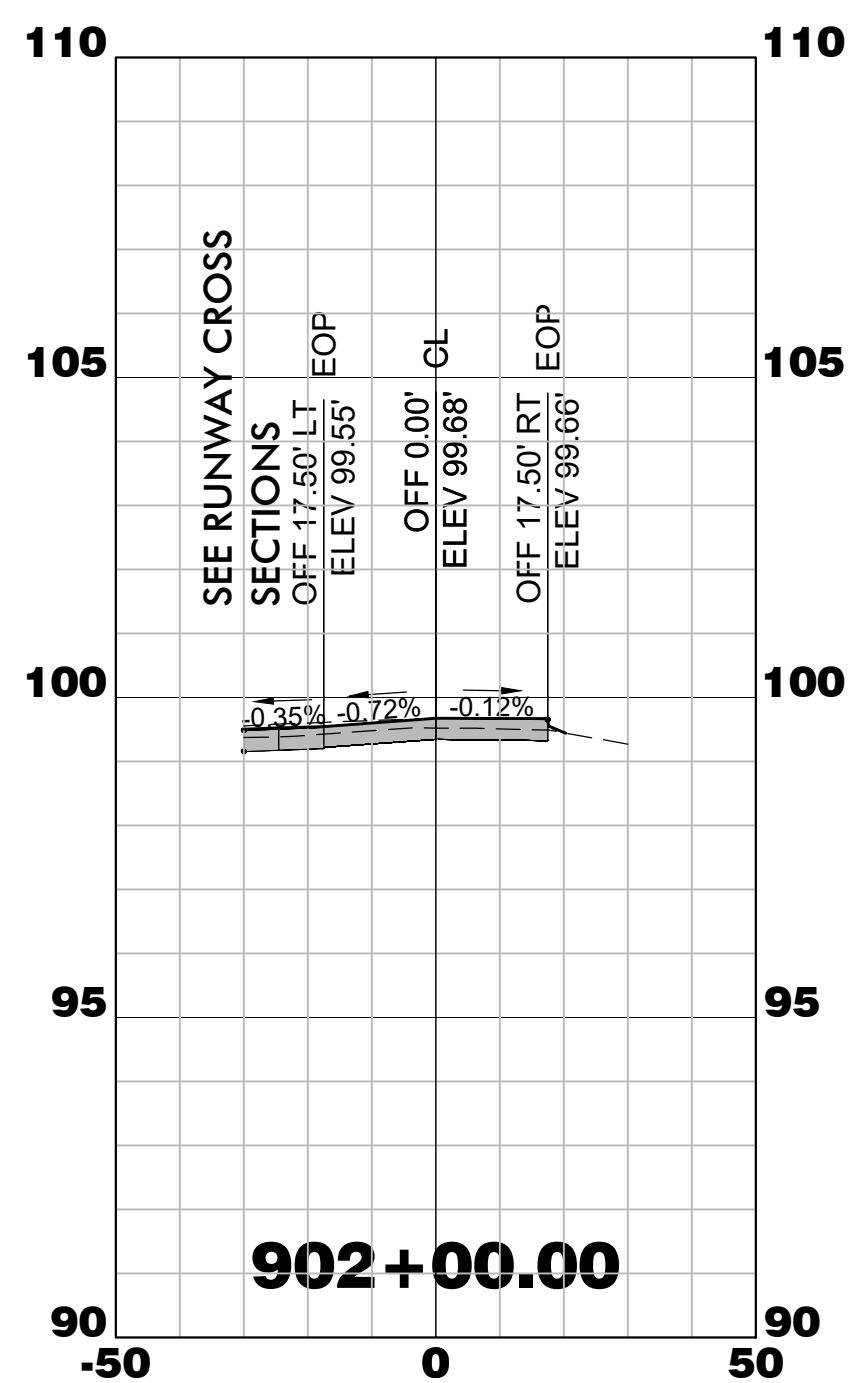
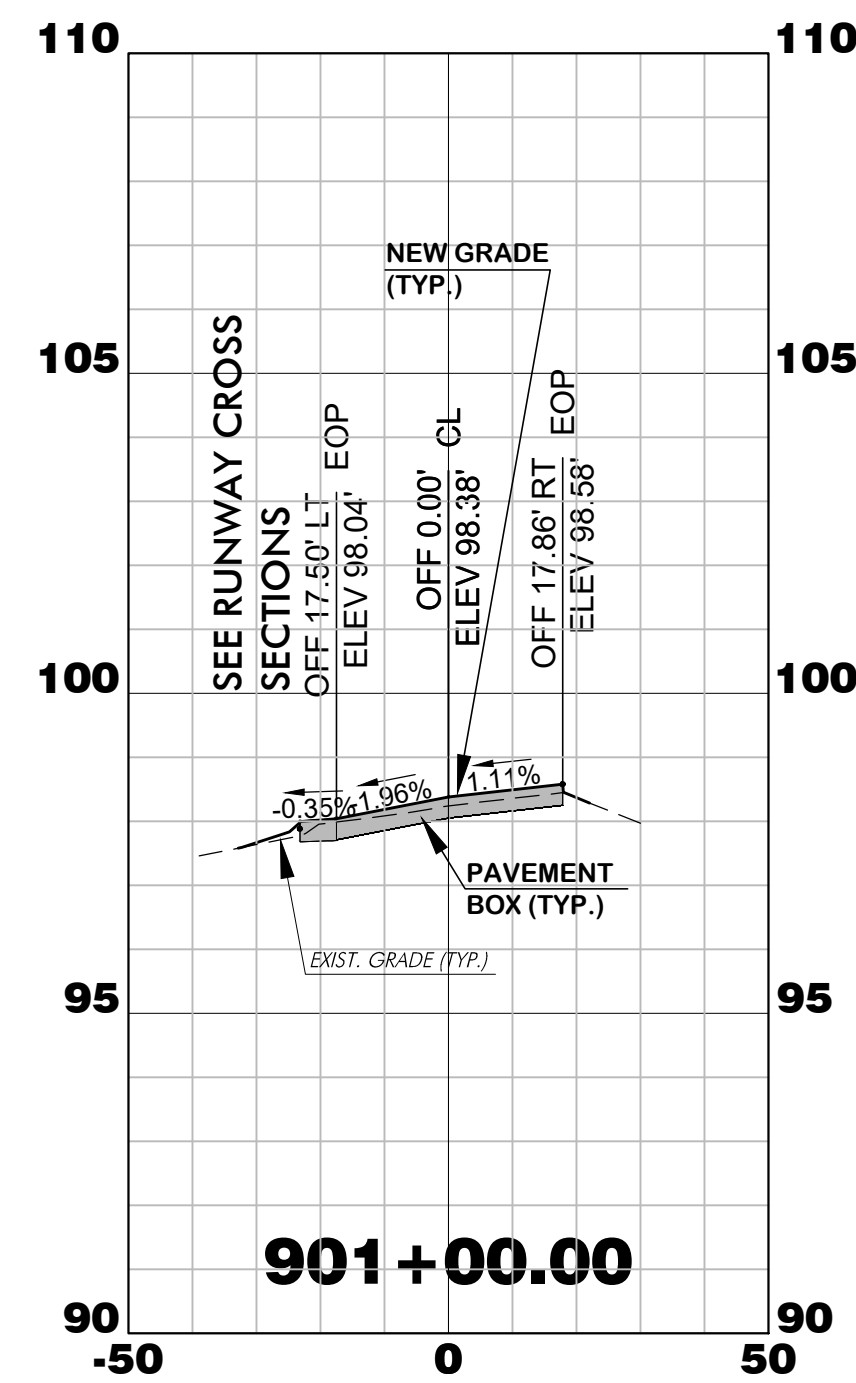
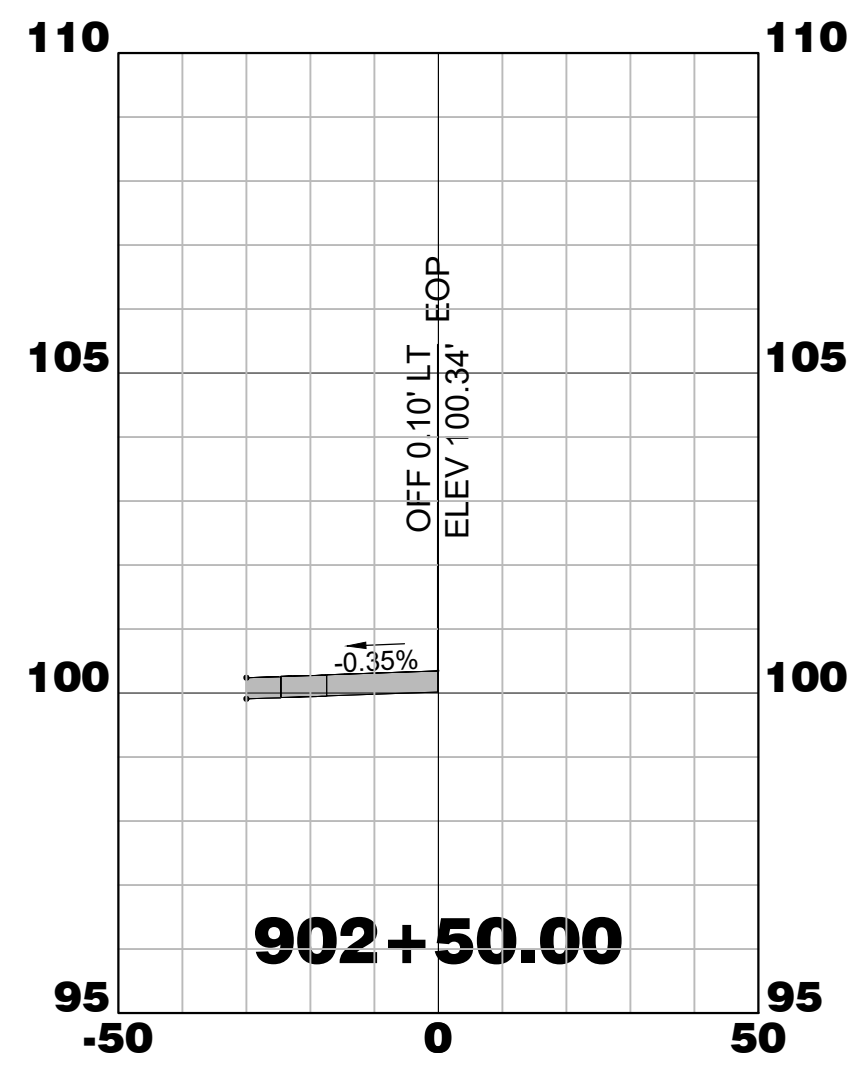
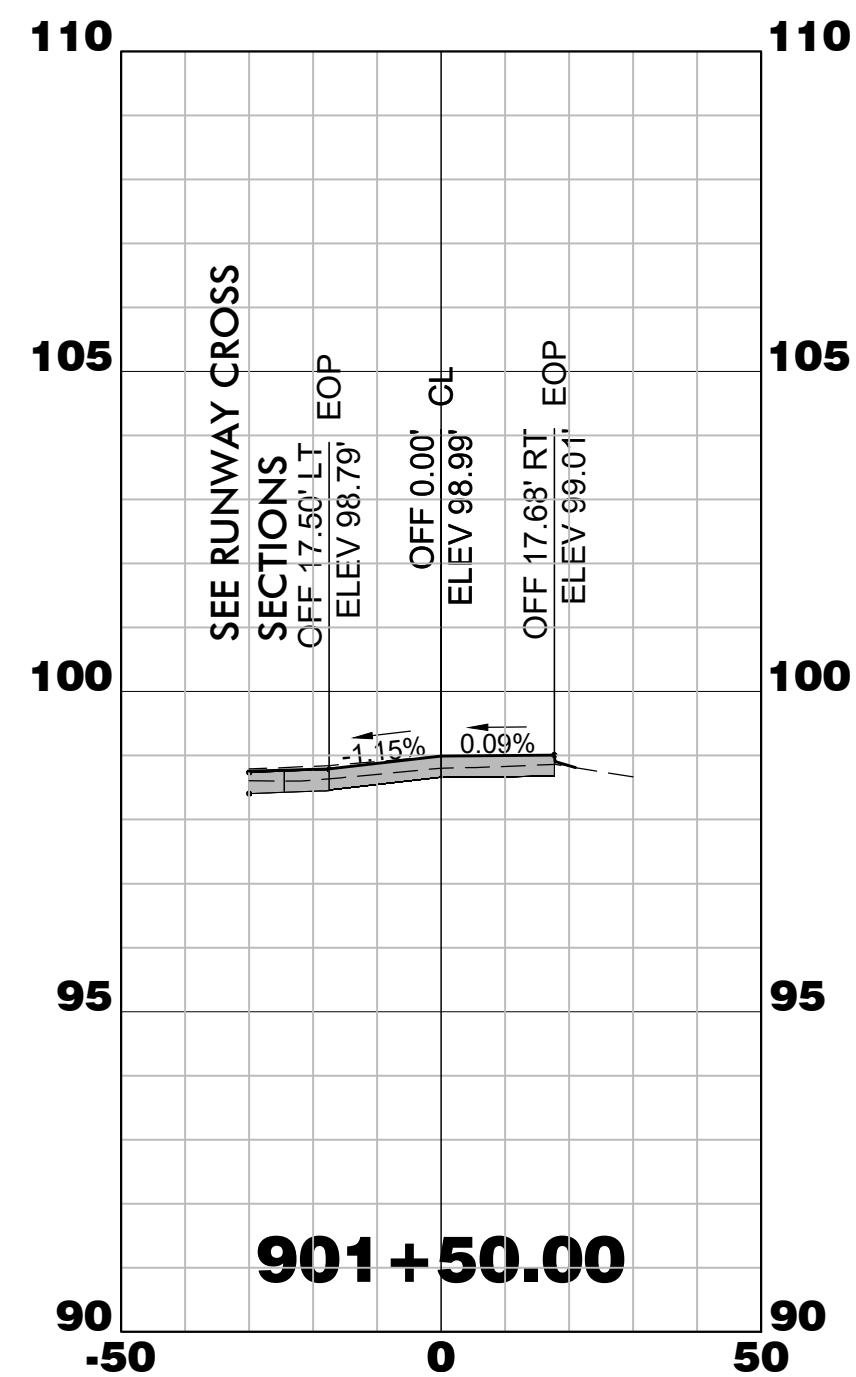
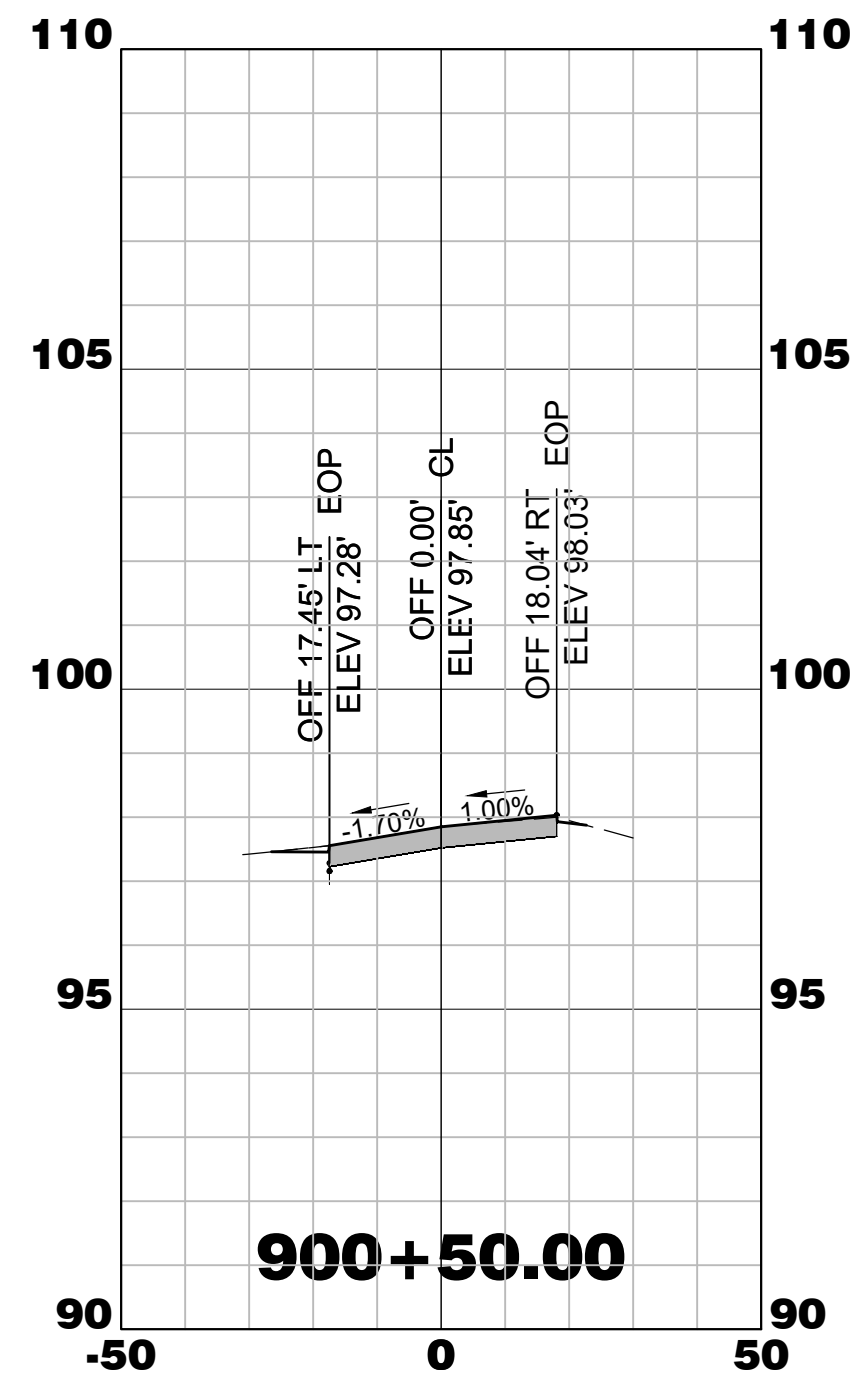
REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

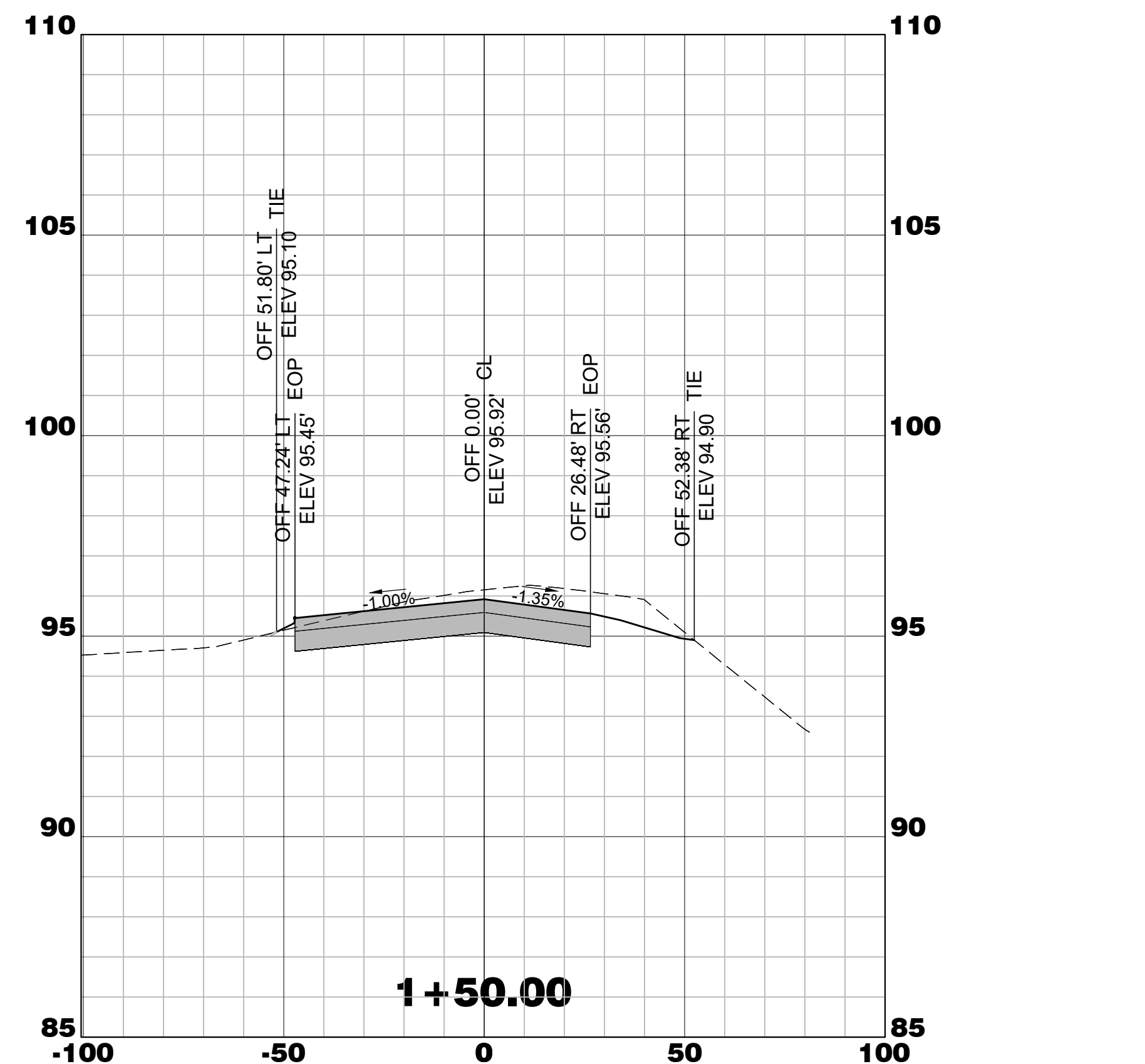
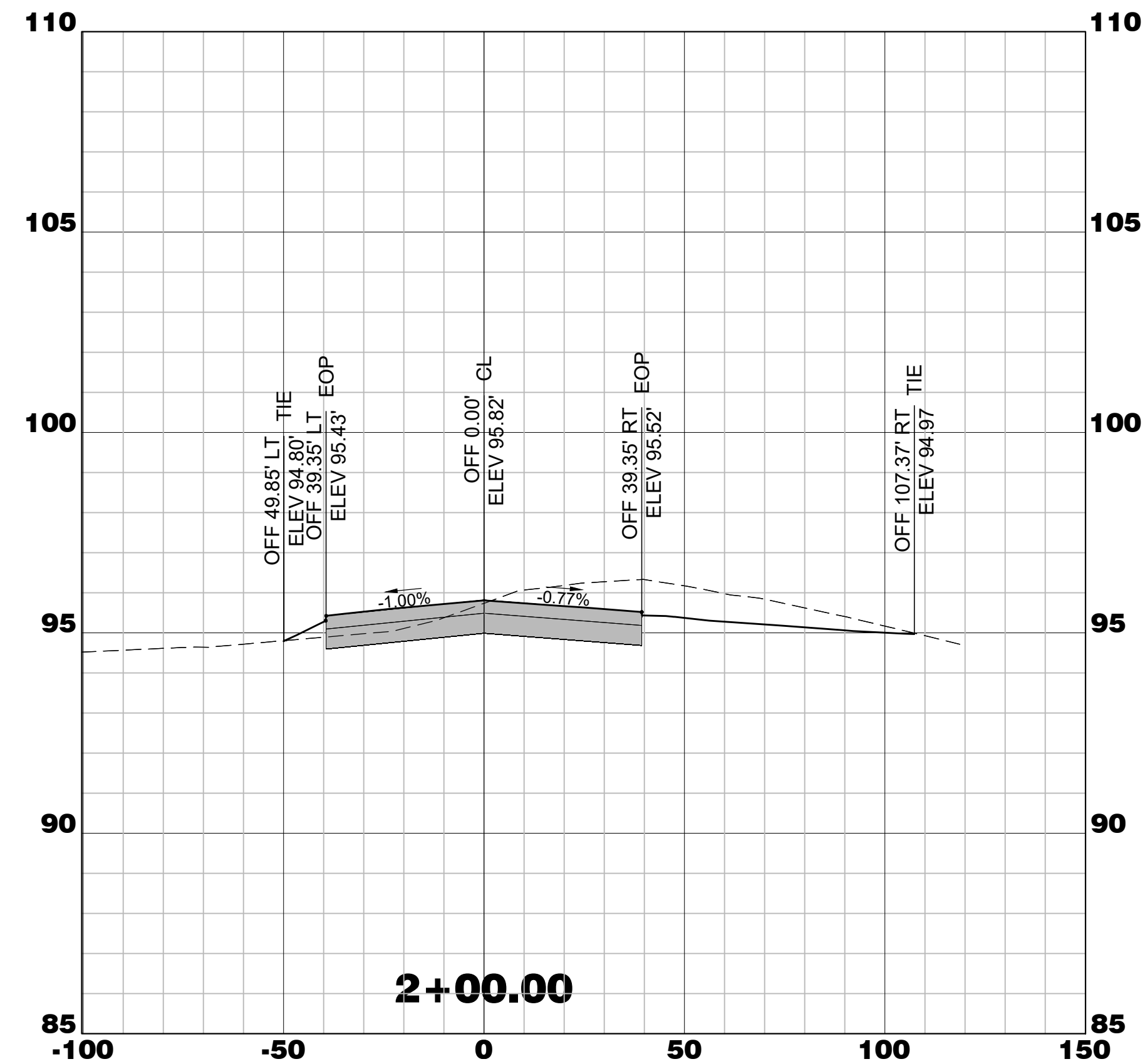
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FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: AIRPORTS
Scale: 1"=50 HORIZ. 1"=5' VERT.	Sheet Number: X-18

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TW A5 CROSS SECTIONS



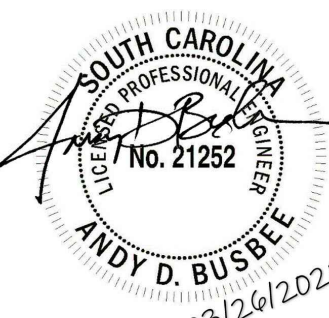
MIDFIELD TW A3 CROSS SECTIONS



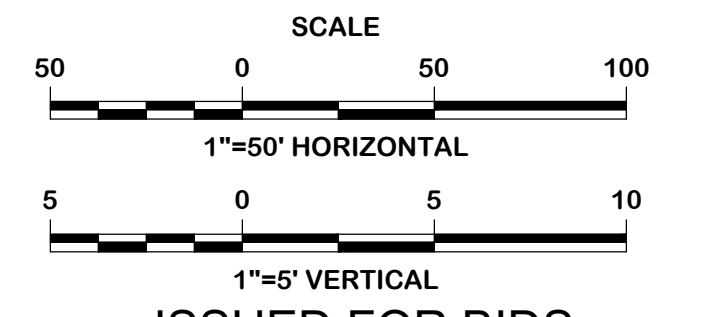
WALTERBORO, SOUTH CAROLINA

Michael Baker INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: NWM	Checked by: ADB
Technician: NWM	Project Number: 192930



ISSUED FOR BIDS

No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
TAXIWAY A5 AND MIDFIELD TAXIWAY A3 CROSS SECTIONS

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

Scale: **1"=50 HORIZ.** Sheet Number:

1"=5' VERT. Drawing Number:

X-19

FIELD LIGHTING NOTES

1. UNLESS OTHERWISE NOTED, ALL UNDERGROUND FIELD POWER MULTIPLE AND SERIES CIRCUIT CONDUCTORS WHETHER DEB OR IN DUCT/CONDUIT SHALL BE FAA APPROVED L-824 TYPE. INSULATION VOLTAGE AND SIZE SHALL BE AS SPECIFIED.
2. NO COMPONENTS OF PRIMARY CIRCUIT SUCH AS CABLE, CONNECTORS AND ISOLATION TRANSFORMERS SHALL BE ROUTED ABOVE GROUND AT LIGHTS, BASES, PULL BOXES, HANDHOLES, ETC.
3. THERE SHALL BE NO EXPOSED POWER, CONTROL CABLES BETWEEN THE POINT WHERE THEY LEAVE THE UNDERGROUND LIGHT BASES AND WHERE THEY ENTER THE EQUIPMENT ENCLOSURES. THESE CABLES SHALL BE ENCLOSED IN RIGID CONDUIT OR IN FLEXIBLE, WATER-TIGHT CONDUIT WITH BREAKABLE COUPLING(S) AT THE GRADE OR THE HOUSING COVER, AS SHOWN ON APPLICABLE DETAILS.
4. THE CABLE ENTRANCES INTO THE FIELD-ATTACHED L-823 CONNECTORS SHALL BE ENCLOSED BY A FULL LENGTH HEAT-SHRINKABLE TUBING WITH CONTINUOUS INTERNAL ADHESIVE.
5. THE ID OF THE PRIMARY L-823 FIELD ATTACHED CONNECTORS SHALL MATCH THE CABLE ID TO PROVIDE A WATER-TIGHT CABLE ENTRANCE. THIS ENTRANCE SHALL BE ENCAPSULATED IN A HEAT SHRINKABLE TUBING WITH CONTINUOUS, FACTORY APPLIED INTERNAL ADHESIVE. HEAT SHRINK SHALL BE ONE-PIECE TYPE WITH TAPE APPLIED ACROSS CONNECTOR JOINT. SEE DETAIL.
6. L-823 TYPE II, TWO-CONDUCTOR SECONDARY CONNECTORS SHALL BE CLASS "A" (FACTORY MOLDED) OR CLASS B (FIELD ASSEMBLY KIT) WHERE NOTED ON THE DRAWINGS.
7. THERE SHALL BE NO SPLICES IN THE SECONDARY CABLE(S) WITHIN THE LIGHT BASE.
8. ELECTRICAL INSULATING GREASE SHALL BE APPLIED WITHIN THE L-823, SECONDARY, TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THESE CONNECTORS SHALL NOT BE TAPED.
9. SEE SPECIFICATION L-108 FOR REQUIREMENTS OF MINIMUM SLACK TO BE PROVIDED IN THE PRIMARY CABLE AT EACH HANDHOLE AND CONNECTOR TERMINATION.
10. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1/2" ABOVE THE EDGE OF THE COVER IN CASE OF BASE MOUNTED COUPLINGS, OR THE TOP OF THE STAKE IN CASE OF STAKE MOUNTED COUPLINGS.
11. TOPS OF THE STAKES SUPPORTING LIGHT FIXTURES SHALL BE FLUSH WITH THE SURROUNDING GRADE.
12. PLASTIC LIGHTING FIXTURE COMPONENTS, SUCH AS LAMP HEADS AND BASE COVERS, ARE NOT ACCEPTABLE.
13. THE TOLERANCE FOR THE HEIGHT OF TAXIWAY EDGE LIGHTS SHALL BE ± ONE (1) INCH. IN CASE OF STAKE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE STAKE AND THE TOP OF THE LENS. IN CASE OF BASE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE BASE FLANGE AND THE TOP OF THE LENS, THUS INCLUDING THE BASE COVER, THE FRANGIBLE COUPLING, THE STEM, THE LAMP HOUSING AND THE LENS.
14. SOIL PERMITTING, THE L-867 BASES MAY BE PRECAST IN CONCRETE OR CAST IN PLACE.
15. LIGHT I.D. NUMBERING SHALL BE AS DIRECTED BY THE OWNER. COORDINATE IN DETAIL WITH THE OWNER'S AUTHORIZED REPRESENTATIVE.
16. CABLE/SPLICE/DUCT MARKERS SHALL BE PRECAST CONCRETE OF THE SIZE SHOWN. LETTERS/NUMBERS/ARROWS FOR THE LEGEND TO BE IMPRESSED INTO THE TOPS OF THE MARKERS SHALL BE PRE-ASSEMBLED AND SECURED IN THE MOLD BEFORE THE CONCRETE IS POURED. LEGEND INSCRIBED BY HAND IN WET CONCRETE WILL NOT BE ACCEPTABLE.
17. ALL UNDERGROUND CABLE RUNS SHALL BE IDENTIFIED BY CABLE MARKERS AT 200 FEET MAXIMUM SPACING, WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS SHALL BE INSTALLED IMMEDIATELY ABOVE THE CABLE.
18. THE CABLE AND SPLICE MARKERS SHALL IDENTIFY THE CIRCUITS WHICH THE CABLES BELONG TO SUCH AS "RW".
19. LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS SHALL BE IDENTIFIED BY DUCT MARKERS WHERE DUCTS DO NOT TERMINATE IN HANDHOLES.
20. ALL POWER AND CONTROL CABLES IN MANHOLE/HANDHOLES SHALL BE TAGGED. USE ENGRAVED STAINLESS STEEL TAGS TO BE ATTACHED AT BOTH ENDS TO THE CABLE BY THE USE OF PLASTIC STRAPS. MINIMUM OF TWO TAGS SHALL BE PROVIDED ON EACH CABLE IN A HANDHOLE - ONE AT THE CABLE ENTRANCE AND ONE AT THE CABLE EXIT.
21. APPLY AN OXIDE INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS AND BREAKAGE COUPLING THREADS.
22. THERE SHALL BE NO SPLICES BETWEEN THE ISOLATION TRANSFORMERS. L-823 CONNECTORS ARE ALLOWED AT TRANSFORMER CONNECTIONS ONLY, UNLESS OTHERWISE SHOWN.
23. CONCRETE USED FOR BACKFILL AROUND L-867 TRANSFORMER HOUSINGS, MARKERS, ETC. SHALL BE 3000 PSI, MINIMUM, AIR-ENTRAINED, UNLESS NOTED OTHERWISE.

GENERAL NOTES

- G1. CONTRACTOR SHALL BE HELD RESPONSIBLE FOR HAVING VISITED THE SITE AND HAVING FAMILIARIZED HIMSELF WITH THE EXISTING CONDITIONS PRIOR TO SUBMITTING HIS BID.
- G2. SHOULD ANY EXISTING ACTIVE WIRING OR OTHER UTILITIES BE CUT BY NEW TRENCHING, SUCH SHALL BE REPAIRED IN MANNER APPROVED BY THE ENGINEER AND OWNER) AND LEFT IN OPERATING CONDITION AT NO ADDITIONAL COST TO THE OWNER.
- G3. WHERE CONDUCTORS ARE SHOWN ON PLANS IN THE SAME GENERAL AREA, THEY SHALL ALL BE RUN IN A COMMON TRENCH.
- G4. PROVIDE ONLY ONE (1) #6 BARE COPPER COUNTER POISE CONDUCTOR IN TRENCH THAT MAY CONTAIN MULTIPLE SERIES CIRCUIT CONDUCTORS. SEE NOTE #G3.
- G5. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE, LOCAL CODES AND FEDERAL AVIATION ADMINISTRATION STANDARDS.
- G6. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS, INSPECTIONS AND APPROVALS. THE COST OF OBTAINING ALL PERMITS SHALL BE INCLUDED IN THE CONTRACTORS BID PROPOSAL.
- G7. THE CONTRACTOR SHALL PROVIDE ALL MATERIALS AND LABOR TO INSTALL THE ELECTRICAL SYSTEMS AS INDICATED ON THE DRAWINGS. ITEMS NOT SHOWN BUT OBVIOUSLY NECESSARY FOR COMPLETION OF THE WORK SHALL BE INCLUDED.
- G8. ALL EQUIPMENT FURNISHED AND INSTALLED BY THE CONTRACTOR OTHER THAN LED LIGHT FIXTURES, SHALL BE GUARANTEED AGAINST DEFECTS IN MATERIALS AND WORKMANSHIP FOR A PERIOD OF ONE YEAR FROM DATE OF FINAL ACCEPTANCE. LED LIGHT FIXTURES SHALL BE GUARANTEED FOR FOUR(4) YEARS FROM DATE OF FINAL ACCEPTANCE.
- G9. CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING UTILITY COMPANIES AND FAA TO IDENTIFY AND LOCATE ANY UNDERGROUND UTILITIES AND/OR CABLE WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL ASSIST UTILITY COMPANY AND AIRPORT OPERATIONS PERSONNEL IN EFFORTS TO FIELD VERIFY UNDERGROUND UTILITIES.
- G10. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM (INCLUDING FAA APPROVED EQUIPMENT) ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NON COMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE OWNER WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
- G11. IN CASE THE CONTRACTOR SELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT REQUIRING ADDITIONAL WIRING, TRANSFORMERS, ADAPTERS, MOUNTINGS, ETC., TO THOSE SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATIONS, ANY COST FOR THESE ITEMS SHALL BE INCIDENTAL TO THE EQUIPMENT COST.
- G12. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTRO- MAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
- G13. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, ETC. WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE FAA APPROVED.
- G14. A MINIMUM OF THREE (3) COPIES OF INSTRUCTION BOOKS SHALL BE SUPPLIED WITH EACH DIFFERENT TYPE OF EQUIPMENT. THE BOOKS DESCRIBING A MORE SOPHISTICATED TYPE OF EQUIPMENT, SUCH AS REGULATORS, CONTROL PANEL, ETC. AS A MINIMUM SHALL CONTAIN THE FOLLOWING:
 (A) A DETAILED DESCRIPTION OF THE OVERALL EQUIPMENT AND ITS INDIVIDUAL COMPONENTS.
 (B) THEORY OF OPERATION INCLUDING THE FUNCTION OF EACH COMPONENT.
 (C) INSTALLATION INSTRUCTIONS.
 (D) START-UP INSTRUCTIONS.
 (E) PREVENTATIVE MAINTENANCE REQUIREMENTS.
 (F) CHART FOR TROUBLESHOOTING.
 (G) COMPLETE POWER AND CONTROL DETAILED WIRING DIAGRAM(S), SHOWING EACH CONDUCTOR/CONNECTION/COMPONENT. "BLACK" BOXES ARE NOT ACCEPTABLE. THE DIAGRAM OR THE NARRATIVE SHALL SHOW VOLTAGES/CURRENTS/WAVE SHAPES AT STRATEGIC LOCATIONS TO BE USED WHEN CHECKING AND/OR TROUBLESHOOTING THE EQUIPMENT. WHEN THE EQUIPMENT HAS SEVERAL MODES OF OPERATION, SUCH AS SEVERAL BRIGHTNESS STEPS, THESE PARAMETERS SHALL BE INDICATED FOR ALL THE DIFFERENT MODES.
 (H) PARTS LIST WHICH WILL INCLUDE ALL MAJOR AND MINOR COMPONENTS, SUCH AS RESISTORS, DIODES, ETC. IT SHALL INCLUDE A COMPLETE NOMENCLATURE OF EACH COMPONENT AND, IF APPLICABLE, THE NAME OF ITS MANUFACTURER AND THE CATALOG NUMBER.
 (I) SAFETY INSTRUCTIONS.
- G15. ALL EXCAVATION WITHIN 3 FT. OF FAA, NWS, AIRFIELD LIGHTING, TELEPHONE COMPANY AND POWER COMPANY CABLES SHALL BE BY HAND DIGGING.
- G16. CONTRACTOR SHALL SEED ALL AREAS DISTURBED BY CONSTRUCTION.

GROUNDING NOTES

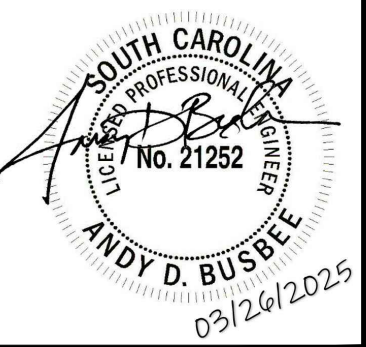
1. GROUND ALL NON-CURRENT-CARRYING METAL PARTS OF ELECTRICAL EQUIPMENT BY USING NO. 6 AWG BARE COPPER WIRE TO BE RUN INSIDE CABINETS AND IN CONDUITS TOGETHER WITH OTHER WIRES. WHERE THIS IS NOT FEASIBLE, RUN THE EXPOSED GROUNDING WIRE PARALLEL OR AT RIGHT ANGLES TO THE BUILDING LINES AND SECURE IT AT LEAST EVERY 24 INCHES AND WITHIN 6 INCHES FROM BEND OR JUNCTION. THE EXPOSED WIRE SHALL BE NO. 6 AWG.
2. ALL GROUND CONNECTIONS TO BUSES, PANELS, ETC. SHALL BE MADE WITH PRESSURE TYPE SOLDERLESS LUGS AND GROUND CLAMPS. SOLDERED OR BOLT AND WASHER TYPE CONNECTIONS ARE NOT ACCEPTABLE UNLESS NOTED OTHERWISE. CLEAN ALL METAL SURFACES BEFORE MAKING GROUND CONNECTIONS.
3. TOPS OF GROUND RODS SHALL BE 6 INCHES BELOW GRADE.
4. ALL CONNECTIONS FOR THE COUNTERPOISE SYSTEM AND TO EQUIPMENT SAFETY GROUND RODS SHALL BE MADE USING EXOTHERMIC WELDS.
5. THE RESISTANCE TO GROUND OF THE NEW COUNTERPOISE SYSTEM SHALL NOT EXCEED 25 OHMS.
6. IF ROCK IS ENCOUNTERED DURING THE GROUND ROD INSTALLATION, THE CONTRACTOR SHALL ATTEMPT TO INSTALL THE GROUND ROD ADJACENT TO THE AREA. IF ROCK IS AGAIN ENCOUNTERED, THE CONTRACTOR SHALL INSTALL THE GROUND ROD AS PER NEC ARTICLE 250, SECTION 250.53 (G) ROD AND PIPE ELECTRODES. NO ADDITIONAL PAYMENT WILL BE MADE DUE TO ROCK ENCOUNTERS.



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Checked by: XXX
Technician: LAW / NWM
Project Number: 192930

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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
LIGHTING GENERAL NOTES

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: ELECTRICAL
Scale: NONE	Sheet Number:
	Drawing Number: LGN-1



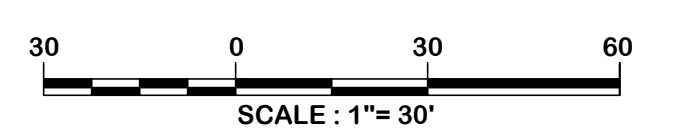
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Designer: BCM / LAW
Checked by: ADB
Technician: LAW
Project Number: 192930



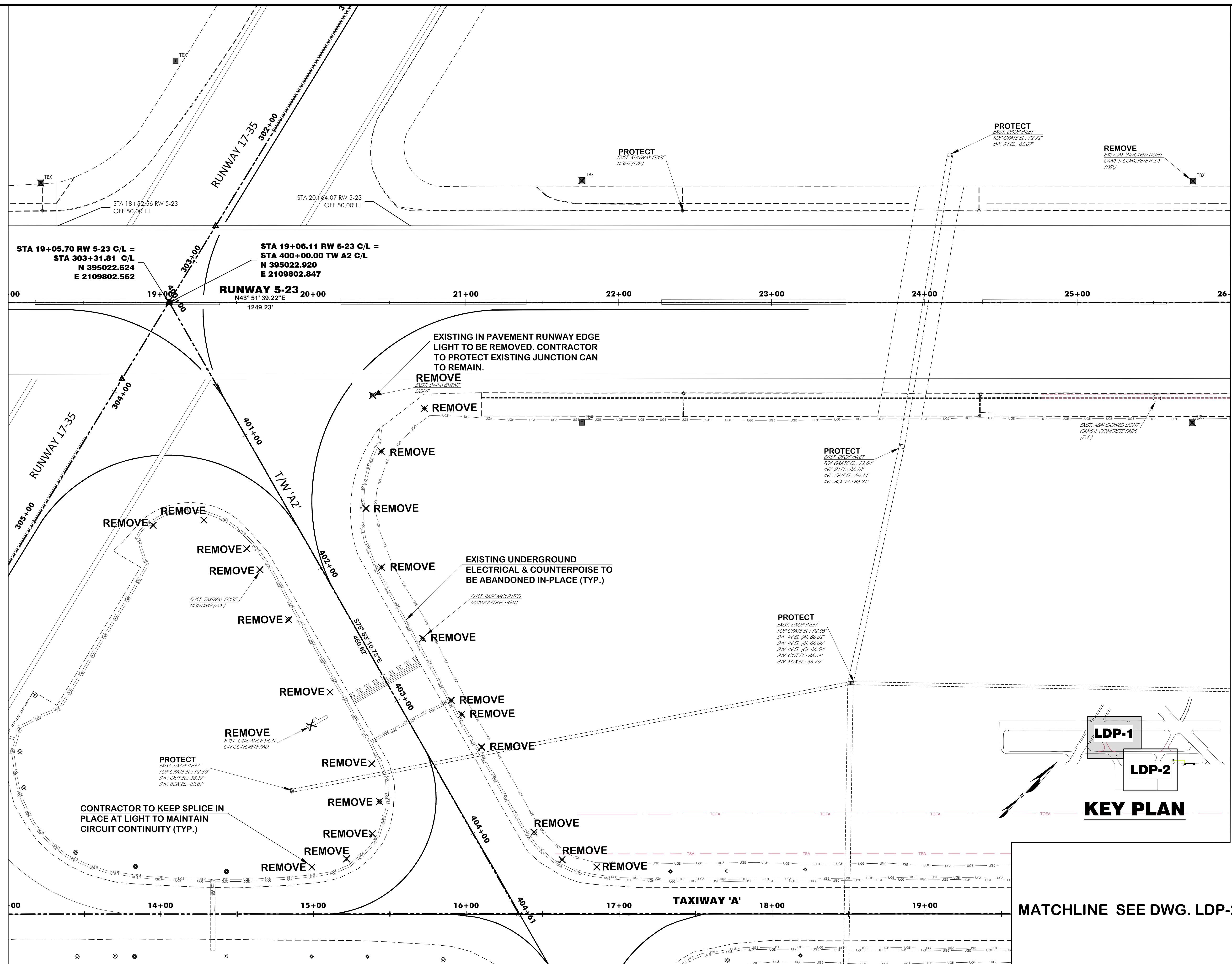
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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

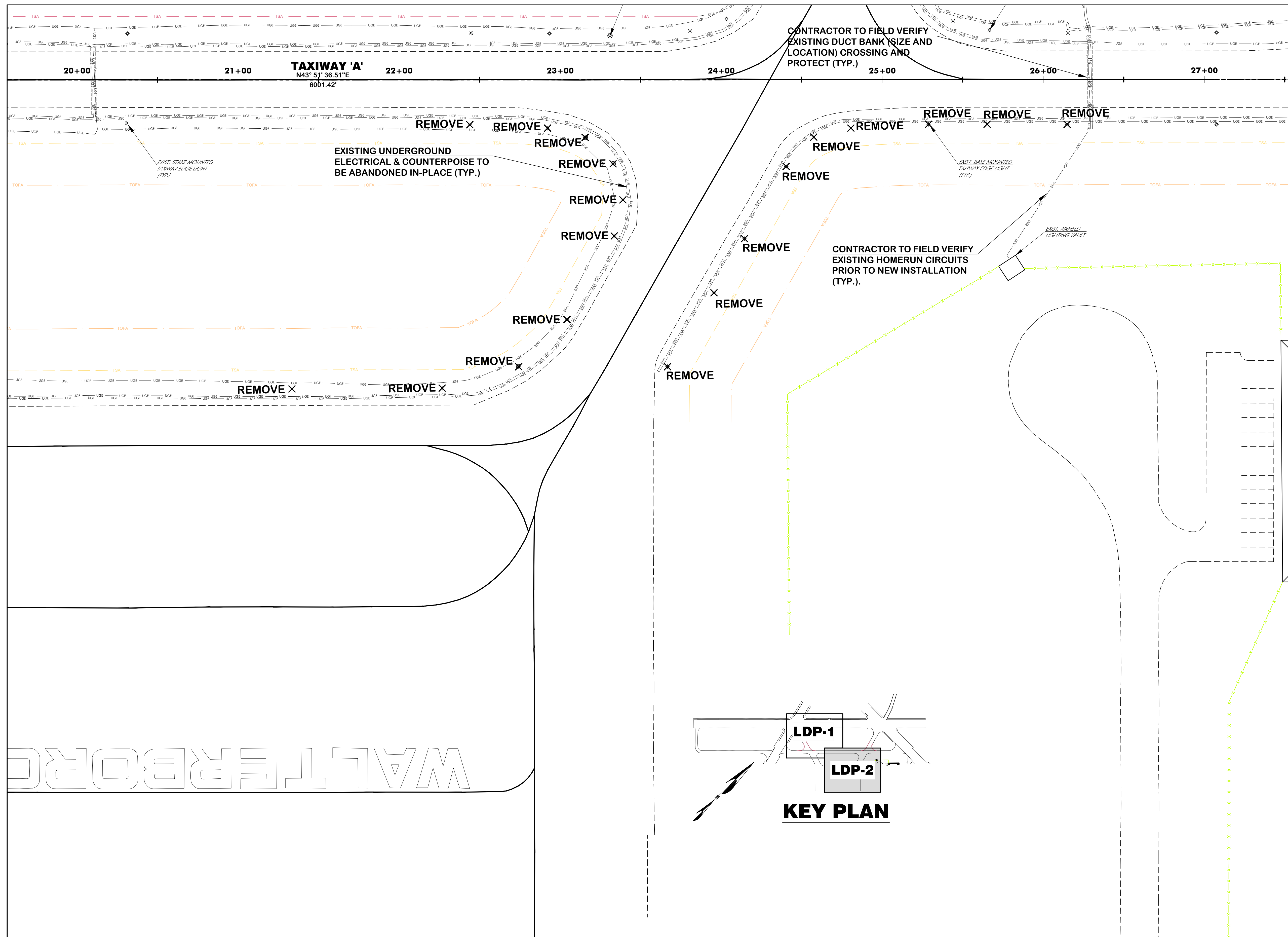
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FAA A.I.P. Project Number:
3-45-0057-025-2025
Date: MARCH, 2025
Division: AIRPORTS
Scale: 1"= 30'
Sheet Number: -
Drawing Number: **LDP-1**



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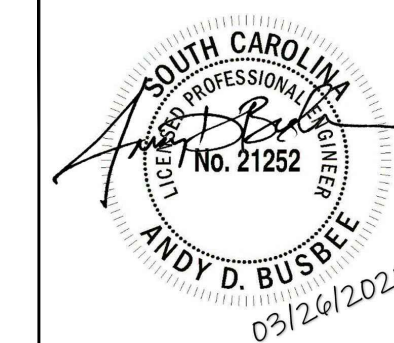
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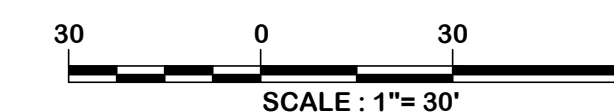
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Designer: BCM / LAW	Checked by: ADB
Technician: LAW	Project Number: 192930



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REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**LIGHTING DEMOLITION
PLAN NO. 2**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025	Division: AIRPORTS
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Drawing Number: LDP-2

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LIGHTING LAYOUT PLAN LEGEND



EXISTING L-861T(L) LED MEDIUM INTENSITY TAXIWAY EDGE LIGHT WITH BLUE GLOBE, L-867B BASE MOUNTED IN EARTH SHOULDER THAT SHALL REMAIN.



EXISTING L-861T(L) LED MEDIUM INTENSITY TAXIWAY EDGE LIGHT WITH BLUE GLOBE, STAKE MOUNTED IN EARTH SHOULDER THAT SHALL REMAIN.



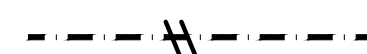
RELOCATED L-861T(L) LED MEDIUM INTENSITY TAXIWAY EDGE LIGHT WITH BLUE GLOBE, NEW STAKE, MOUNTED IN EARTH SHOULDER. PROVIDE NEW ISOLATION TRANSFORMER. PROVIDE #6 BARE SOLID COPPER JUMPER BONDED TO STAKE. CONNECT JUMPER TO GROUND ROD USING EXOTHERMIC WELD. GROUND ROD, #6 JUMPER AND CONNECTION SHALL BE INCLUSIVE TO THE LIGHT RELOCATION PAY ITEM.



RELOCATED L-861T(L) LED MEDIUM INTENSITY TAXIWAY EDGE LIGHT WITH BLUE GLOBE, BASE MOUNTED IN EARTH SHOULDER. PROVIDE NEW ISOLATION TRANSFORMER. PROVIDE #6 BARE SOLID COPPER JUMPER BONDED TO STAKE. CONNECT JUMPER TO GROUND ROD USING EXOTHERMIC WELD. GROUND ROD, #6 JUMPER AND CONNECTION SHALL BE INCLUSIVE TO THE LIGHT RELOCATION PAY ITEM.



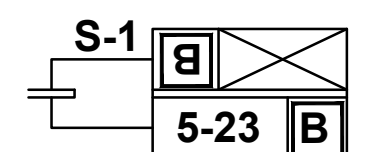
NEW L-861T(L) LED MEDIUM INTENSITY TAXIWAY EDGE LIGHT WITH BLUE GLOBE, BASE MOUNTED IN EARTH SHOULDER. PROVIDE NEW ISOLATION TRANSFORMER. PROVIDE #6 BARE SOLID COPPER JUMPER BONDED TO STAKE. CONNECT JUMPER TO GROUND ROD USING EXOTHERMIC WELD. GROUND ROD, #6 JUMPER AND CONNECTION SHALL BE INCLUSIVE TO THE LIGHT RELOCATION PAY ITEM.



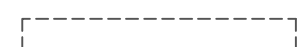
1 - 1/C #8 5KV, L-824C CABLE RUN DIRECT EARTH BURIED (DEB) IN EARTH SHOULDER WITH EARTH BACKFILL. NUMBER OF SLASH MARKS "---" WHERE SHOWN DENOTES THE NUMBER OF NEW CABLES REQUIRED.



1 - 1/C #6 BARE SOLID COPPER COUNTERPOISE IN EARTH TRENCH INSTALLED ONE HALF DISTANCE BETWEEN TAXIWAY OR RUNWAY PAVEMENT EDGE AND CIRCUIT WHERE AIRFIELD LIGHTING CIRCUIT IS RUN PARALLEL TO PAVEMENT EDGE. PROVIDE 1 - 1/C #6 BARE SOLID COPPER COUNTERPOISE RUN 4" ABOVE POWER CABLES IN LOCATIONS THAT POWER CABLES ARE NOT ROUTED PARALLEL TO PAVEMENT EDGE. SEE DETAILS.

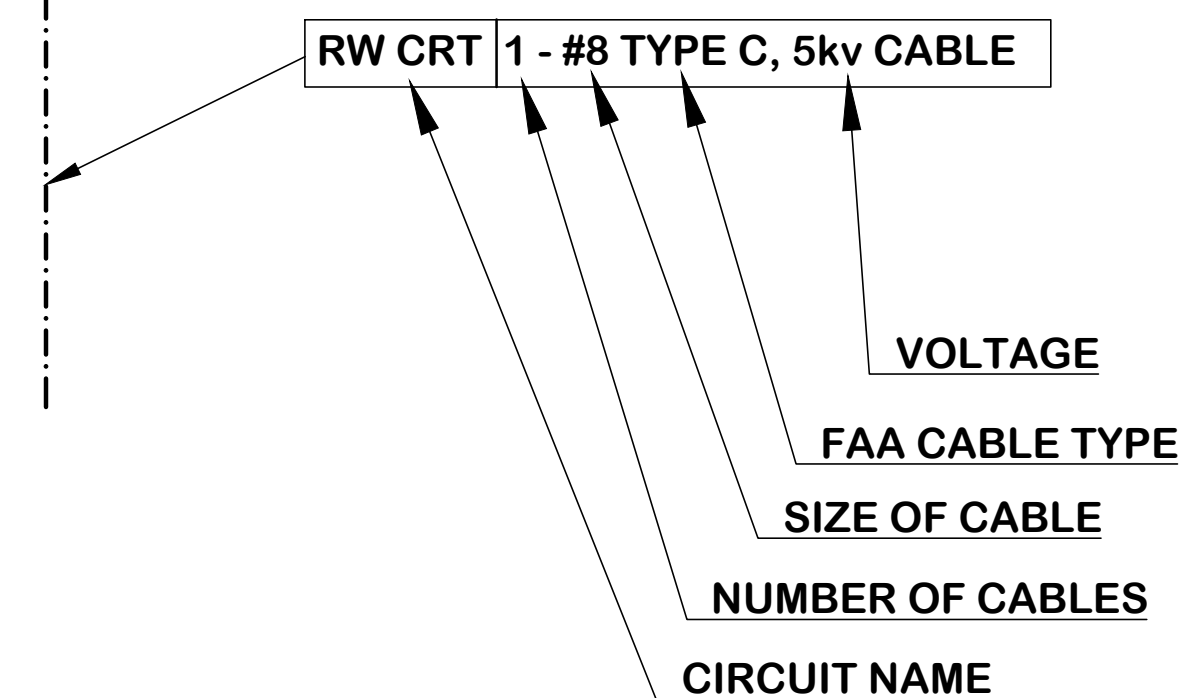


NEW LED L-858(L) TAXIWAY GUIDANCE SIGN MOUNTED ON CONCRETE PAD. SEE SIGN SCHEDULE AND SIGN INSTALLATION DETAILS. SIGN I.D. NUMBERING AS DIRECTED BY THE OWNER



EXISTING CONCRETE ENCASED DUCT BANK. FIELD VERIFY SIZE, QUANTITY AND EXACT LOCATION.

CIRCUIT DESIGNATION AND CALLOUTS



LIGHTING PLAN GENERAL NOTES

- DO NOT SCALE THESE DRAWINGS. DIMENSIONS SHALL BE VERIFIED FROM ACTUAL FIELD CONDITIONS. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING CONDITIONS.
- SEE RUNWAY AND TAXIWAY LIGHT INSTALLATION DETAILS FOR LOCATION DISTANCE AWAY FROM PAVEMENT EDGE OR MARKING (TYPICAL). CONTRACTOR SHALL DETERMINE EXACT LOCATION FOR LIGHTS AND SIGNS SPACING BETWEEN UNITS BY MEASURING START AND END POINTS INDICATED AND EQUALLY SPACED BETWEEN. DO NOT SCALE DRAWING AND DO NOT SPACE USING DIMENSION SHOWN UNLESS START AND ENDING POINT IS FIELD VERIFIED TO BE CORRECT. CONTRACTOR SHALL STAKE OUT LOCATION OF LIGHTS AND SIGNS FOR RESIDENT INSPECTORS REVIEW PRIOR TO INSTALLATION.
- DO NOT DISTURB OR DAMAGE ANY EXISTING CONDUITS, CABLES, DUCT BANKS, LIGHTS OR EQUIPMENT THAT ARE NOT IN THE SCOPE OF THIS PROJECT. ANY CIRCUITS DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR TO THE OWNERS SATISFACTION AT CONTRACTORS EXPENSE.

LIGHTING LAYOUT PLAN NOTES

- BOND NEW COUNTERPOISE TO EXISTING COUNTERPOISE USING EXOTHERMIC WELDS. LABOR AND MATERIALS SHALL BE INCLUSIVE TO THE COUNTERPOISE PER LINEAR FOOT PAY ITEM.
- PROVIDE 1-1/C #8 5KV, L-824C DIRECT BURIED CABLE(S) TO EXISTING STAKE OR BASE MOUNTED MITL AND CONNECT TO ISOLATION TRANSFORMER USING L-823 CONNECTOR KITS WITH HEAT SHRINK TUBING. PROVIDE BARE #6 COPPER COUNTERPOISE AND CONNECT TO EXISTING COUNTERPOISE USING EXOTHERMIC WELDS. CONNECTIONS AND CONNECTOR KITS SHALL BE INCLUSIVE TO THE SPECIFICATION L-108 DIRECT BURIED CABLE INSTALLATION PAY ITEM.
- TAXIWAY INFORMATIONAL SIGNS SHALL BE CONNECTED TO THE TAXIWAY EDGE LIGHT CIRCUIT AS INDICATED. CONNECT USING L-823 CONNECTOR KITS WITH HEAT SHRINK TUBING. ALL LABOR AND MATERIALS SHALL BE INCLUSIVE TO THE L-108 CABLE INSTALLATION PAY ITEMS.
- CONTRACTOR SHALL REMOVE ALL UNUSED OR INACTIVE CABLES FROM THE DUCT BANK OR HANDHOLE. THIS WORK SHALL BE CONSIDERED AS INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT WILL BE MADE.
- TAXIWAY EDGE LIGHTS CABLE ARE RUN IN THIS AREA. FIELD LOCATE AND PROTECT TO AVOID DAMAGE.



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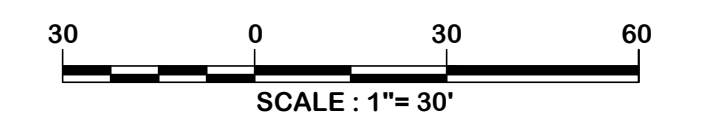


Designer:
BCM / LAW

Checked by:
ADB

Technician:
LAW

Project Number:
192930



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REVISIONS			
No.	Description	Date	By

Project Name:

**RUNWAY 5-23
REHABILITATION**

Drawing Name:

**LIGHTING LAYOUT
LEGEND**

FAA A.I.P. Project Number:

3-45-0057-025-2025

Date: MARCH, 2025

Division: AIRPORTS

Scale:

1" = 30'

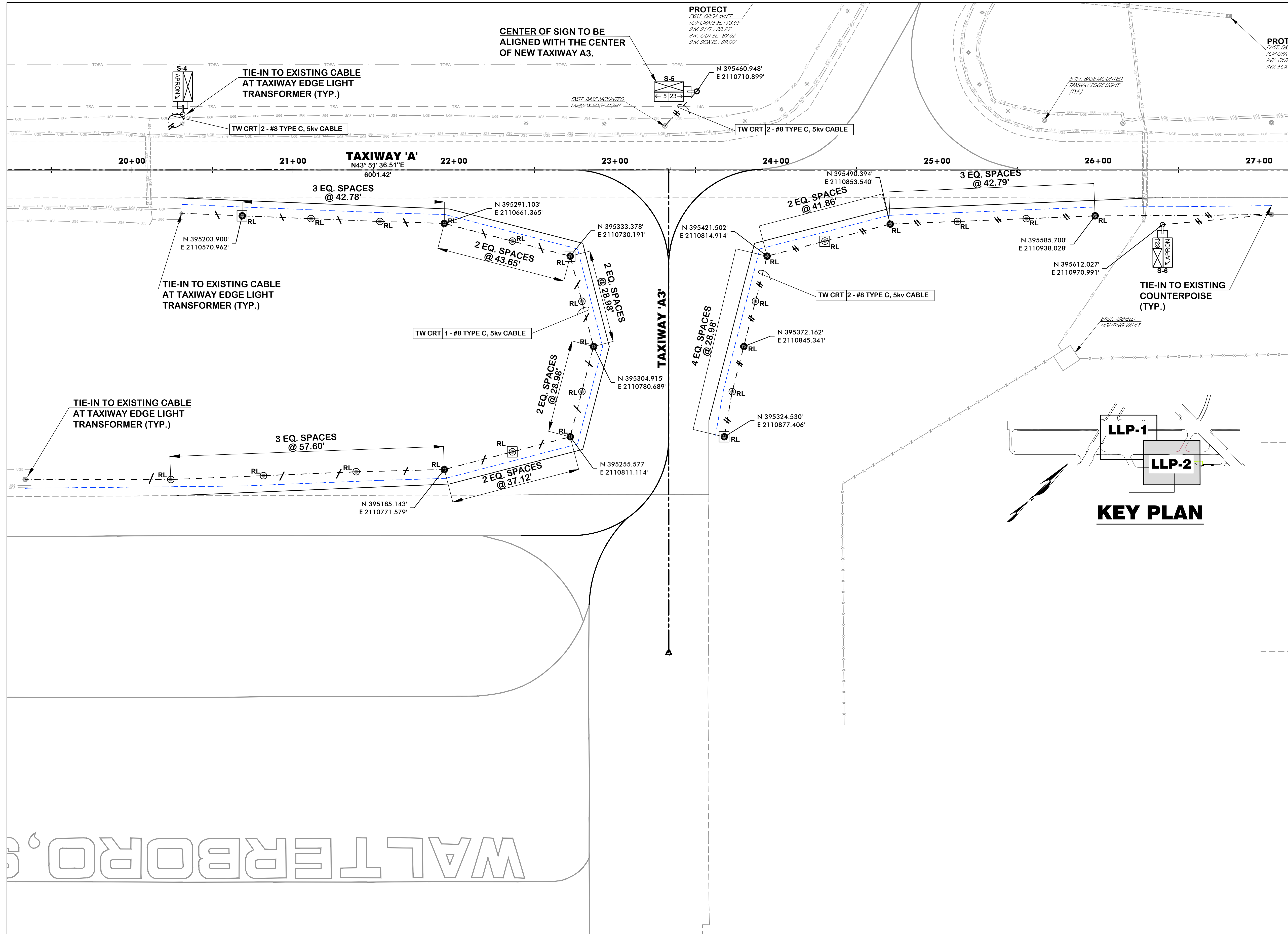
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LLL-1

MATCHLINE SEE DWG. LLP-1



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Designer: BCM / LAW
Checked by: ADB
Technician: LAW
Project Number: 192930



ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
**RUNWAY 5-23
REHABILITATION**

Drawing Name:
**LIGHTING LAYOUT
PLAN NO. 2**

FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **AIRPORTS**

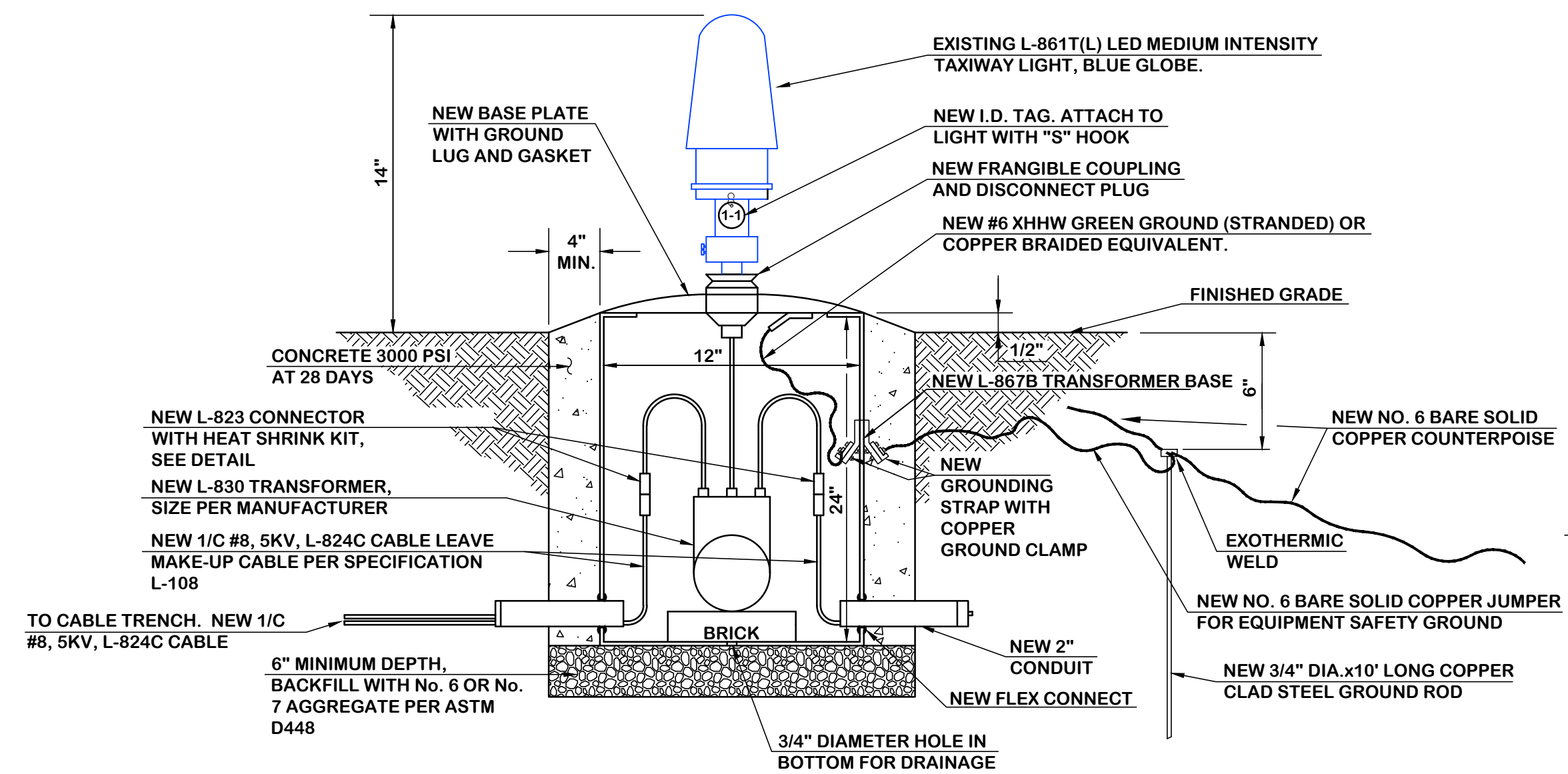
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Drawing Number:
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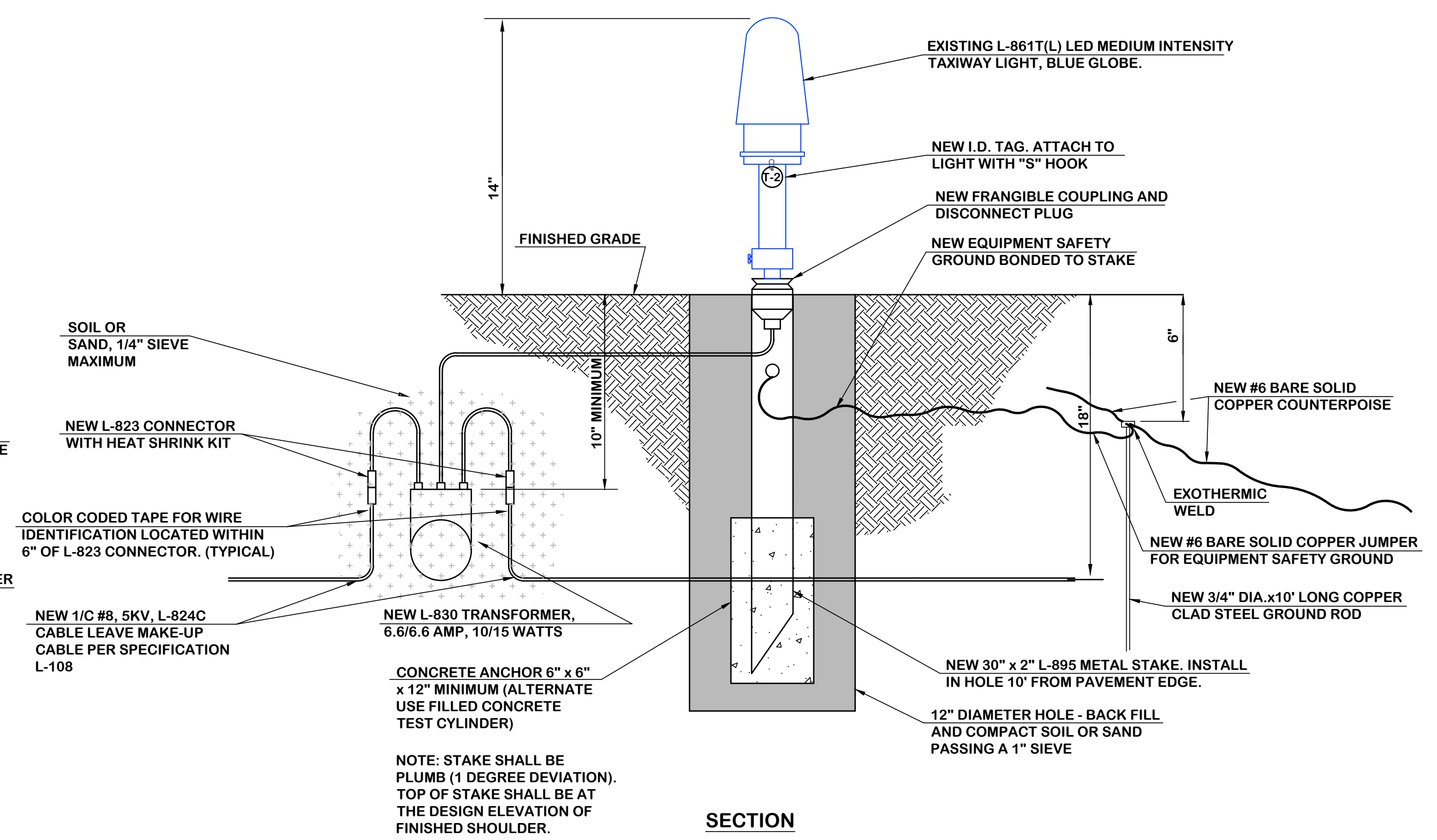
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NOTES:
 1. ALL BOLTS SHALL BE STAINLESS STEEL.
 2. APPLY ANTI-SEIZE TO ALL MALE THREADS.

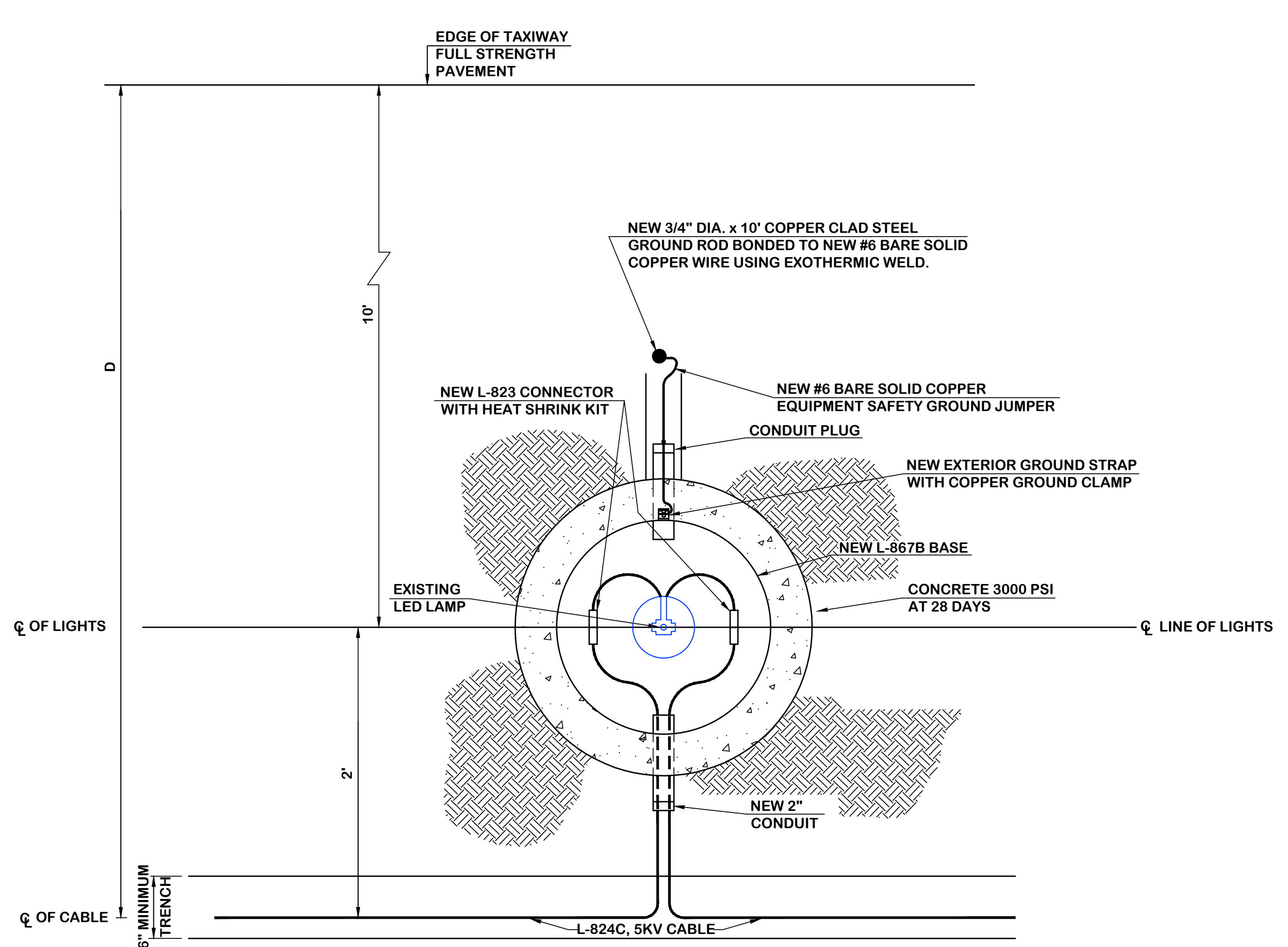


SECTION
BASE MOUNTED TAXIWAY LIGHT DETAIL - RELOCATED
 N.T.S.

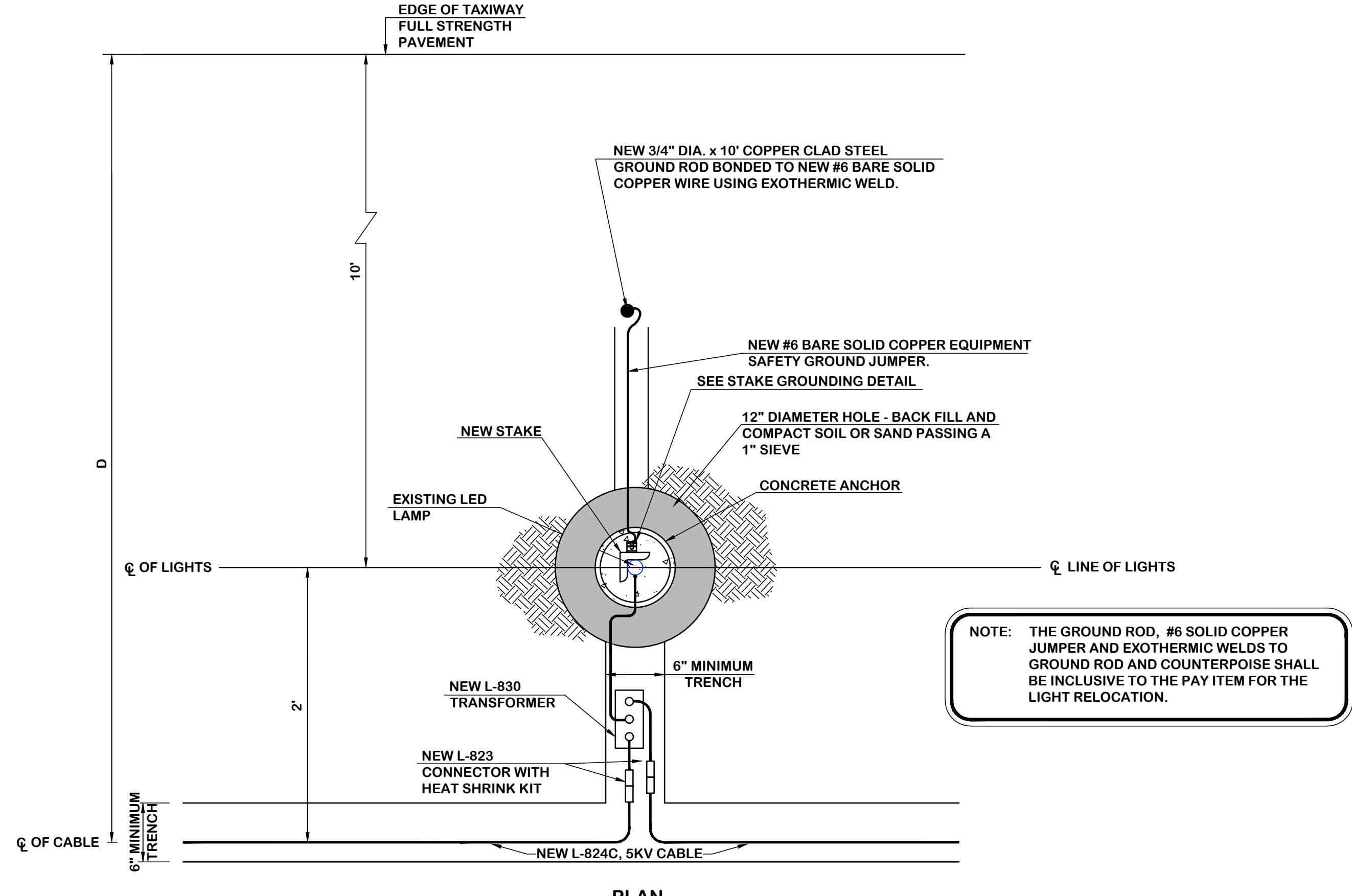
NOTE: THE GROUND ROD, #6 SOLID COPPER JUMPER AND EXOTHERMIC WELDS TO GROUND ROD AND COUNTERPOISE SHALL BE INCLUSIVE TO THE PAY ITEM FOR THE LIGHT RELOCATION.



SECTION
STAKE MOUNTED TAXIWAY LIGHT IN EARTH SHOULDER DETAIL - RELOCATED
 N.T.S.



PLAN
BASE MOUNTED TAXIWAY LIGHT DETAIL - RELOCATED
 N.T.S.



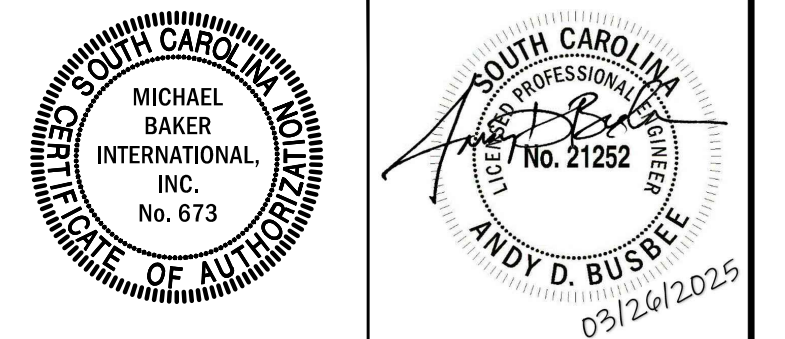
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STAKE MOUNTED TAXIWAY LIGHT IN EARTH SHOULDER DETAIL - RELOCATED
 N.T.S.

NOTE: THE GROUND ROD, #6 SOLID COPPER JUMPER AND EXOTHERMIC WELDS TO GROUND ROD AND COUNTERPOISE SHALL BE INCLUSIVE TO THE PAY ITEM FOR THE LIGHT RELOCATION.



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Designer: RES
 Technician: MTW
 Checked by: GHJ
 Project Number: 192930

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REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

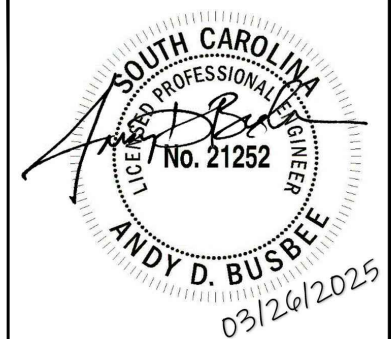
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FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: MARCH, 2025
 Division: ELECTRICAL

Scale: AS NOTED
 Drawing Number: LD-1

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Designer: RES	Checked by: GHL
Technician: MTW	Project Number: 192930

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**RUNWAY 5-23
REHABILITATION**

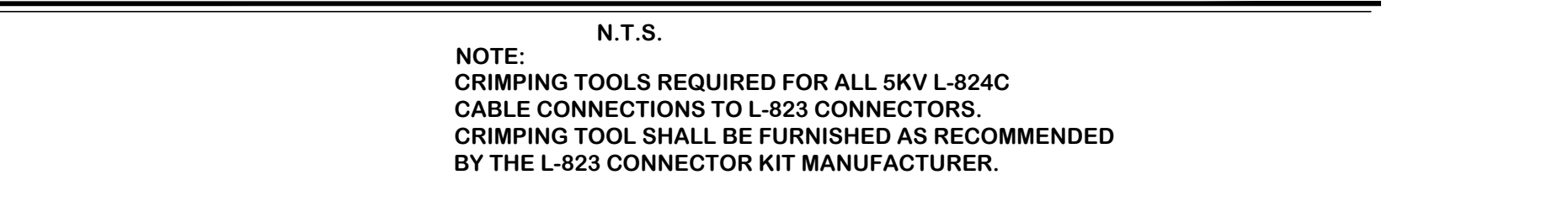
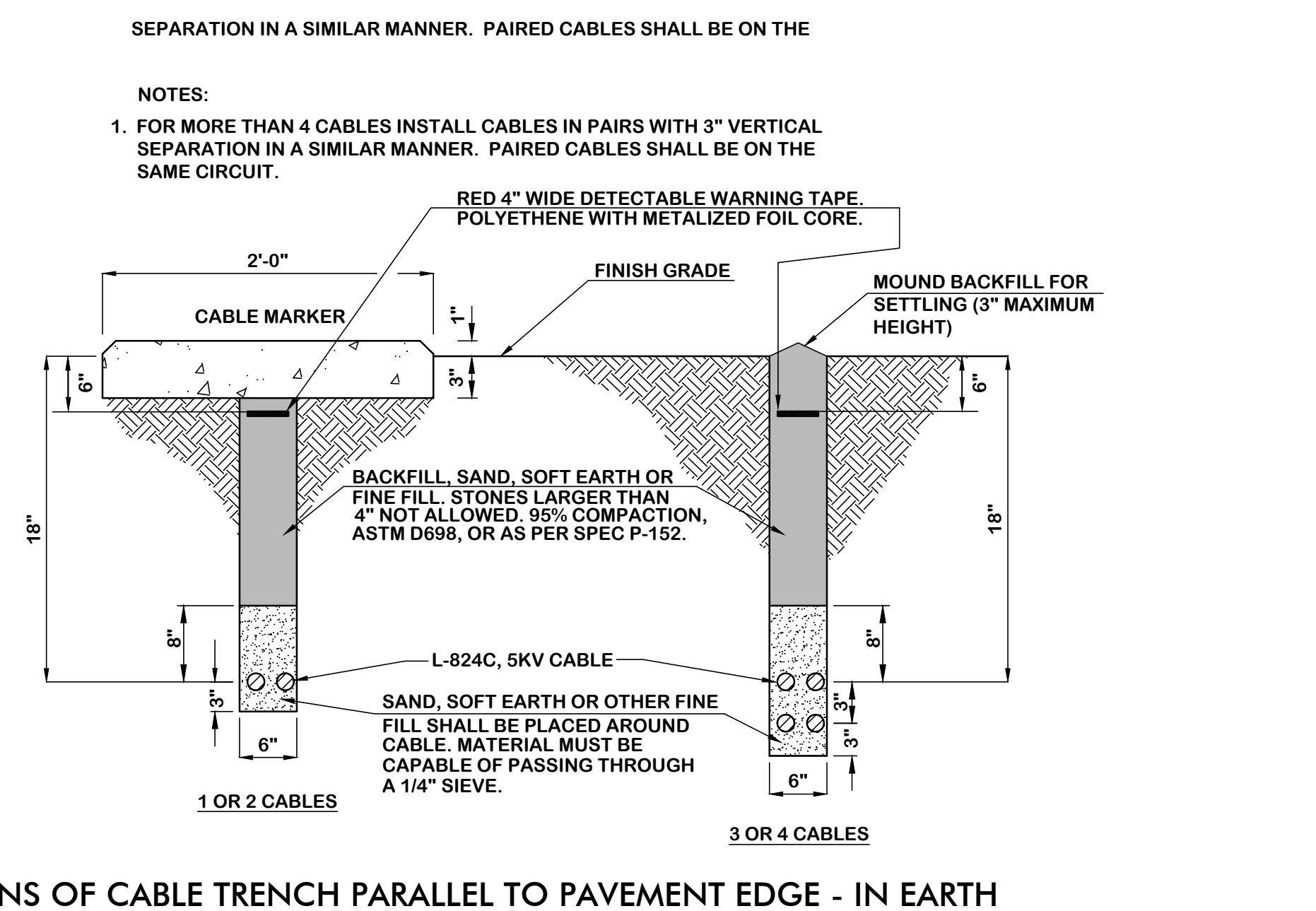
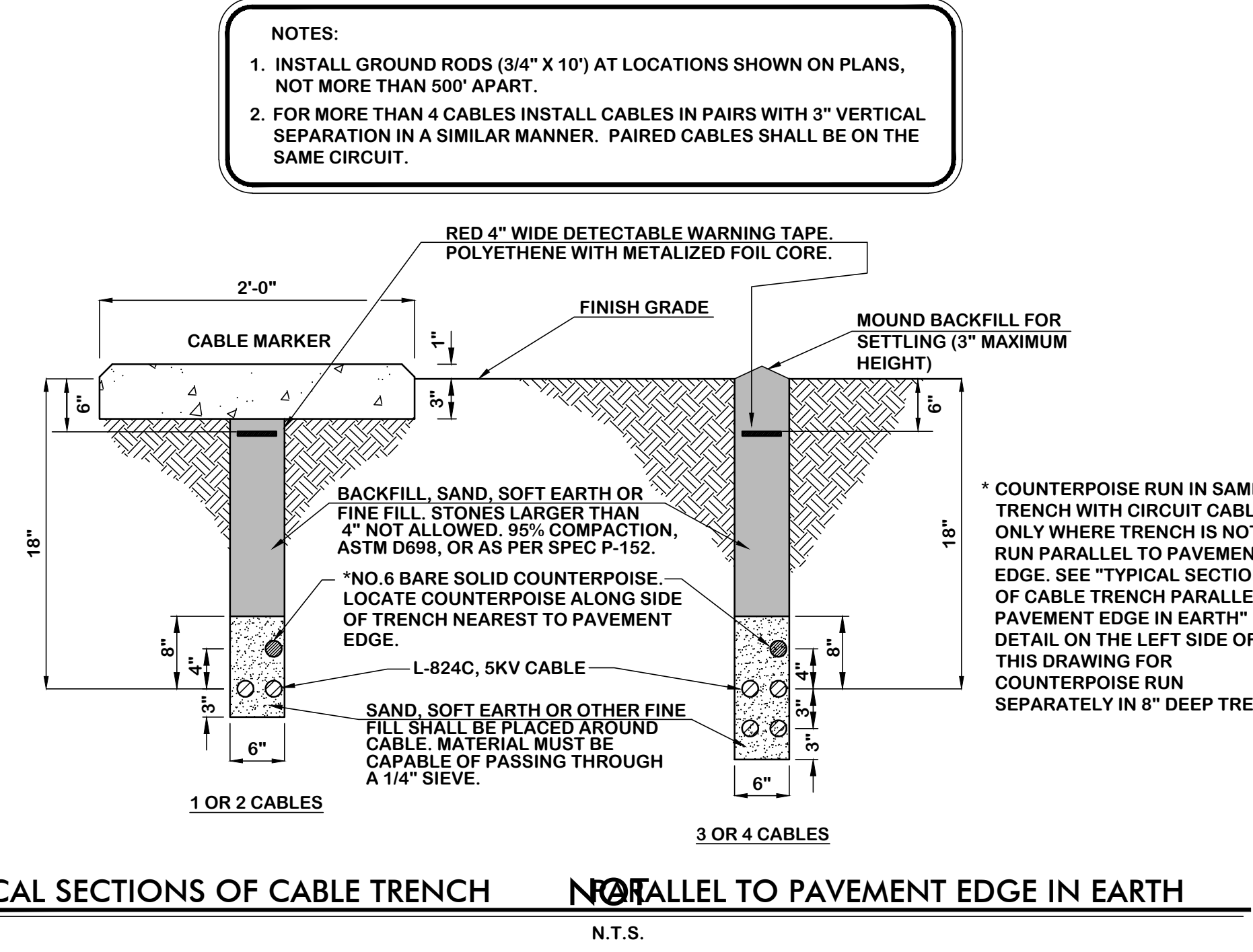
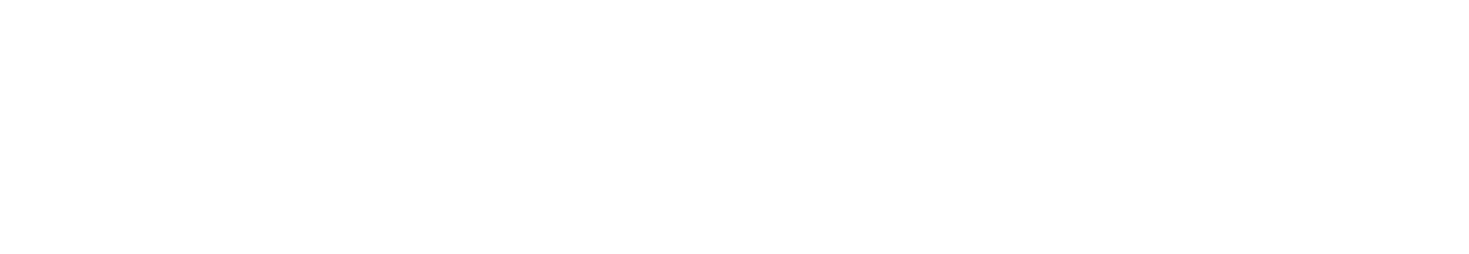
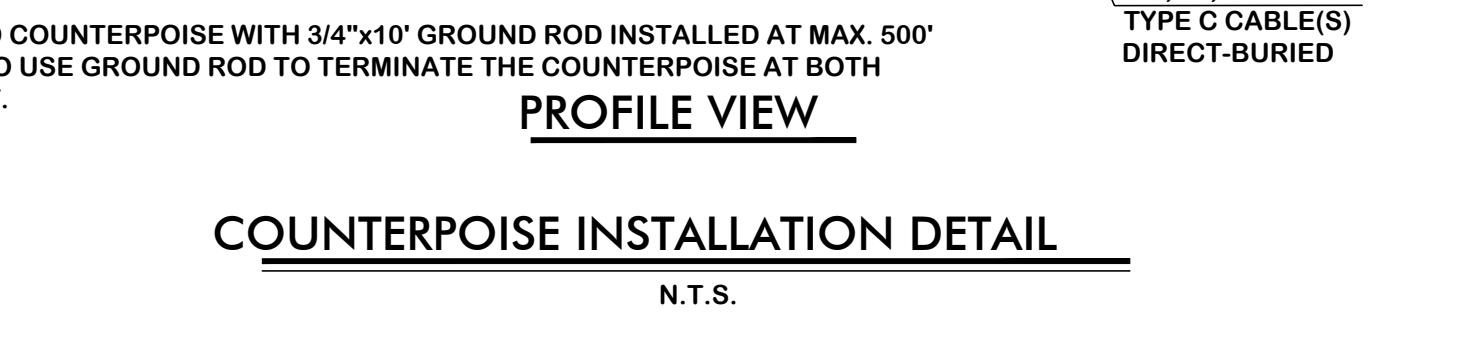
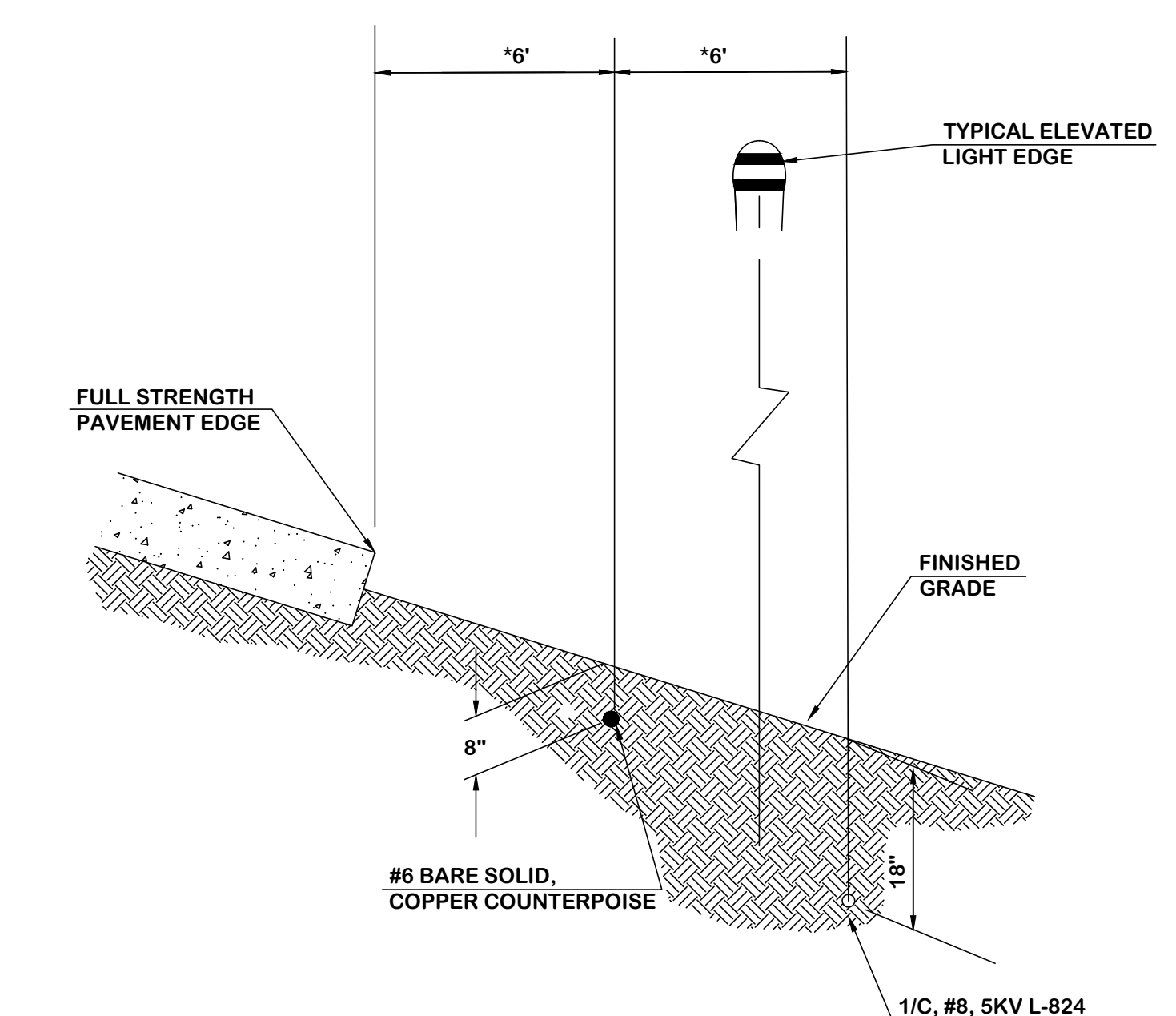
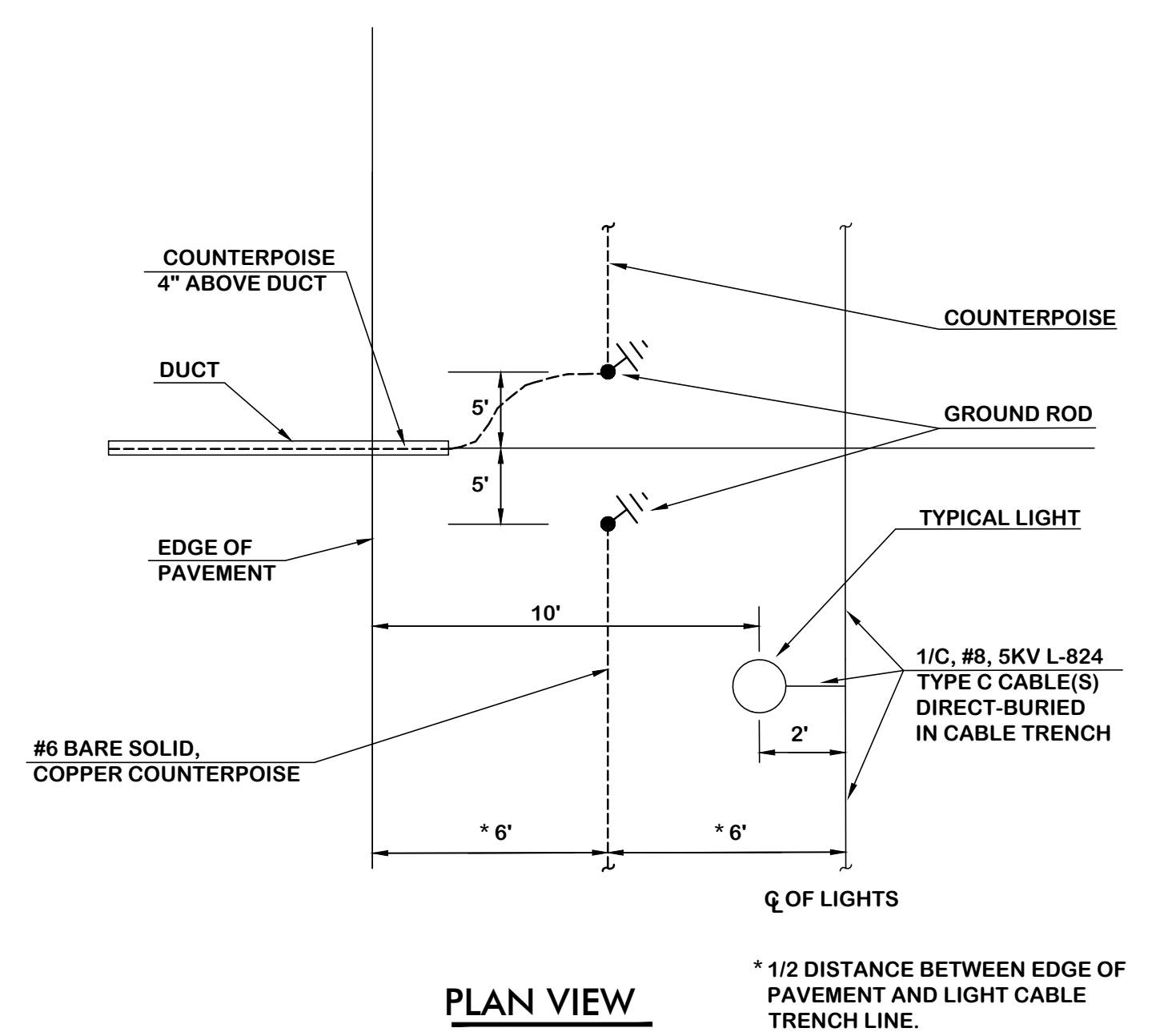
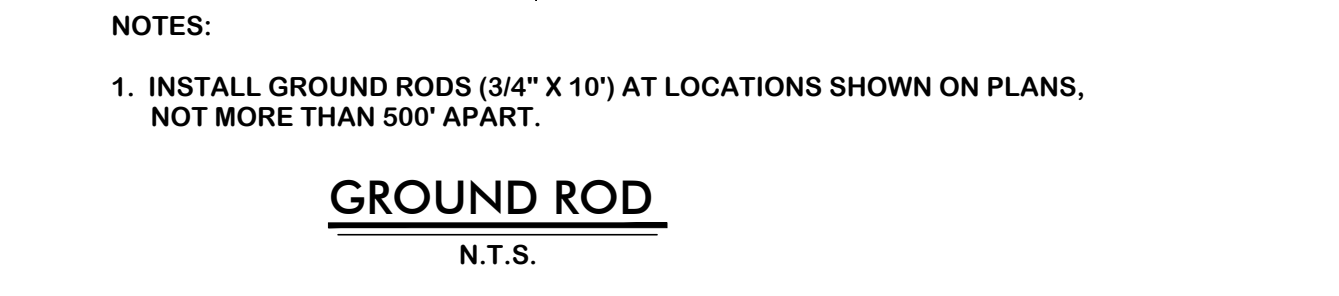
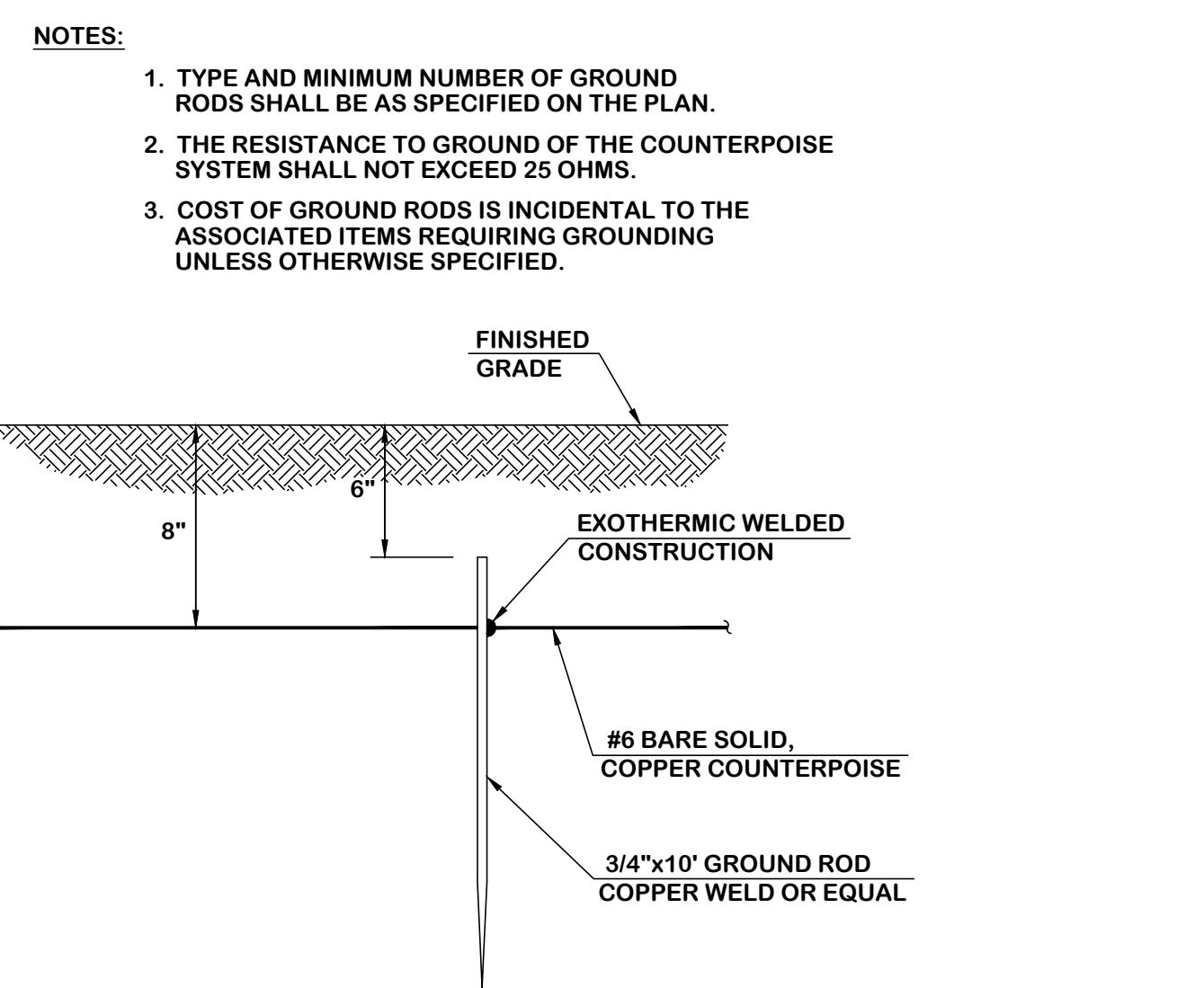
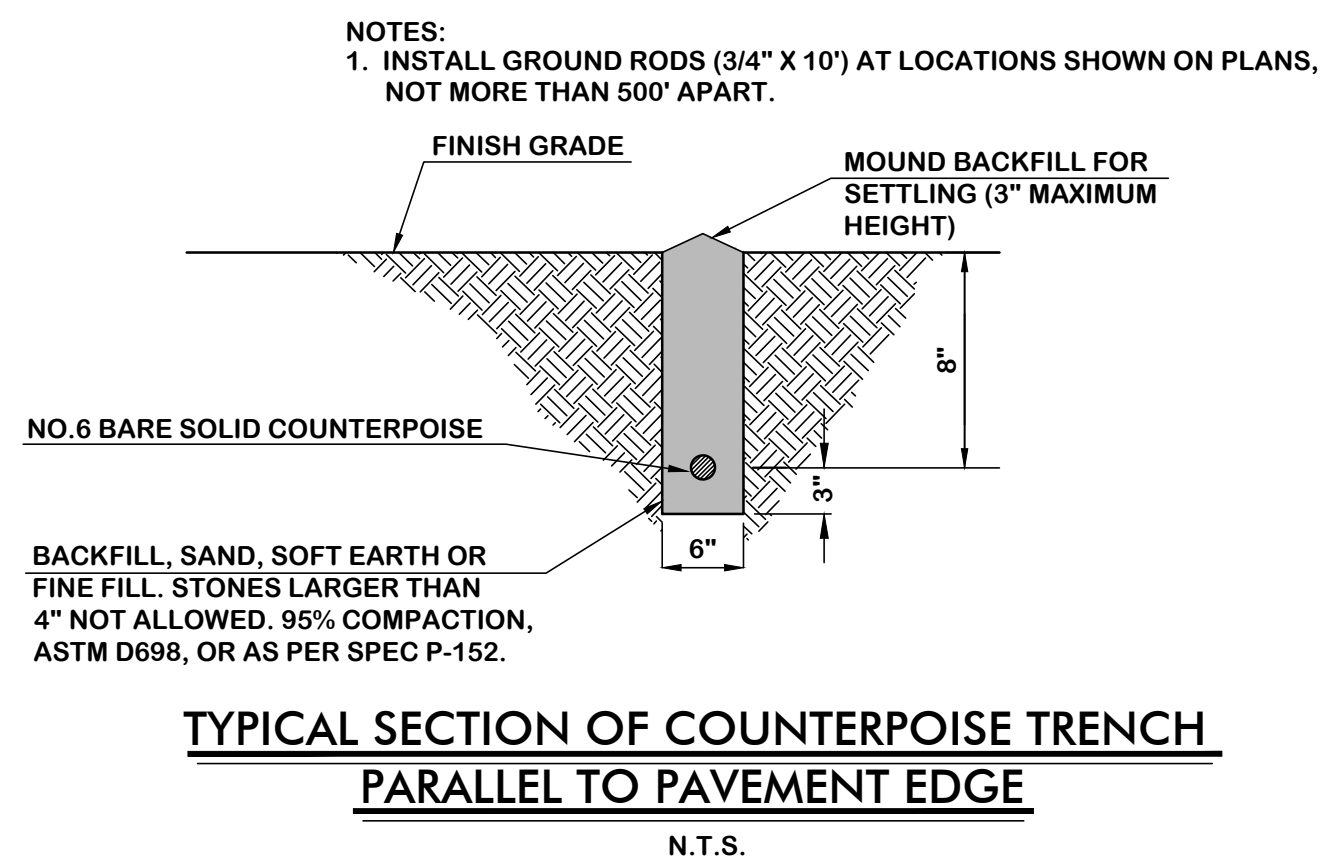
**LIGHTING DETAILS
PLAN NO. 2**

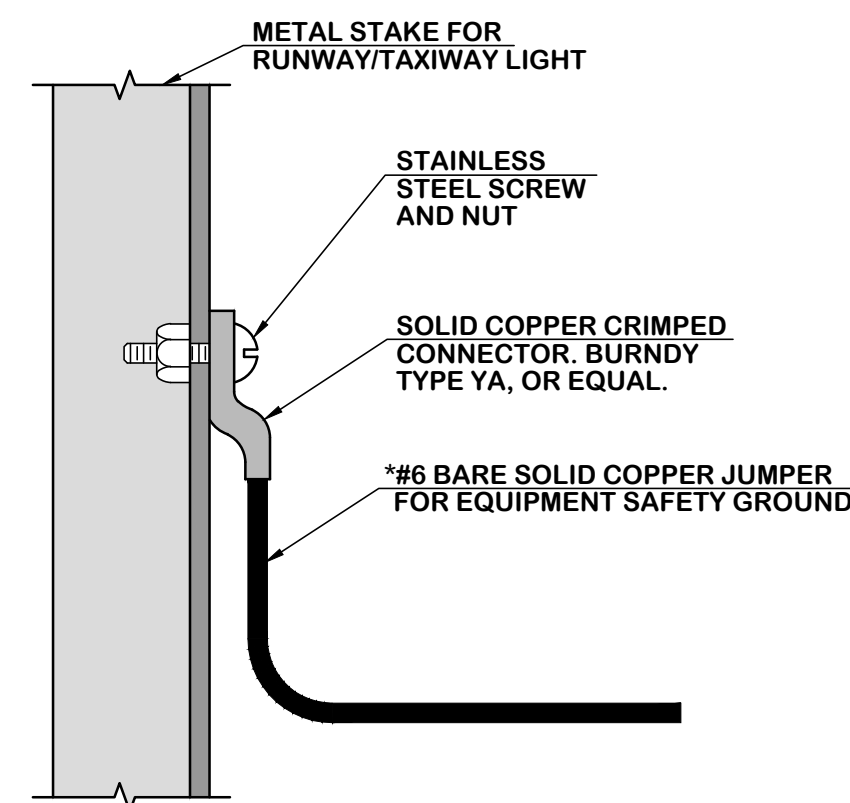
FAA A.I.P. Project Number:
3-45-0057-025-2025

Date: **MARCH, 2025** Division: **ELECTRICAL**

Scale: AS NOTED Sheet Number:

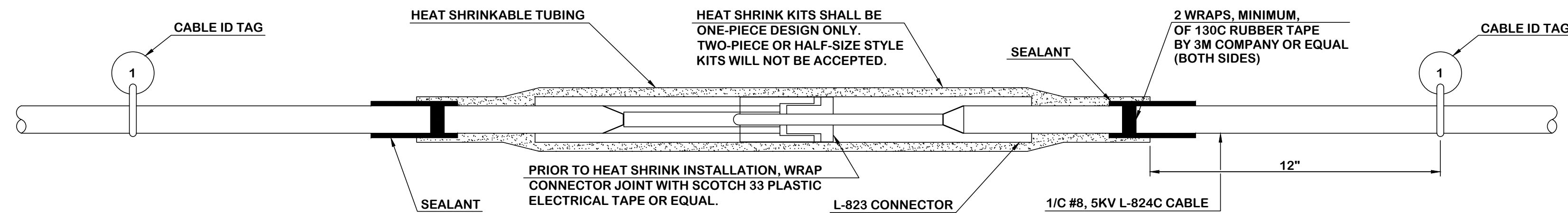
Drawing Number:
LD-2





STAKE GROUNDING DETAIL
N.T.S.

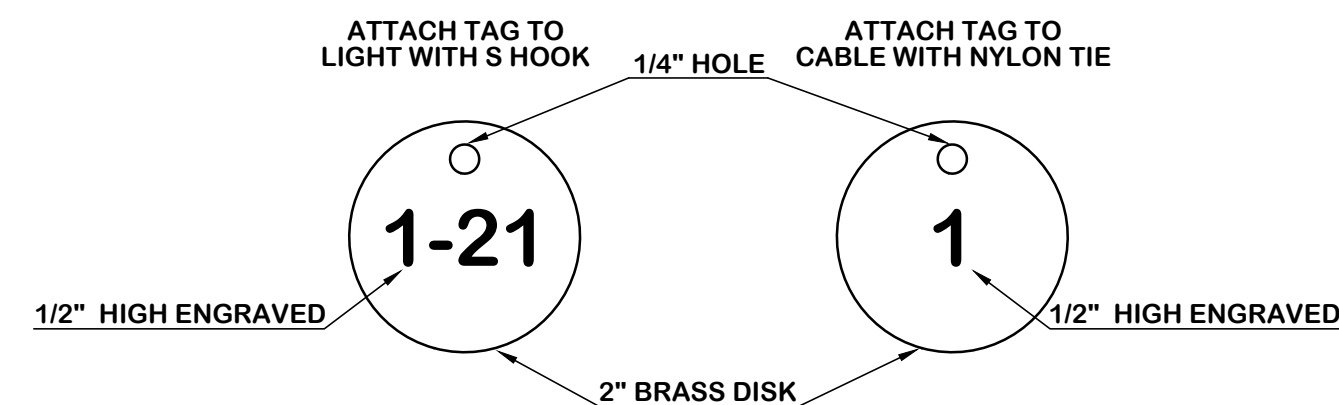
*EQUIPMENT SAFETY GROUND SYSTEM SHALL BE INCLUSIVE TO PAY ITEM FOR LIGHT INSTALLATION.



L-823 CONNECTOR DETAIL
N.T.S.

NOTE: CONTRACTOR MAY ELECT TO USE AMERACE L-823 54 SUPER KIT OR EQUIVALENT OF INTEGRO IN LIEU OF USING HEAT SHRINK KITS. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE USE OF SUPER KITS.

CABLES SPLICES SHALL BE AS APPROVED BY THE OWNER'S AUTHORIZED REPRESENTATIVE. THE CONTRACTOR'S CABLE SPLICE TECHNICIAN SHALL DEMONSTRATE TO THE OWNER, AN APPROVED CABLE SPLICE MADE IN OWNER'S AUTHORIZED REPRESENTATIVE'S PRESENCE. THIS WORK SHALL BE DEMONSTRATED AND APPROVED PRIOR TO ANY CABLE SPLICING WORK FOR THIS PROJECT.

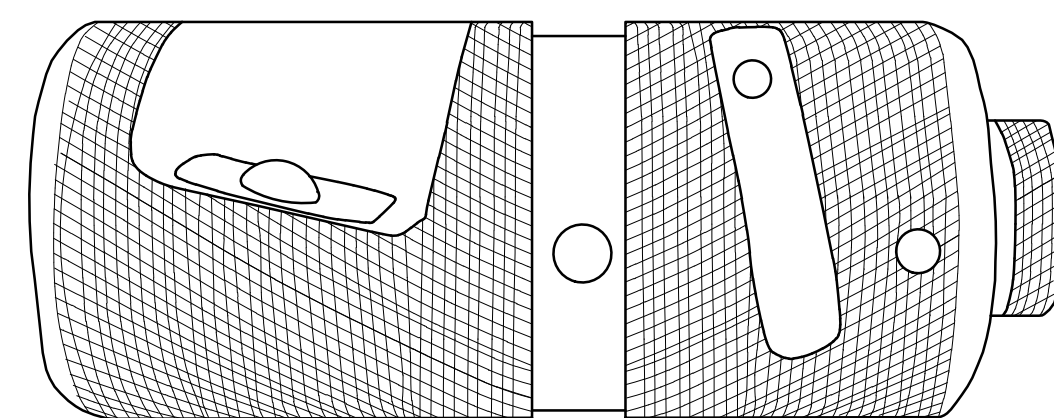


LIGHT ID TAG
N.T.S.

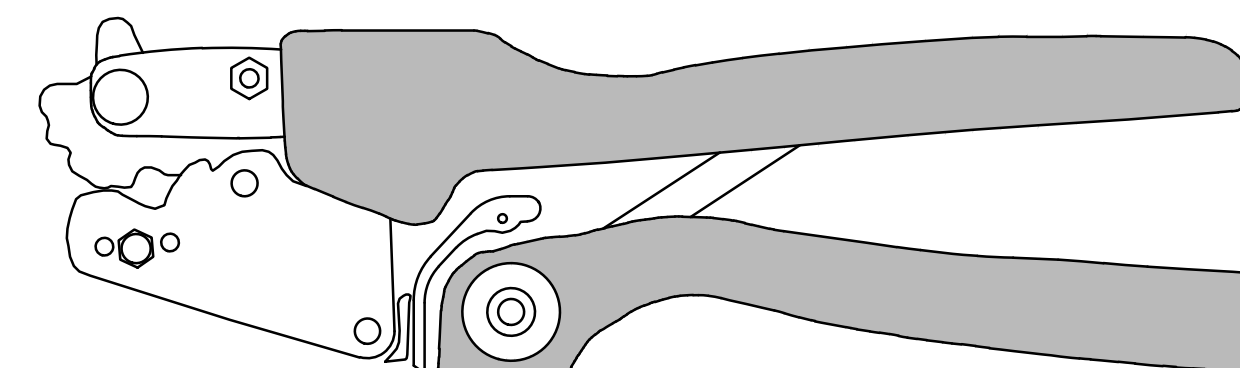
CABLE ID TAG
N.T.S.

NOTE: COORDINATE NUMBERING METHOD WITH THE OWNER.

NOTE: CABLE END STRIPPER/PENCILLER REQUIRED FOR ALL 5KV L-824C CABLE.

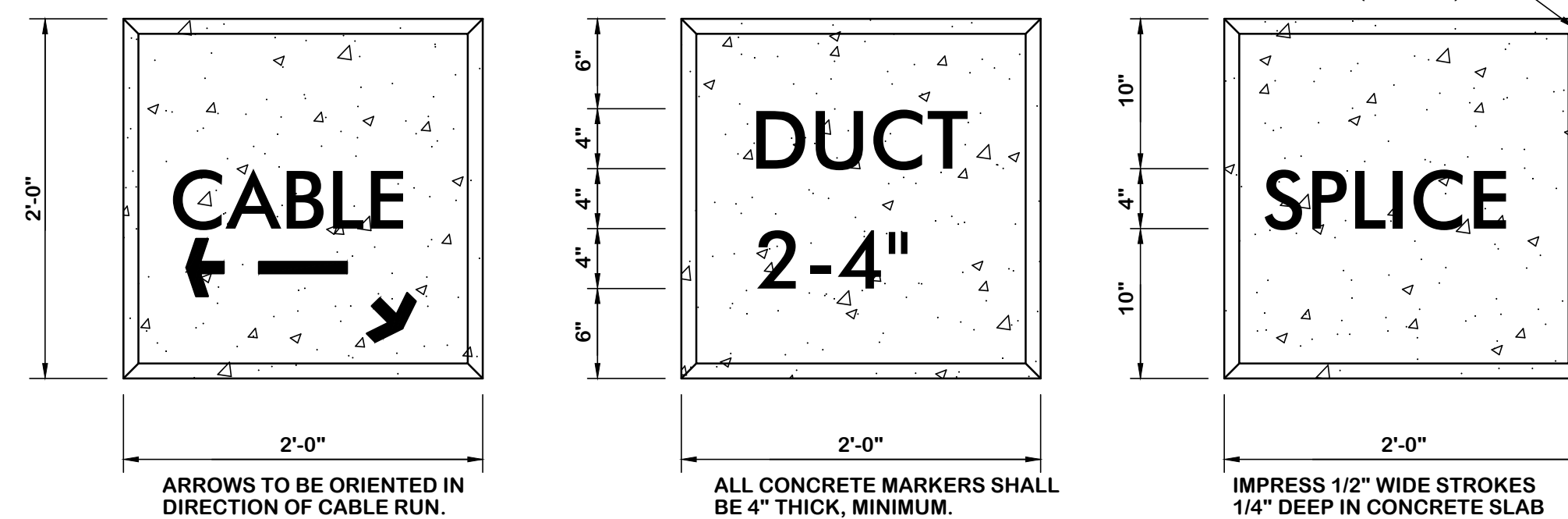


CABLE END STRIPPER/PENCILLER
N.T.S.



CRIMPING TOOL
N.T.S.

NOTE: INSTALL CABLE MARKERS AT ALL TURNS AND JUNCTIONS, AT NOT LESS THAN 200' INTERVALS ON STRAIGHT TRENCH RUNS, AND 10' FROM ENDS OF DUCT RUNS.



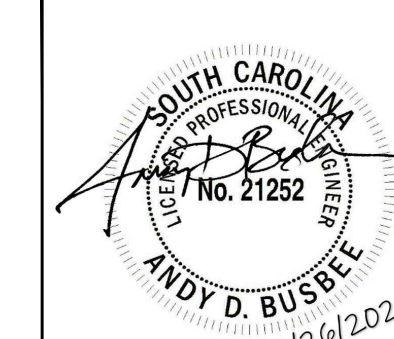
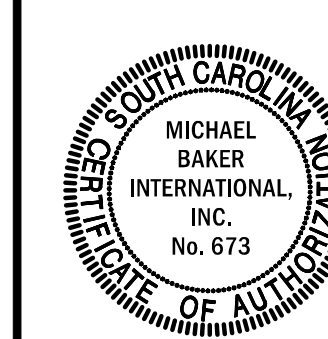
CABLE, DUCT AND SPLICE MARKERS
N.T.S.



WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

Michael Baker International
700 Huger Street
Columbia, South Carolina 29201



Designer: RES	Checked by: GHL
Technician: MTW	Project Number: 192930

ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
LIGHTING DETAILS PLAN NO. 3

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: ELECTRICAL
Scale: AS NOTED	Sheet Number: LD-3
Drawing Number: LD-3	

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Designer: RES

Checked by: GHL

Technician: MTW

Project Number: 192930

ISSUED FOR BIDS

No.	Description	Date	By

Project Name: **RUNWAY 5-23 REHABILITATION**

Drawing Name: **LIGHTING DETAILS PLAN NO. 4**

FAA A.I.P. Project Number: **3-45-0057-025-2025**
Date: **MARCH, 2025** Division: **ELECTRICAL**

Scale: **AS NOTED** Sheet Number: **LD-4**
Drawing Number: **LD-4**

NEW AND RELOCATED L-858 SIGN SCHEDULE

NO.	MESSAGE (SEE NOTE 5)				CLASSIFICATION SIZE-STYLE	PAY ITEM DESCRIPTION SIZE - # FACES - # CHARACTERS - #	MAXIMUM VA LOADING	CIRCUIT FOR CONNECTION	REMARKS
	SIDE A	(TYPE)	SIDE B	(TYPE)					
S-1	AP/RO/N >	(V/Y/Y)	T5	(Y)	1-2	1-2-8 (3 MODULES)	-	TAXIWAY	NEW SIGN
S-2	<- 5/23 ->	(V/Y/Y)	(BLANK)	-	1-2	1-2-5 (3 MODULES)	-	TAXIWAY	NEW SIGN
S-3	<-A/PR/ON	(V/Y/Y)	(BLANK)	-	1-2	1-2-6 (3 MODULES)	-	TAXIWAY	NEW SIGN
S-4	AP/RO/N >	(V/Y/Y)	(BLANK)	-	1-2	1-2-6 (3 MODULES)	-	TAXIWAY	NEW SIGN
S-5	<- 5/23 ->	(V/Y/Y)	(BLANK)	-	1-2	1-2-5 (3 MODULES)	-	TAXIWAY	NEW SIGN
S-6	<-A/PR/ON	(V/Y/Y)	T2/3	(Y/Y)	1-2	1-2-9 (3 MODULES)	-	TAXIWAY	NEW SIGN

NOTES:

- 1. ALL SIGNS SHALL BE INTERNALLY ILLUMINATED, WITH RETROREFLECTIVE TYPE FACE.
- 2. PROVIDE EARTH BERM FOR CONCRETE PAD TO LEVEL SIGN AS REQUIRED.
- 3. ALL SIGNS SHALL BE CLASS 1 (-20C OPERATION), UNLESS SPECIFIED OTHERWISE. CLASS 2 (-55C OPERATION) SIGNS ARE ACCEPTABLE.
- 4. FAA CLASSIFICATIONS:
TYPE Y : L-858 INFORMATIONAL - YELLOW BACKGROUND, BLACK LEGEND.
TYPE R : L-858 MANDATORY - RED BACKGROUND, WHITE LEGEND.
TYPE B : L-858 RUNWAY DISTANCE MARKER - BLACK BACKGROUND, WHITE LEGEND.
TYPE L : L-858 LOCATION - BLACK BACKGROUND, YELLOW LEGEND.

STYLE 1 - 120 VOLT POWER.
STYLE 2 - 4.8 TO 6.6 AMP POWER (3 STEP SYSTEM).
STYLE 3 - 2.8 TO 6.6 AMP POWER (5 STEP SYSTEM).
STYLE 4 - UNLIGHTED.
STYLE 5 - FIXED 5.5 AMP POWER (CONNECTED STEP 2 OF 3 STEP REGULATOR)

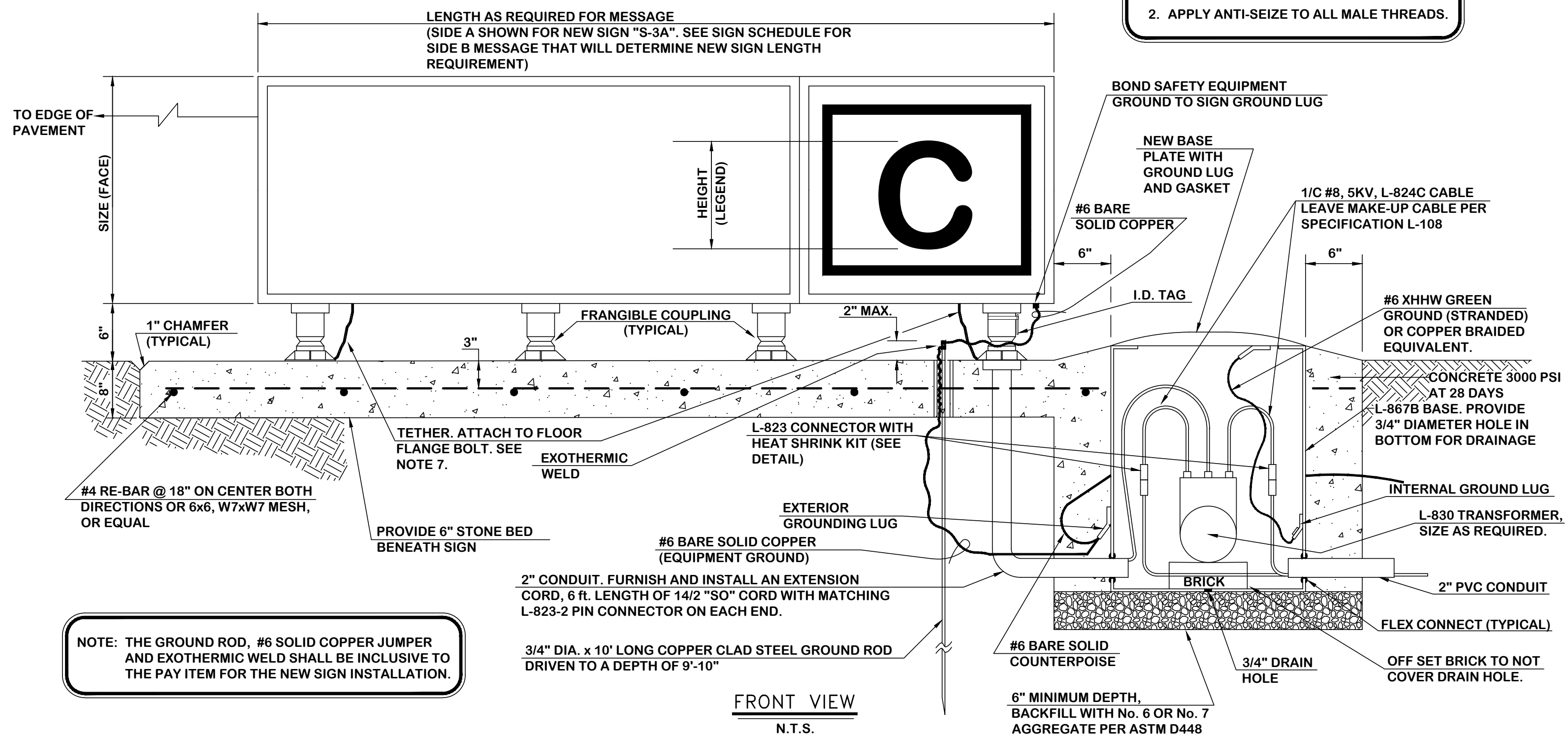
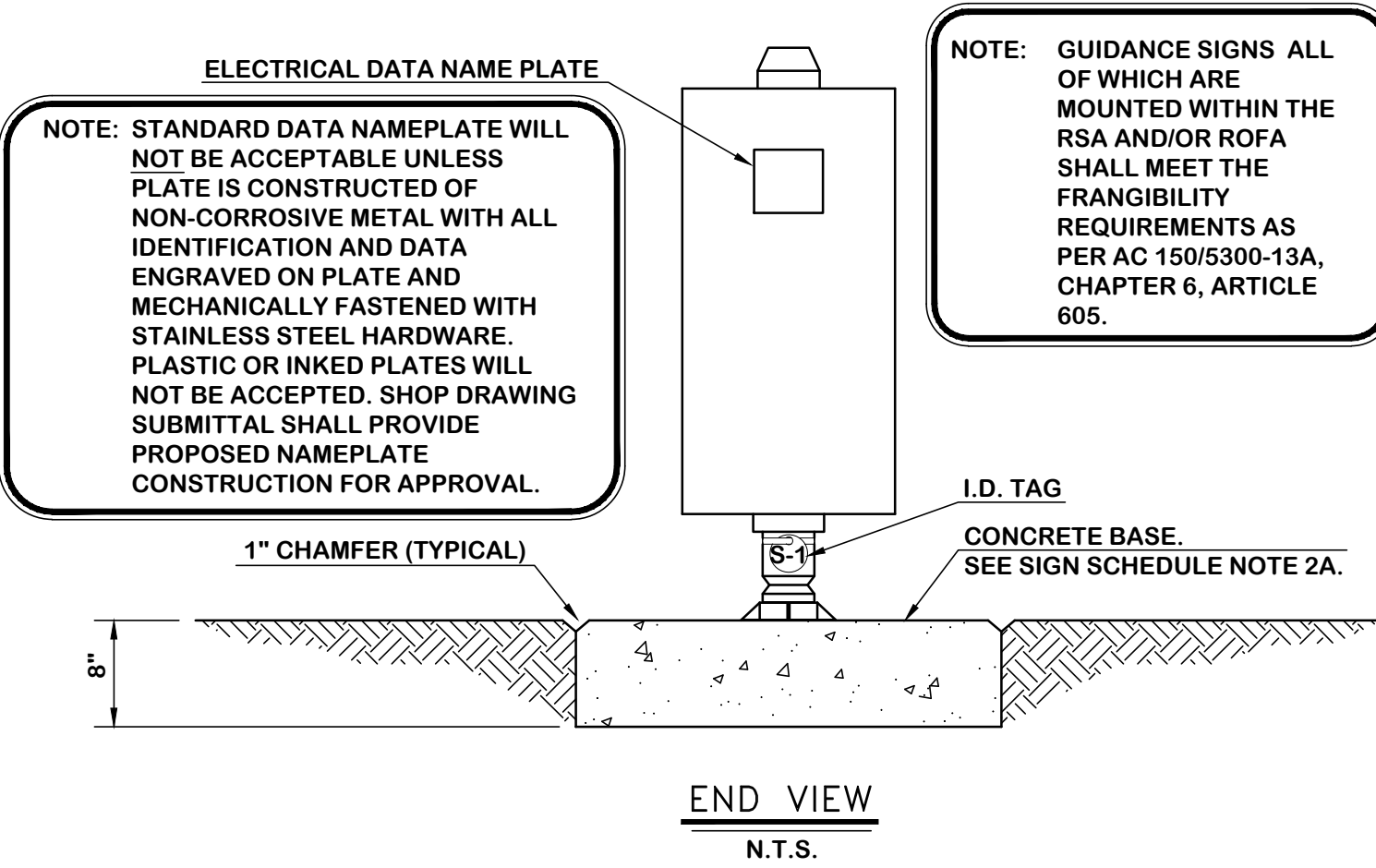
CLASS 1 - (-20C) OPERATION.
CLASS 2 - (-55C) OPERATION.
- 5. "/ " REPRESENTS A SPACE OR DIVIDER BAR BETWEEN THE CHARACTERS ON THIS SCHEDULE.
- 6. SEE SPECIFICATION L-125 AND NOTE ON "END VIEW" DETAIL ON THIS SHEET FOR DATA NAME PLATE REQUIREMENTS.
- 7. SIGNS THAT CONSIST OF MULTIPLE SEPARATE MODULES (NOT CONNECTED TOGETHER IN A CONTINUOUS FRAME) SHALL HAVE A MINIMUM OF ONE TETHER PER MODULE. SIGNS THAT USE MULTIPLE MODULES CONNECTED TOGETHER IN A CONTINUOUS FRAME SHALL HAVE A TETHER AT BOTH ENDS A MINIMUM OF TWO(2) TETHERS.

SIZE 2 SIGN NOTES

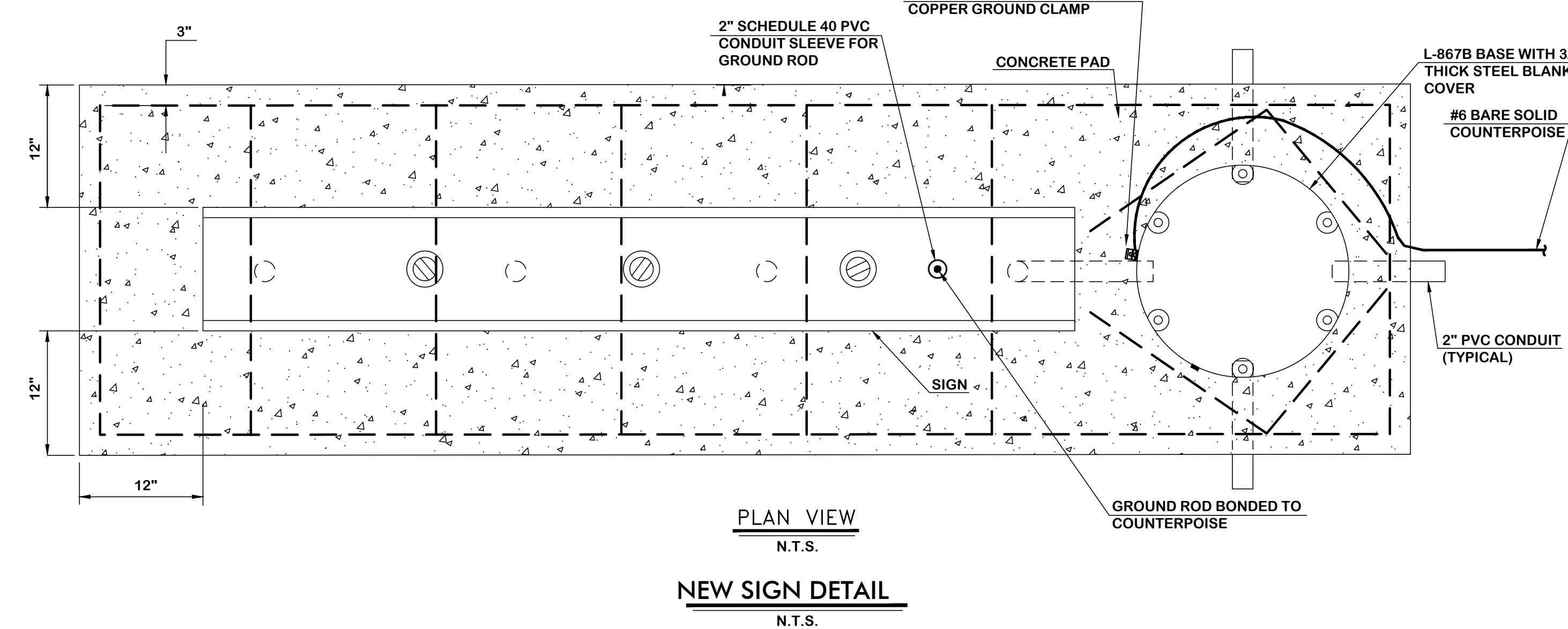
- 1A. SIZE 2 TAXIWAY SIGN HOUSINGS SHALL BE LOCATED AS SHOWN OFF THE FULL STRENGTH EDGE OF PAVEMENT. (SIGNS SHALL BE LOCATED AT A MINIMUM DISTANCE OF 20 FEET AND A MAXIMUM DISTANCE OF 35 FEET FROM THE EDGE OF TAXIWAY FULL STRENGTH PAVEMENT TO THE NEAREST SIDE OF THE SIGN HOUSING.)
- 2A. SIGN PAD LOCATIONS SHALL BE STAKED OUT BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- 3A. ALL SIGNS SHALL BE CONNECTED TO THE SIGN CIRCUIT AS NOTED IN THE SIGN SCHEDULE.
- 4A. SIGN ARRAYS SHOWN AS SINGLE SIGNS. ACTUAL ARRAY MAY BE TWO SIGNS (OR MORE) WHERE SIGN LENGTH EXCEEDS FAA SPEC AC 150/5345-44. PAY ITEM SHALL BE FOR THE TOTAL SIGN ARRAY AS SHOWN IN "MESSAGE" ON SIGN SCHEDULE INSTALLED COMPLETE. NO SEPARATE PAYMENT WILL BE MADE FOR INDIVIDUAL SIGNS IN THAT ARRAY. THE DISTANCE BETWEEN THE MULTIPLE SIGN HOUSINGS IN THE ARRAY SHALL NOT BE LESS THAN 3" AND NO MORE THAN 12".
- 5A. THE SHOP DRAWING SUBMITTAL FOR APPROVAL SHALL USE THE SAME NUMBERING SYSTEM AS SHOWN IN THE SIGN SCHEDULE. SUBMITTAL SHALL INCLUDE EACH SIGN ARRAY VA LOADING.
- 6A. ARROW ORIENTATION SHALL BE DETERMINED FROM DRAWINGS AND NOT FROM THE "L-858 SIGN SCHEDULE". THE SIGN MANUFACTURER SHALL BE FURNISHED DRAWINGS BY THE CONTRACTOR TO ALLOW FOR ACCURATE ARROW ORIENTATION. THE SHOP DRAWING SUBMITTAL FOR SIGNS SHALL INCLUDE ARROW ORIENTATION.

NOTES:

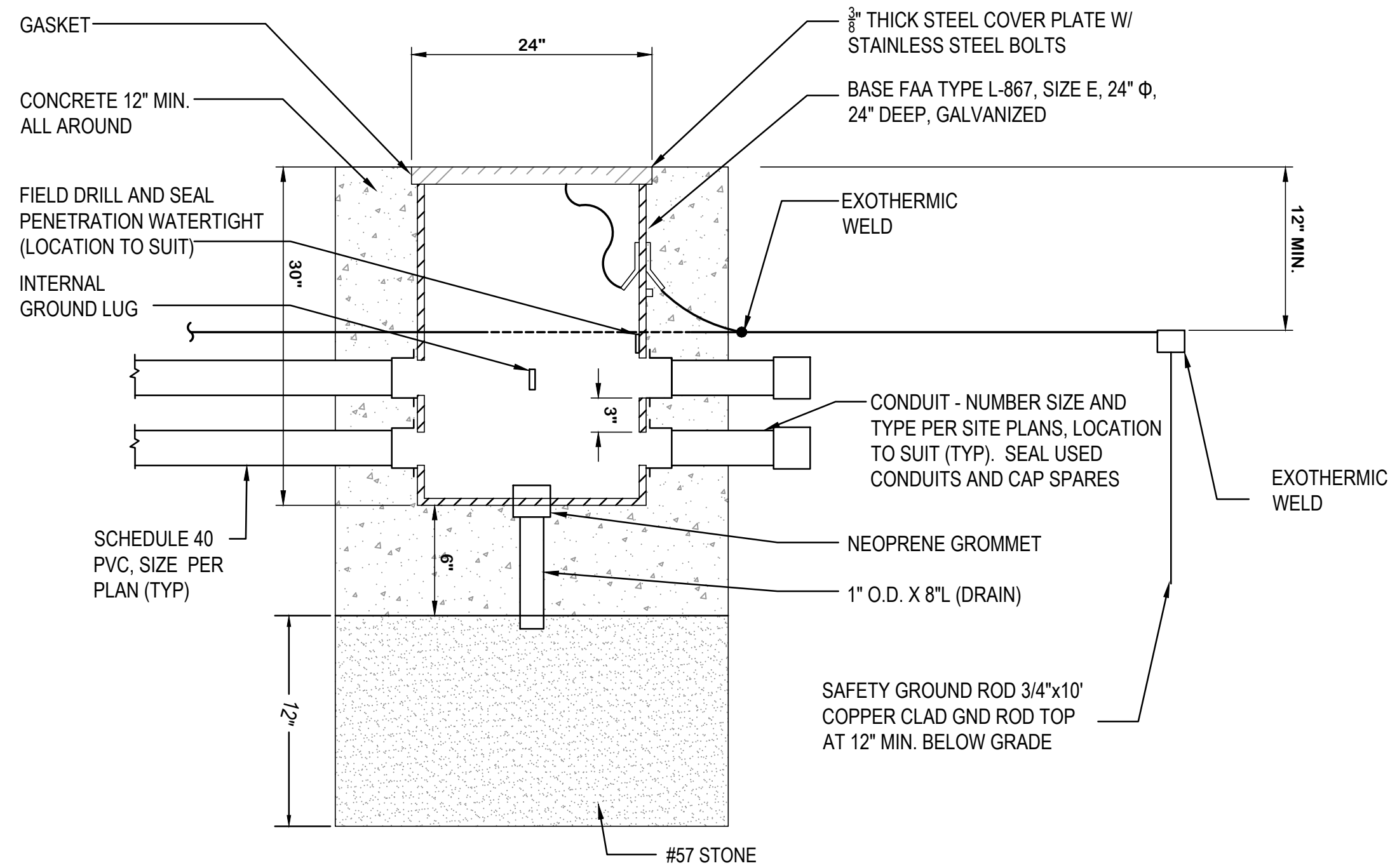
- 1. ALL BOLTS SHALL BE STAINLESS STEEL.
- 2. APPLY ANTI-SEIZE TO ALL MALE THREADS.



NOTE: THE GROUND ROD, #6 SOLID COPPER JUMPER AND EXOTHERMIC WELD SHALL BE INCLUSIVE TO THE PAY ITEM FOR THE NEW SIGN INSTALLATION.



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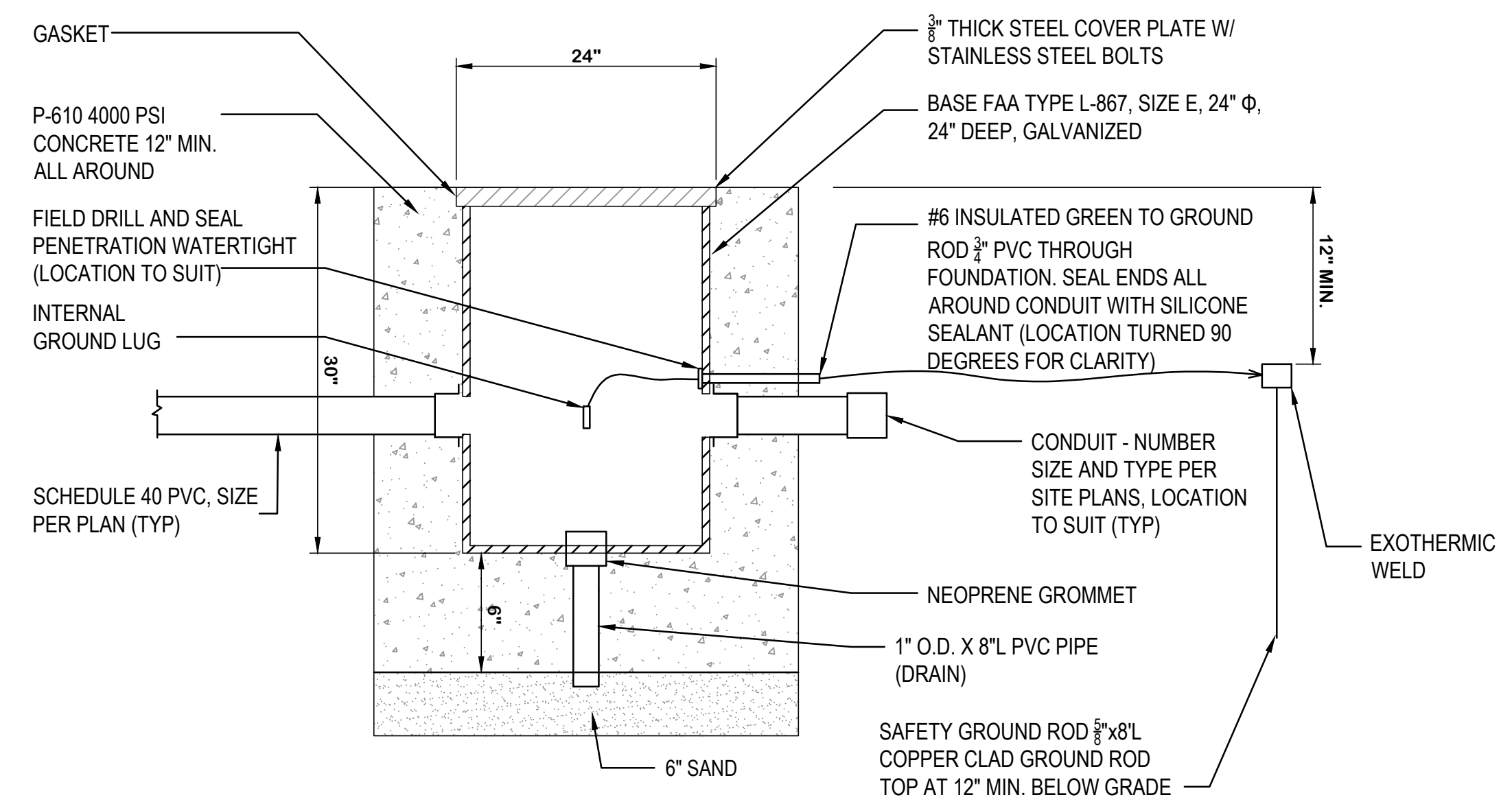


L-867 JUNCTION CAN (AIRCRAFT RATED)

NOT TO SCALE

NOTES:

- INSTALL PVC CONDUIT CONNECTING LIGHT BASES AS SHOWN ON LAYOUT. NO SHARP ANGLES SHALL BE MADE IN CONNECTING CONDUIT TO A NEW LIGHT BASE THAT MIGHT INHIBIT PULLING CABLE.
- INSTALL COUNTERPOISE AND GROUND AS SHOWN ON THIS AND OTHER ELECTRICAL DETAILS.
- ALL LIGHT BASES SHALL BE PROPERLY POSITIONED, ALIGNED AND INSPECTED PRIOR TO PLACING CONCRETE. ANY BASE INSTALLED INCORRECTLY SHALL BE REMOVED AND RE-INSTALLED AT CONTRACTORS EXPENSE ACCORDING TO DESIGN CRITERIA AND PRIOR TO PLACING CONCRETE. ENSURE TIGHT CONNECTIONS BETWEEN CONDUIT SECTIONS AND LIGHT BASE TO PREVENT CONCRETE FROM ENTERING LIGHT BASE OR CONDUIT.
- ENCASE LIGHT BASE AS SHOWN. VERIFY CONCRETE ENCASEMENT UNDER AND AROUND LIGHT BASE MEETS DIMENSIONS SHOWN.
- FOR LIGHT BASES THAT WILL HAVE FIXTURES INSTALLED, INSTALL ISOLATION, TRANSFORMER, GROUND AND CONNECT TO NEW CABLE AS SHOWN.
- DO NOT USE SHIPPING BOLTS FOR LIGHT FIXTURE INSTALLATION (AS REQUIRED). USE ONLY STAINLESS STEEL, TYPE 316 FOR LIGHT FIXTURE INSTALLATION, OF A LENGTH THAT ALLOWS MINIMUM OF THREE EXPOSED THREADS WHEN FULLY ENGAGED AND TORQUED, WITH A MANUAL, CALIBRATED TORQUE WRENCH, TO THE MANUFACTURER'S SPECIFICATIONS. USE TWO LOCK WASHERS FOR EACH BOLT.
- INSTALL CIRCUIT ID TAG ON EACH CABLE.



L-867 JUNCTION CAN (ONE CONDUIT)

L-867 JUNCTION CAN

NOT TO SCALE

NOTES FOR INSTALLATION OF BASE CANS AND FIXTURES IN TURF

- INSTALL PVC CONDUIT CONNECTING LIGHT BASES AS SHOWN ON LAYOUT. NO SHARP ANGLES SHALL BE MADE IN CONNECTING CONDUIT TO A NEW LIGHT BASE THAT MIGHT INHIBIT PULLING CABLE.
- INSTALL COUNTERPOISE AND GROUND AS SHOWN ON THIS AND OTHER ELECTRICAL DETAILS.
- ALL LIGHT BASES SHALL BE PROPERLY POSITIONED, ALIGNED AND INSPECTED PRIOR TO PLACING CONCRETE. ANY BASE INSTALLED INCORRECTLY SHALL BE REMOVED AND RE-INSTALLED ACCORDING TO DESIGN CRITERIA AND PRIOR TO PLACING CONCRETE. ENSURE TIGHT CONNECTIONS BETWEEN CONDUIT SECTIONS AND LIGHT BASE TO PREVENT CONCRETE FROM ENTERING LIGHT BASE OR CONDUIT.
- ENCASE LIGHT BASE AS SHOWN. VERIFY CONCRETE ENCASEMENT UNDER AND AROUND LIGHT BASE MEETS DIMENSIONS SHOWN.
- FOR LIGHT BASES THAT WILL HAVE FIXTURES INSTALLED, INSTALL ISOLATION, TRANSFORMER, GROUND AND CONNECT TO NEW CABLE AS SHOWN.
- DO NOT USE SHIPPING BOLTS FOR LIGHT FIXTURE INSTALLATION (AS REQUIRED). USE ONLY STAINLESS STEEL, TYPE 316 FOR LIGHT FIXTURE INSTALLATION, OF A LENGTH THAT ALLOWS MINIMUM OF THREE EXPOSED THREADS WHEN FULLY ENGAGED AND TORQUED, WITH A MANUAL, CALIBRATED TORQUE WRENCH, TO THE MANUFACTURER'S SPECIFICATIONS. USE TWO LOCK WASHERS FOR EACH BOLT.
- INSTALL CIRCUIT ID TAG ON EACH CABLE.

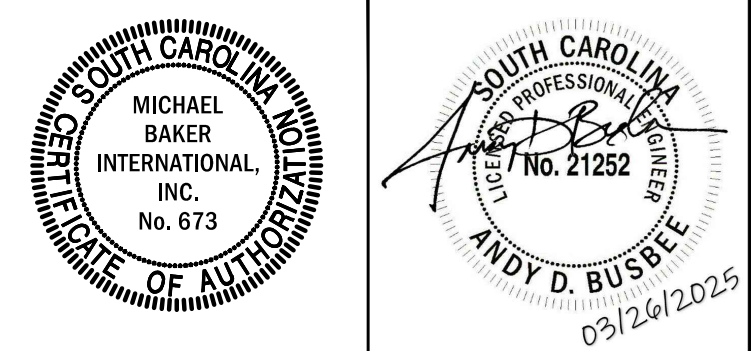
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WALTERBORO, SOUTH CAROLINA

Michael Baker
INTERNATIONAL

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700 Huger Street
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Designer: RES	Checked by: ADB
Technician: NWM	Project Number: 192930

NOTE:
1. SEE SHEET LD-5 FOR SIZE 2 SIGN NOTES.

ISSUED FOR BIDS

REVISIONS			
No.	Description	Date	By

Project Name:
RUNWAY 5-23 REHABILITATION

Drawing Name:
LIGHTING DETAILS PLAN NO. 5

FAA A.I.P. Project Number: 3-45-0057-025-2025	
Date: MARCH, 2025	Division: ELECTRICAL
Scale: AS NOTED	Sheet Number: LD-5
Drawing Number: LD-5	